

EASTMOOR STREET

Charlton Riverside

Planning Submission

November 2021

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1.

INTRODUCTION

- 1.1 Summary
- 1.2 Project Team
- 1.3 Application Site



^ Proposed CGI.

1.1 SUMMARY

This design and access statement summarises the proposal for the site at Nos.6 61-81Coppers Yard, Eastmoor Street and No. 6&10 Westmoor Street in Charlton, London, SE7 8LX.

Planning history for the site:
An application was turned down by the planning committee, application ref: 20/1924/F

The current scheme which is documented here was revised from the refusal scheme in line with objectors comments and recommendations.

The application is "Demolition of existing structures and erection of buildings between 4 and 8 storeys in height (including roof access level areas with a maximum total height of 29.5m AOD), comprising 149 residential units, 740.6 sqm flexible employment floorspace (Use class E(g) and B8) and 555 sqm flexible retail and community uses (Use class E and F2) with associated landscaping and new public realm, access and infrastructure works, refuse and recycling storage, car parking and cycle parking and associated development."

This scheme is one of the first coming forward in the Charlton Riverside opportunity area.

Following the previous refusal, the project team have amended the scheme to reflect the objections. We believe the revised storey heights will satisfy the concerns of local residents and fit seamlessly into the evolving fabric of Charlton Riverside.

1.2 PROJECT TEAM

Aitch Group

Client
www.aitchgroup.co.uk

Alan Camp Architects LLP

Architects
https://alancamp.com/

Studio Bosk

Landscape Architect
https://www.studio-bosk.com/

BPTW

Planning Consultant
www.bptw.co.uk

Avison Young

Daylight / Sunlight Consultant
http://www.avisonyoung.co.uk

JAW Sustainability

Sustainability Consultant
www.jawsustainability.com

Ardent

Transport Consultant
www.ardent-ce.co.uk

Lexington Communications

Political Consultant
www.lexcom.co.uk

Form Structural Design

Structural Engineer
www.form-sd.com

Clarke Banks

Fire Engineer
www.clarkebanks.com

TPS

M&E Consultant
www.taylorprojectservices.com

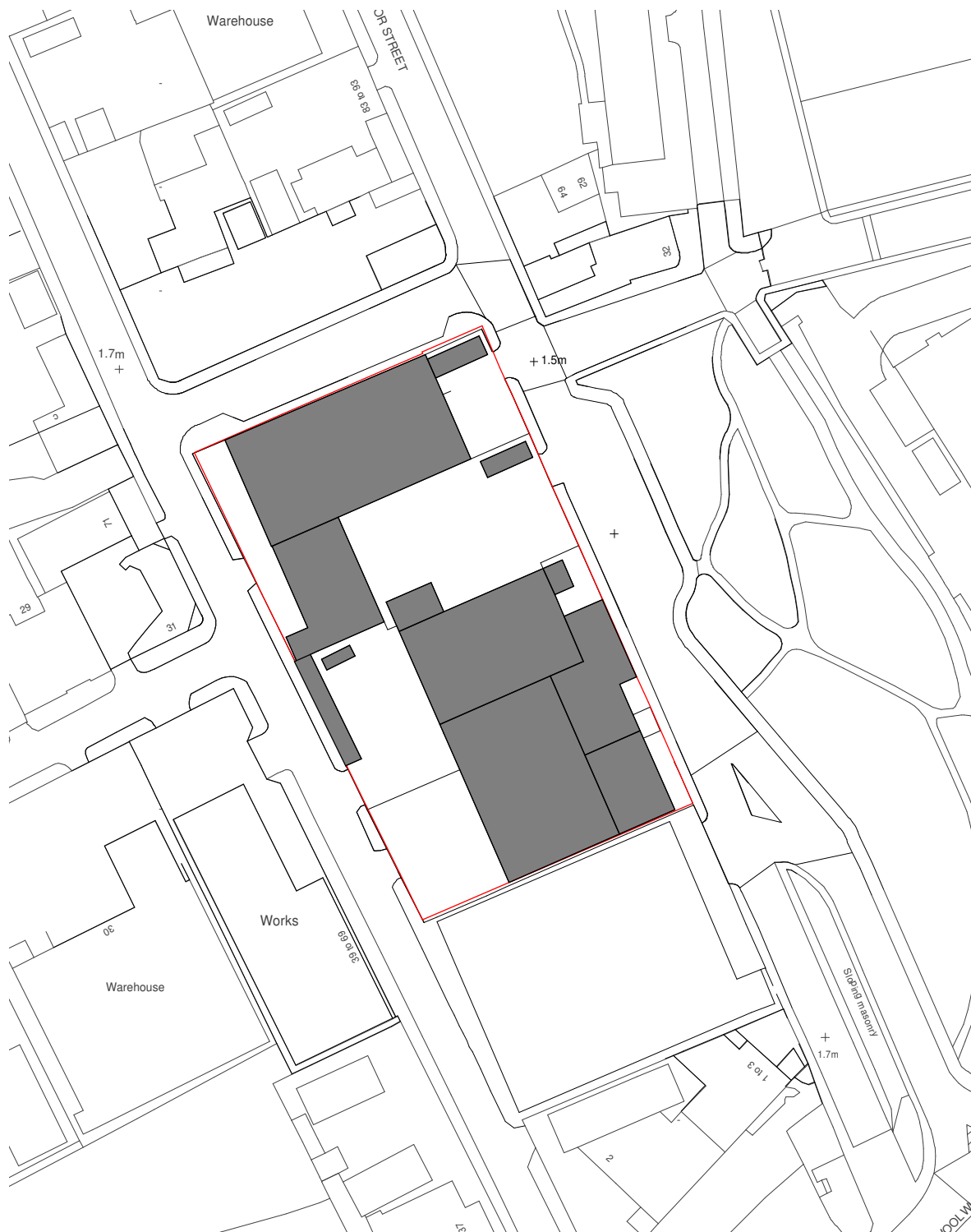
Lichfields

TVIA
www.lichfields.co.uk

The Visualiser Ltd

CGI's
www.thevisualiser.co.uk

This **DESIGN & ACCESS STATEMENT** has been prepared for Aitch Group, with input across the project team to support the planning application for the site at Eastmoor Street.



^ Existing site plan

1.3 APPLICATION SITE

The site address is 'Land at Nos. 6, 61-81 and Coopers Yard, Eastmoor Street and Nos. 6 & 10 Westmoor Street, Charlton, London, SE7 8LX'

The site sits to the North West of the Charlton Riverside industrial area. The Charlton Riverside SPD documents the development aspirations for the area.

On site there are a series of industrial sheds or 1-2 storeys housing a variety of Sui Generis business. This condition is matched in the surrounding area.

To the East of site is Moore Park, to the North, West and South low rise industrial area.

2.

SITE CONTEXT

- 2.1 The site
- 2.2 Planning policy context
- 2.3 Emerging context



^ Aerial views, source: Google Images

2.1 THE SITE

Charlton Riverside

Site Location

The site is located in the Charlton Riverside industrial estate in the Borough of Greenwich. The site is located between Eastmoor and Westmoor Street with the northern boundary being defined by Mirfield Street. To the east of the site lies Moore Park which extends to the Thames path and Thames Barrier to the north. To the south the park extends to the Woolwich Road. The park is proposed as a north-south green route within the masterplan linking Maryon Park to the south to the Thames pathway.

Local Character

The site sits in the heart of the existing industrial estate currently characterised by industrial warehouses ranging from one to three storeys

in height. A locally listed Pub, now used as a veterinary practice, sits to the north east of the site.

Emerging Context

Charlton Riverside is designated as an opportunity area by the London Plan and RBG's Charlton Riverside SPD. The Charlton Riverside SPD outlines the vision for the area to deliver 5000-7500 new homes and 4,400 additional jobs. In the local area, several planning applications have already been submitted in recent years. The evolution of the area is starting to take place and the massing, scale and proposed uses of the emerging context reflect this process.



^ Aerial views, source: Google Images

V 1 - Westmoor Street looking north

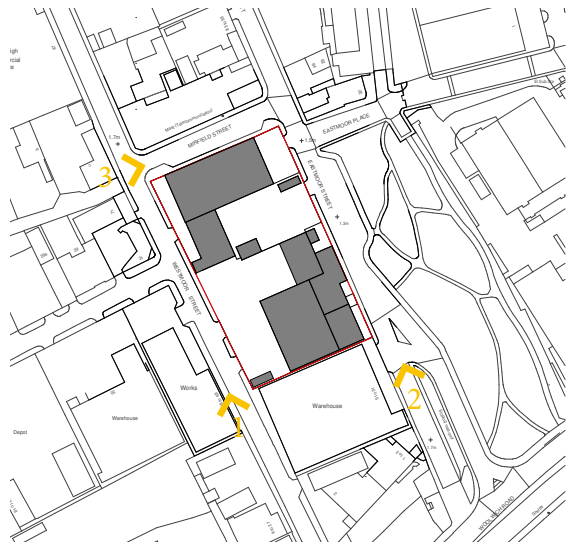


V 3 - Mirfield Street looking east



^ 2 - Eastmoor Street looking north

The street views illustrate the existing boundary conditions of the site. The light industrial uses have created an area with compromised pedestrian accessibility. Numerous vehicles are parked on pavements forcing pedestrians to walk in the carriageway of the surrounding roads. This creates a very unsafe environment for pedestrians. The site currently has no through routes between Eastmoor 7 Westmoor Street, access is from the entrance on Eastmoor Street only.





^ Site - existing uses

The site consists of a 0.592 hectare rectangular parcel of land defined by Eastmoor Street, Westmoor Street, Mirfield Street to the north and an industrial unit to the south.

The site is currently occupied by a number of industrial units/ warehouses:

1 - Purple Outline

Type: Storage
Class: B8

2 - Blue Outline

Type: Storage
Class: B*

3 -Orange Outline

Type: Car Breakers / Scrap Yard
Class: Sui Generis

4 - Yellow Outline

Name: All Scrap London Metal Ltd
Type: Scrap Metal Dealer / Yard
Class: Sui Generis

5 - Red Outline

Name: W Rackley & Co Ltd
Type: Commercial vehicle maintenance, parts & repairs
Class: Sui Generis/B2

6 - Green Outline

Name: Whitton Castings
Type: Metal Castings
Class: Sui Generis/B2

Existing Site Commercial GIA = 2,354m²

v Green & blue SPD infrastructure overlaid on existing site map.



^ SPD Extract: Public open space provision

2.2 PLANNING POLICY CONTEXT

Charlton Riverside SPD

The Charlton Riverside SPD was published in June 2017. The diagram adjacent has been developed by extracting the infrastructure strategy from the SPD and overlaying it with the existing plot and road patterns. Aligning the future provisions of the SPD with the existing context/site ownerships will ensure the viability of proposed roads, pedestrian routes and green landscaped spaces.












East-West Road and Secondary Green Link

The site occupies a key location on the proposed east - west link road and also the secondary green link. The new east-west link road is currently proposed to pass to the north of the site and continue east to Warspite Road. However, the location of the locally listed public house will prevent this being realised in its current drawn

form. Discussions with the Council and TfL have taken place to agree the alignment of the new road and address this issue. This process is detailed in the 'Design Development' section of this report.

In terms of existing site ownership and road layout, the secondary green link has the potential to naturally develop as a continuation of the existing Penhall Road. This placement allows for the secondary green link to extend through the centre of the site and be animated by residential entrances and the active frontages of the commercial units.

Key

- | | | |
|---|--|--|
|  Site boundary |  Key civic, public realm |  Potential 'Green Bridge' link |
|  District parks (including sports provision) |  Secondary public realm corridors |  Primary street tree planting |
|  Neighbourhood and local parks |  School grounds |  Secondary street tree planting |
|  Riverside pocket parks |  Sustainable drainage within open space network | |



2.3 EMERGING CONTEXT

Existing Context

The greatest challenge facing the Charlton Riverside area is the contrast between the existing industrial uses and buildings on the sites and the emerging context of mixed use commercial and residential schemes. The area also includes the safeguarded wharf which will continue to function as a wharf with direct access to the river Thames. HGVs associated with wharf activities will continue to use the local road network.

Emerging Context

The Charlton Riverside SPG creates a policy framework to guide the incremental re-development of the area. The area currently consists of a series of sites and fragmented parcels of land within multiple ownerships, also including land owned by RBG. The SPG seeks to ensure that piecemeal development does not fetter the wider, fundamental key ambitions for the area - new road links, pedestrian and cycle routes and the development of green amenity spaces. The SPG also sets out RBG's policy in terms of height and density for the area.

The initial proposals emerging through the planning process, although not yet consented, reflect the potential emerging context.

Flint Glass Wharf - Komoto

Stone Foundries - Montreaux

Faraday Works- U & I

Herringham Quarter - Hyde Housing



^ Illustrative Masterplan

Herringham Quarter

Client: Hyde Housing

Architect: Alan Camp Architects and Fletcher Priest Architects

Location: Charlton Riverside / Front

The Charlton Riverside SPD outlines the vision for the area to deliver 5000-7500 new homes and 4,400 additional jobs. Herringham Quarter masterplan will in total deliver up to 1,212 new homes. The masterplan for the area has been developed and designed by Fletcher Priest Architects.

The overarching principles of the masterplan are to:

- Deliver a vibrant, diverse, mixed use neighbourhood
- Provide early delivery of affordable homes
- Open up the riverside
- Integrate existing and new employment uses
- Link with the community south of Woolwich Road
- Establish a framework for coordination across adjoining land ownerships

The masterplan translates the principles and intentions of the SPD into a viable vision. Site

boundaries are defined that reflect both the existing context and the future provisions for roads, green links and pedestrian routes.

Alan Camp Architects designed the detail application for sites A and C and therefore have a strong understanding of the SPD and the vision for Charlton Riverside. The principles that were developed during this design process can be applied to the Eastmoor Street scheme to ensure a strong neighbourhood connectivity between the sites.



Herringham Quarter - Site A

Site A delivers 500 homes within three blocks, ranging from 4 - 10 storeys in height. Commercial spaces are provided on the ground floor to create active frontages to pedestrian routes and landscaped areas. A community use and food & beverage units are offered along the Thames path whilst a convenience store and a small service unit eg a post office are offered on Herringham Road. The character of the scheme builds on the site's industrial past whilst creating a modern, contemporary appearance.



^ Proposed CGI's showing Site A

^ Typical Site A floor plate showing dual aspect



Herringham Quarter - Site C

Site C delivers 218 homes within three blocks, ranging from 6- 10 storeys in height. Commercial spaces are provided on the ground floor including a two storey community/leisure use adjoining a proposed neighbourhood square. Other proposed uses include workshops, a creche and a healthcare use eg. a dentist. The commercial units are framed within a two storey plinth across all three buildings. New Lydenberg Street is lined with workshop spaces whilst Lydenberg Gardens, to the rear, is overlooked by the creche and healthcare uses.



^ Proposed CGI's showing Site C

^ Typical floor plate Site C

V Proposed street views



Evelyn House site

Client : Optivo

Architect: BPTW Architects

Location: Eastmoor Street

BPTW Architects have developed proposals for the Evelyn House site, the client is Optivo Housing Association. The site sits immediately to the south of the Eastmoor site.

Following consultation with Optivo, an agreement has been reached to set both schemes 9m away from the shared boundary creating an 18m separation between the sites. This allows the space between the sites to become a welcoming, landscaped pedestrian area. The Eastmoor site proposal has commercial units edging this space providing an active frontage and natural surveillance.

Vehicular access will be limited to service vehicles and a car club space. The Evelyn House site scheme currently proposes car park access,

V Proposed typical plan



refuse stores and plant/service spaces edging the boundary.

The images adjoining were presented at a recent public consultation. The proposal is expected to deliver circa. 67 homes within blocks ranging from 4-7 storeys. Two commercial units are proposed at ground floor.

The scheme was refused at committee in July 2021 following concerns over the height compared to the SPD.



^ Proposed street views

3.

SITE INVESTIGATION

3.1 Opportunities and Constraints



^ Existing site plan showing constraints

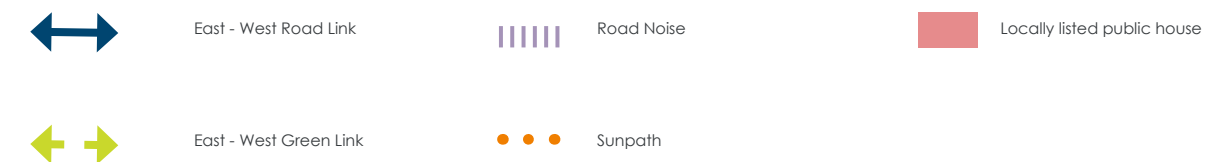
3.1 OPPORTUNITIES & CONSTRAINTS

Constraints map

The site has a number of constraints to overcome, some of which, can also be seen as unique opportunities such as the east-west proposed green link and the creation of the first section of the east-west link road. The industrial uses surrounding the site will remain in use in the short term but gradually begin to diminish as various development plots come forward. The Evelyn House site, immediately to the south of the site, is following a similar development timeline and proposes a mixed-use residential scheme.

The protected wharf located on the riverfront will however, remain in use generating traffic noise along Westmoor Street. To mitigate against this, wintergardens will be provided as private amenity along this street. Eastmoor Street may eventually be closed to traffic in line with SPD provisions to extend the park. The Environment Agency have currently confirmed they still require access but the scheme will be designed to ensure proposals are viable should the road be closed even to servicing vehicles. The flood risk of the site means residential accommodation cannot be provided at ground floor.

1. Industrial uses surrounding the site
2. Noise of HGVs along Westmoor Street
3. Air quality
4. Flood zone 3
5. Allowance for E-W link road within SPD
6. Green link within SPD
7. Sunpath
8. Neighbouring properties- emerging schemes
9. Access required by Environment Agency along Eastmoor Street
10. Improvements to Moore Park and the potential closure of Eastmoor Street within SPD
11. Locally listed public house
12. TE2100 residential level set at min 6.5m





^ Site plan showing opportunities

Opportunities map

The site's position on the fringe of the Charlton Riverside area ensures safe, quick access for new residents along Eastmoor Street from Woolwich Road. This route avoids any potential conflict between industrial vehicles and pedestrians along Westmoor Street whilst remaining industrial sites are developed. Moore Park to the east, creates the opportunity to provide residents with extensive views whilst providing natural surveillance to the park. The scheme can improve the park boundary by restricting Eastmoor Street to servicing vehicles only; helping to create a safe pedestrian route. The site has the opportunity to provide the first section of both the east-west road and the east-west green, pedestrian link.

1. District centre within SPD, opportunity to provide a range of commercial uses
2. Potential to provide active frontages on all sides of the site
3. Ensure delivery of the E-W road link by reducing the build zone of the site
4. Delivery of the SPD green link- opportunity to create green landscaped space
5. Potential to improve Eastmoor Street
6. Utilise views from the site to the neighbouring Moore Park, the river Thames and Maryon Park/ Gilbert's Pit
7. Conceal car parking
8. Improve street environments on all sides of the site
9. Create south facing amenity for residents



4.

DESIGN EVOLUTION

- 4.1 Project Timeline
- 4.2 Initial Strategy
- 4.3 Design Development
- 4.4 Public Consultations
- 4.5 Previous Application
- 4.6 Refused scheme development



4.1 PROJECT TIMELINE

Oct 2019 - Sept 2021

The project commenced in October 2019 with a Pre-application submitted to RBG by BPTW Architects. Alan Camp Architects replaced BPTW Architects in November 2019. There has been an intensive programme of Pre-Application meetings with the Royal Borough of Greenwich (RBG), the Greater London Authority (GLA) and Transport for London (TfL). A series of meetings including a site visit have also been held with RBG's Design Review Panel. All meetings and consultations from the 23 March have been held virtually due to the lockdown imposed by the COVID -19 pandemic. Meetings and consultations have included :

- 8no. presented design iterations over nine months
- 3no. formal Pre-application meetings with RBG & other design progress meetings
- 2no. Pre-application meetings with GLA
- 2no. presentations to RBG's independent Design Review Panel
- 1no. Council Leader Meeting
- 1no. Online public consultation
- 1no. community webinar
- Video call with Royal Greenwich Trust School

Meetings

- Oct 2019 Initial Feasibility/Pre-application 01

BPTW Architects appointment ends

- Nov 2019 Alan Camp Architects replace BPTW

- 07 Jan RBG Regeneration Meeting

- 16 Jan 2020 Environment Agency Meeting

- 27 Jan 2020 Pre- application 02 RBG

- 30 Jan 2020 Pre- application 01 GLA

- 17 Feb 2020 Design Review Panel Briefing

- 28 Feb 2020 Site Visit & Design Review Panel 01

- 31 March 2020 Pre-application 02 GLA

- 22 April 2020 Pre-application 03 RBG

- 01 May 2020 Design Review Panel 02

- 05 May 2020 Public Consultation

- 07 May 2020 Landowners Meeting RBG

- 18 May 2020 Design Progress Meeting RBG

- 22 May 2020 Video Call Royal Greenwich

School Trust

- 22 May 2020 Video Call Local Ward Councillor

John Fahy

- 17 June 2020 Briefing to Council Leaders

- 17 June 2020 Community Webinar

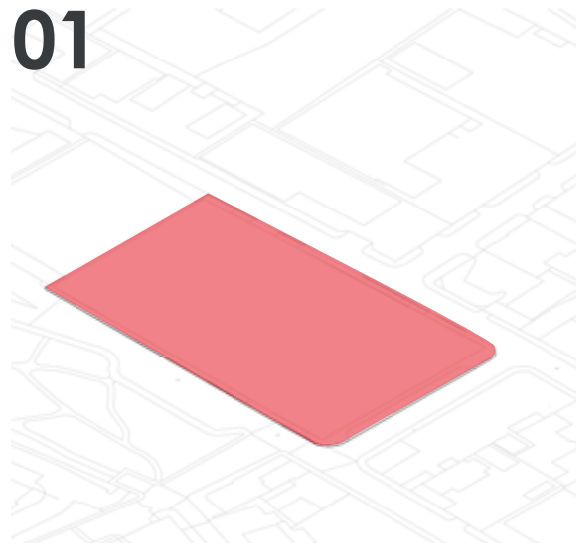
- May 2021 Charlton Together

- June 2021 Design Workshop

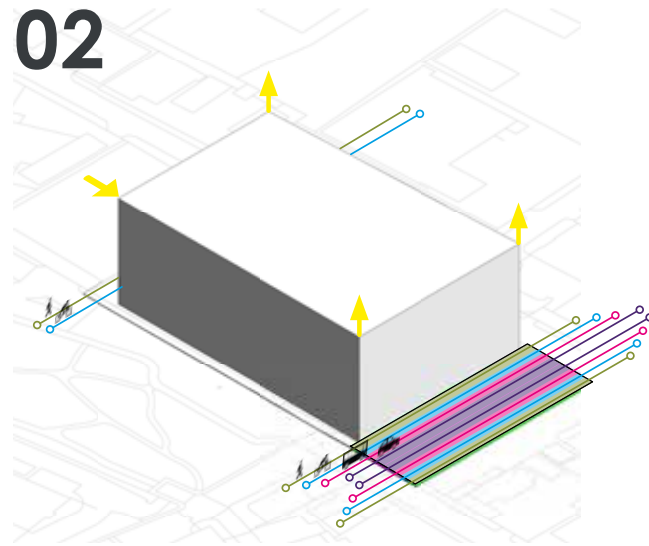
- July 2021 Planning Board

- Nov 2021 Planning Submission

01



02



4.2 INITIAL STRATEGY

Massing

01 - Site sits on a North - South Orientation

02 - Maximum mass extruded whilst allowing for new green link to the south and east-west Link to the north for cars, bus & cycle lands

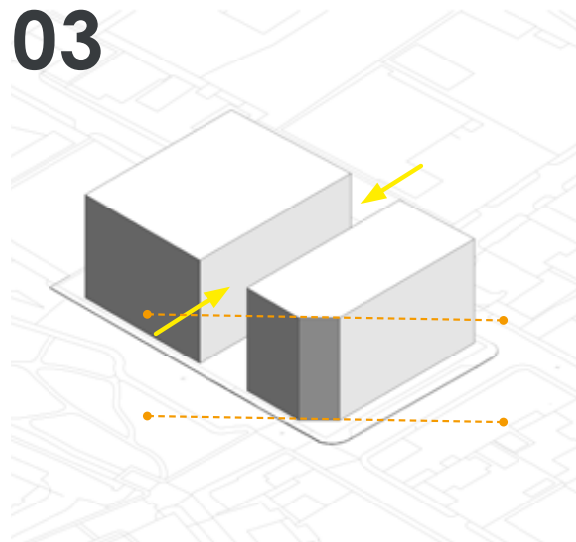
03 - Division of mass through the centre of site, continuing the route from the existing Penhall Road and creating a second green link.

04 - Courtyard massing on upper floors with 21m distance separation for light and views. Use of deck access to increase dual aspect % on flats

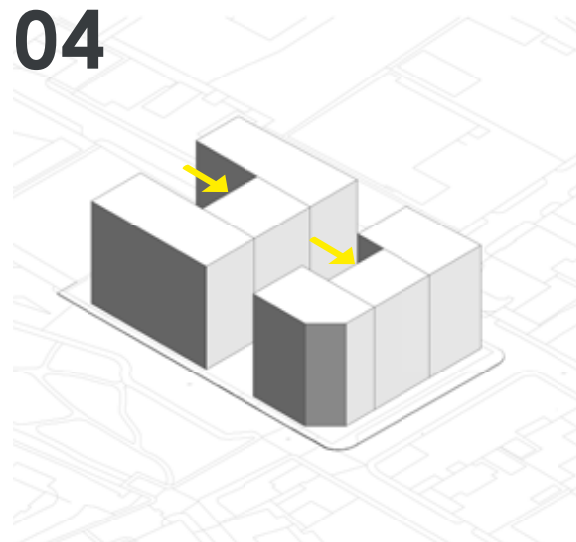
05 - Lower the South blocks to increase the daylight entering the green link. Reduction in height of the North East block to have a more subservient relationship to the locally listed pub (now veterinary centre).

06 - Use of roof spaces for communal amenity areas and boosting the biodiversity of the build.

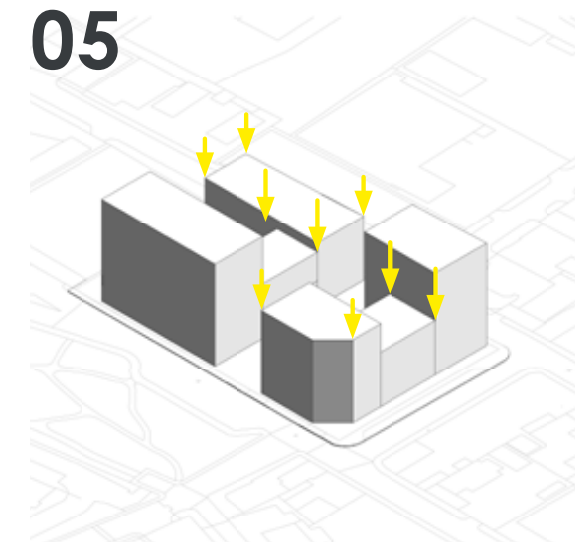
03



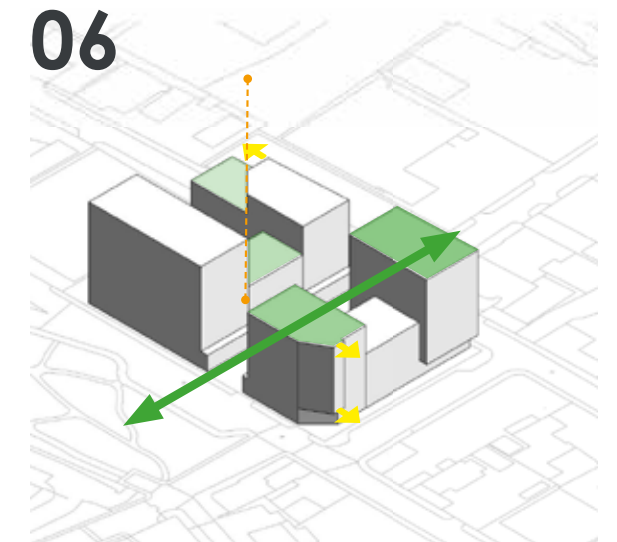
04



05



06



4.3 DESIGN DEVELOPMENT



RBG Pre-application 01
// 08.20

7-10 Storeys
232 Homes
1,350m² Commercial space

BPTW



RBG Design Review Panel
// 02.20

4-10 Storeys
230 Homes
2261m² Commercial space
Two blocks: North and South
Two green links created through site

ACA



Planning Submission
// 06.20

6-10 Storeys
202 Homes
1,801m² Commercial space
Two blocks: North and South
Two green links created through site

ACA

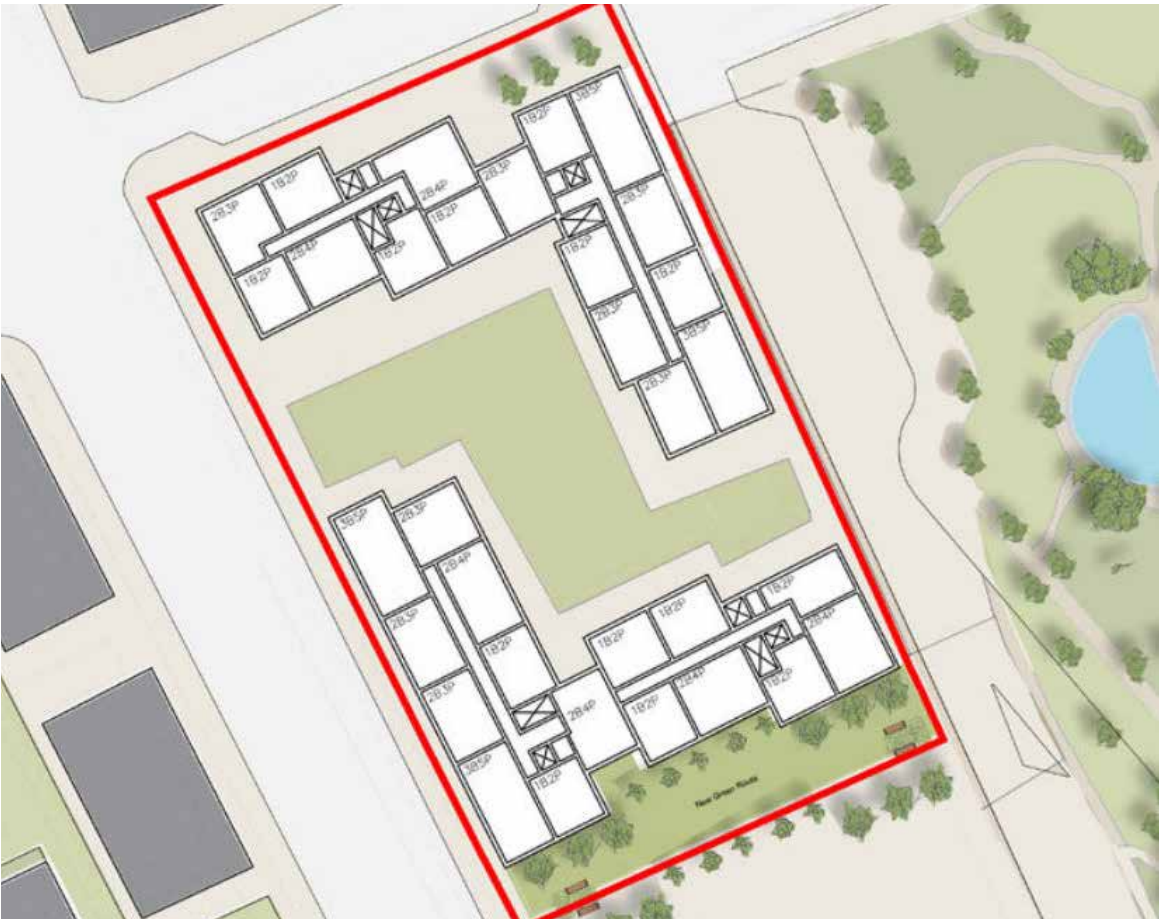


Planning Submission
// 11.21

4-8 Storeys
149 Homes
1,296m² Commercial space
Two blocks: North and South
Two green links created through site

ACA

V Proposed massing: north-east corner - BPTW Architects



^ Proposed typical floor plate BPTW

RBG Pre-application 01 (BPTW) // 08.20

In October 2019, Aitch and their architects BPTW met with RBG to discuss proposals for the site.

The scheme proposed a comprehensive redevelopment of the site to deliver 232 homes (35% affordable housing) with approximately 1,350m² of commercial space at ground floor. The built form created a landscaped podium at first floor enclosed by blocks ranging from 7 to 10 storeys, with car parking at grade under the landscaped podium.

Meeting Notes from Pre-application Meeting 01 with RBG - 08 October 2019

Officers repeated the advice given at the Developer Forum that developers should hold off submitting an application for 6 months.

Officers noted that the proposal exceeded the heights indicated in the Masterplan SPD and suggested that the proposal was too tall but did not indicate what height they considered acceptable.

Officers suggested that there could be more variation in height between different massing elements and felt that further design/townscape justification was required.

Officers accepted that the SPD indicates that the north west of the site is intended to contribute to a new Local Centre

Officers confirmed that the recent adoption of the Conservation Area and the sensitive receptors that are the two Locally Listed Buildings were key factors that should be considered when proposing the height of the proposal. Officers did not confirm the aspects of the Locally listed Buildings that would be most important to be protected.

Officers supported the principle of redevelopment but required further justification on the height/density of the proposed development.

Officers appreciated that the width of the new east-west green link to the south of the Site is difficult to confirm due to the scale of the Masterplan information.

Officers accepted that there is limited detail in the Masterplan regarding the function or design of the new green route.

Officers urged Aitch to open discussions with the neighbouring land owners but understood the confidentiality and sensitivities currently at play.

Officers confirmed that the Core Strategy seeks 35% Affordable housing. The GLA will seek 50% as the Site is former SIL land. Mix sought is 70/30 in favour of London Affordable Rent tenure.

V Proposed isometric



^ Proposed long section

RBG Design Review Panel // 02.20

The scheme was developed following comments from the second Pre-planning application with RBG and the first meeting with the GLA.

The scheme had drastically evolved from BPTW's initial scheme, with two new green links bisecting the site creating two main residential blocks: the North block and South block.

Meeting Notes from Design Review Panel 01
28 February 2020

Recommendation that RBG brings together local landowners to discuss how to deliver infrastructure, transport and landscape design in the Charlton area

Develop further detail on how proposed route and green links will connect to existing and proposed routes. Width of green link and play space needs increasing to improve daylight/sunlight, further landscaped space could be provided adjacent to the Optivo site

Investigate alternative route for EA to the Thames Barrier so Moore Park can be improved

Greater understanding of the heritage is required to inform the design, add variation to the elevations to respond to context

Re-consider the vehicular entrance to the south of the site, potential for vehicles to access directly off Westmoor Street

Consider utilising mezzanine levels for cycle storage to improve the size of the commercial units.

Create further variety to height on the site and reduce the height, ensure green spaces receive adequate daylight/sunlight

Look to increase the number of dual aspect units and consider the positioning of family units and their access to amenity. Avoid double loaded access corridors explore different typologies

Simpler expression of building on corner of Mirfield Street and Eastmoor Street, removal of curve.

Possibility to create civic space in this location. Further consideration of building materials, perhaps a lighter brick rather than the red brick proposed

Hierarchy of spaces private/public needs further exploration

Biodiversity to be considered to increase character of landscaping proposed and the adjacent Moore Park

V Proposed isometric



RBG Pre-application 03 // 04.20

Pre-application RBG 03

The scheme was presented virtually by Alan Camp Architects to attendees from RBG.

Meeting Notes from Pre-application RBG 03 - 22 April 2020

Exact number and location of affordable homes should be confirmed prior to submission.

The southern 10 storey block appears bulky and incongruous and it is recommended that the main parapet should be reduced to 6 storeys with the potential to explore a two storey set-back element above.

There are also concerns about the form of the northern 10 storey block. It is considered that the corner 10 storey block should be expressed more legibly as the local marker on the site and the rest of the building simplified and possibly lowered down by a storey, with a more subtle marker at the junction with Eastmoor Place, towards the locally listed pub. The tallest element of the scheme reads as too broad and bulky and it is suggested that the option of subdividing this block into two slightly shifted elements, with a taller element at the street junction and a lower one towards the green link, is considered.

It is proposed that the non-residential floorspace would be for flexible use and it is noted that the commercial units within the southern block would have floor to ceiling heights of 6m to allow for a range of users and the insertion of mezzanine floors. Whilst the desire to provide flexible spaces is understood, further information should be provided with regard to the detailed layout / specification of the units to demonstrate that they will meet the needs of accommodating employment generating uses (B1).



^ View south along Eastmoor Street



^ View along Penhall Gardens



The Emerging Proposals

Our plan for the regeneration of this site conforms to the aspirations of the Council for the Charlton Riverside Masterplan by hitting the following requirements:

Our proposals	Contribution to the Charlton Riverside Masterplan
<ul style="list-style-type: none"> c.230 new homes in a range of sizes, including 1, 2 and 3 bedroom homes in two buildings ranging up to 10 storeys in height 	<ul style="list-style-type: none"> Contributes c.230 new homes to the 5,000 - 7,500 new homes planned for masterplan area
<ul style="list-style-type: none"> The provision of 35% affordable housing, available for both affordable rent and shared ownership 	<ul style="list-style-type: none"> Hit the affordable housing targets set by Greenwich Council for new development
<ul style="list-style-type: none"> Ground floor provision for commercial space 	<ul style="list-style-type: none"> Integration of employment uses into all new development and contribute to the 4,400 new jobs in the Charlton Riverside Masterplan
<ul style="list-style-type: none"> 30 car parking spaces alongside 14 wheelchair accessible spaces 	<ul style="list-style-type: none"> Conform to the Council's parking standards and GLA standards for wheelchair accessible parking spaces
<ul style="list-style-type: none"> Provision for play space for young children 	<ul style="list-style-type: none"> Realise the Charlton Riverside Masterplan vision for children to play outside with residents and visitors enjoying a varied and attractive selection of leisure, recreation and social activities

Envisioned CG of our built and emerging proposals



Our Vision

The proposals have been designed to showcase the highest quality in architecture, and will reflect the Council's aspirations to improve accessibility and the public realm in the Charlton Riverside area.

Our plans will deliver around 230 new homes, delivering much needed new housing in a range of sizes and tenures. This reflects the Masterplan's guidance to deliver housing options for a diverse range of groups, including first time buyers and young families.

We will provide 35% affordable housing, which is the expected percentage by the Council for this development, split between affordable rented and shared ownership tenures. 30 car parking spaces will be provided, in line with the Council's parking guidelines.

The homes will be designed to be highly sustainable and energy efficient, with a number of homes specifically designed for wheelchair access.

The design will be to the highest quality, and the new green spaces will provide a welcoming and attractive places for residents to live, work and play.



The Charlton Riverside Masterplan

Aitch Group's site is located within the Charlton Riverside Masterplan area, and the Council's clear vision for this area in the coming years includes the delivery of:

<ul style="list-style-type: none"> 5,000 - 7,500 new homes 	<ul style="list-style-type: none"> 4,400 new jobs
<ul style="list-style-type: none"> The delivery of family housing and 35% affordable homes 	<ul style="list-style-type: none"> An integration of employment uses into new developments
<ul style="list-style-type: none"> A range of building heights, up to 10 storeys 	<ul style="list-style-type: none"> Provision for and enhancement of new local housing assets
<ul style="list-style-type: none"> A creative design response to the need for flood protection 	<ul style="list-style-type: none"> New transport routes that reflect the area's historic layout

There will be a number of other opportunities which will be explored through the masterplan and delivered in the coming years. These will be explored in the next stage of the process.

Aitch Group's team is working with the Council and other stakeholders to ensure that the proposals for the development are in line with the Masterplan vision and to ensure that the site is integrated into the wider area.



^ Extracts from the online public consultation

4.4 PUBLIC CONSULTATIONS

Virtual

In order to engage local residents and stakeholders in the light of on-going COVID-19 restrictions, Lexington Communications adapted a physical public exhibition in to a 'virtual' one that enabled local people to view and comment on the initial proposals for 33-81 Eastmoor Street and 6-10 Westmoor Street.

This virtual public exhibition went 'live' on 5th May 2020, with residents alerted to this online consultation via a letter being hand delivered to over 3,000 local homes and businesses in a 700m radius of the Eastmoor Street site. This was supplemented by emails, letters and submissions via the online comment forms of previously identified stakeholders. The circa. 41,000 local residents in three local postcodes covering the local area were also targeted with Facebook adverts, with those who clicked on the adverts taken directly to the consultation website. Separately to our own consultation work, the consultation was also publicised by a local blog that is widely read by people living in Charlton, called 'Charlton Champion'.

Our Virtual Public Exhibition

The website was set up to provide a virtual exhibition experience, with a depiction of a physical exhibition set-up on the website, alongside graphics representing what residents

would see. Visitors could access each element of the exhibition by clicking on hotspots on the site. These hotspots included:

- A set of consultation banners detailing the proposal
- A range of indicative scheme CGIs
- A fly through
- A download page where visitors could access:

The consultation banners

The Council's supplementary planning document Links to Aitch Group's corporate website and development brochure. Contact details for any stakeholder that wished to discuss the site in further detail with a member of the project team. A feedback form so visitors could comments on the proposals.

The Feedback – Headlines

As of 22nd May, the project team had received 9 feedback responses, with 7 feedback forms from the website and two ad hoc correspondences. All of these have been incorporated into this feedback report. In responding to the 7 questions given to visitors, the most numerous comment theme was support for the scheme. Respondents believed that the proposals offer a number of opportunities including: new housing, commercial opportunities for small businesses and green space.



4.5 PREVIOUS APPLICATION

Ref: 20/1924/F

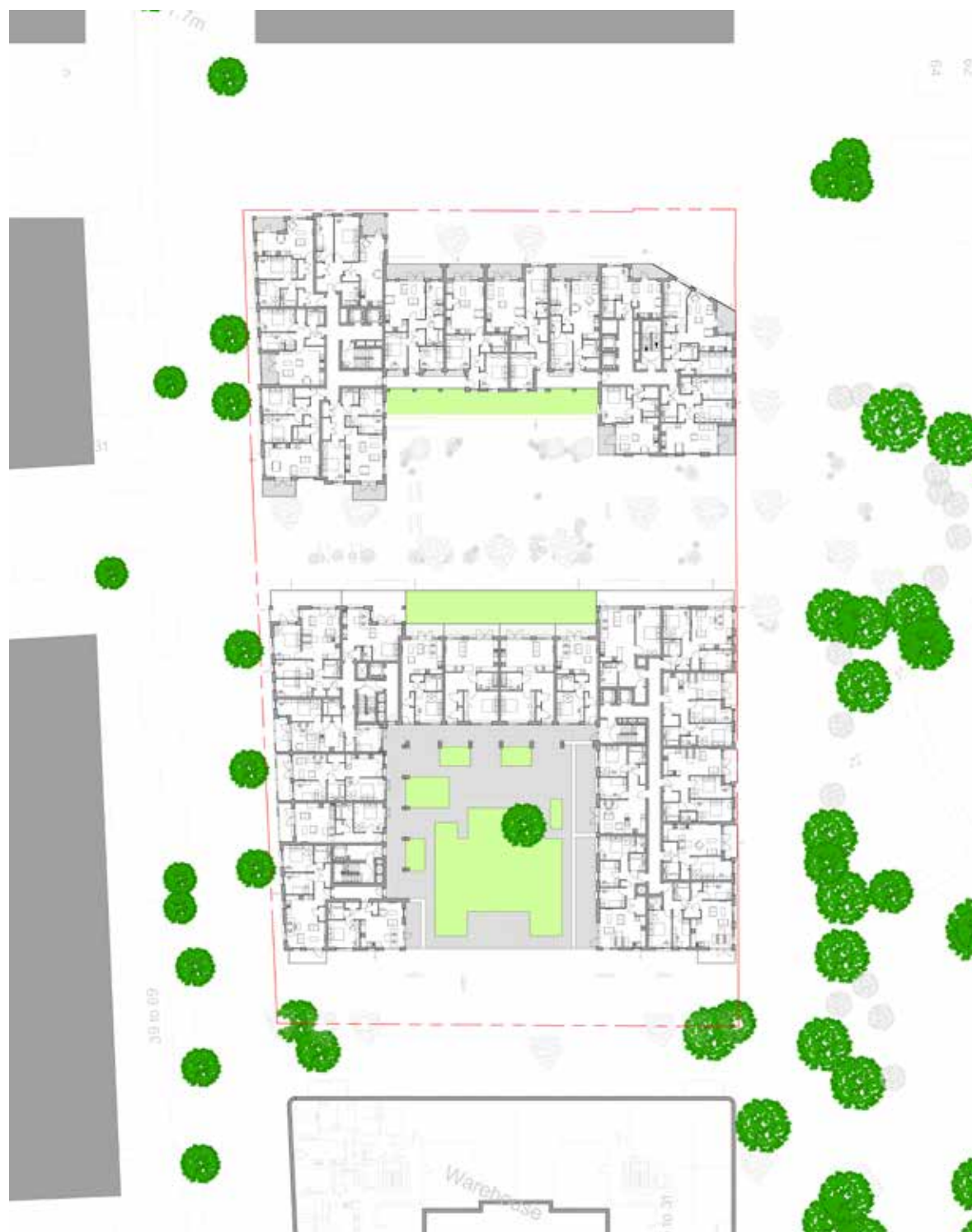
This scheme comprised of two main blocks of 6 to 10 storeys, totalling 192 residential units, 631 sqm B1/B8 flexible employment floorspace and 510 sqm flexible retail and community uses. (Use Classes A1-A5 and D1).

The percentage of affordable housing stood at 35.4% with a split of 80% rented and 20% shared ownership.

RBG planning department recommended that full planning permission be Granted for this application and that the development was considered acceptable.

During the application process, the design team worked in close consultation with the Royal Borough of Greenwich and its design officer which resulted in substantial changes to the scheme

The committee meeting voted against application despite the recommendation of its own planners, referring to the SPD and the discrepancy in height and variation.



^ Proposed first floor plan

The scheme was composed of two main blocks, a North and a South, that was split by 2 green links.

The scheme had no symptoms of over development, and with the use of deck access, achieved a high % of dual aspect flats (77%).

The proposed palette of materials was a light buff brick, a darker grey brick and a glazed brick on ground floor to frame the entrance into the cores and commercial units.

The tallest element of the proposal was the North Wets Tower, which faced Mirfield Street. This way finder was strategically placed to draw attention to the local hub as proposed by the SPD and break up the roof profile of the scheme.

The South block at 8 storeys was more subservient to the North block. This block featured characteristic saw tooth roofs which topped a two storey set back pop up, referencing the industrial heritage of the area.



^ Proposed north elevation

V Revised visualisations , view from Eastmoor street looking South



^ Revised visualisations , view from Moore Park

Submission amendments

Post submission, the design team worked closely with the RBG to ensure the scheme matched the Council's expectations for the site.

The major amendment to the scheme was a reduction in height:

- The South block reduced by a single storey
- The North block reduced by a single storey

As a result of these changes, the scheme reduced from 192 to 188 units.

Metal cladding was also introduced on the facade and the balcony areas to reference the

local industrial heritage. The saw tooth roof was also removed to reduce height.

As a result of these changes, the scheme totalled 188 units (reduction of 4) with a proposed level of affordable housing at 26.6%.



^ Revised visualisations , view from Mirfield street

V Proposed roof option 01



^ Proposed roof option 02

4.6 REVISED SCHEME DEVELOPMENT

Roof Options

Post refusal, we amended the scheme to reflect the comments from the objectors.

The Southern block was reduced to 5 storeys, with a 3 storey link in the middle. The Northern block was reduced to 5 storeys with the tower element reduced to 7. The scheme now complies with the height requirements of the SPD with the exception of the tower at 7 storeys, however this follows the agreement from RBG that in this area the provision of a local hub allows higher elements to act as a way finder for the area.

Option 01 involved a pitch roof to the South block with a industrial chimney form.

Option 02 involved typical pitched roofs with the gable ends facing Moore park, a alternative option of this was to have saw tooth pitches.

Option 03 was a double pitch with the gable ends facing the green link.

These options were not favoured due to the increase in height and the flat roof option was agreed to move forward for the resubmission.

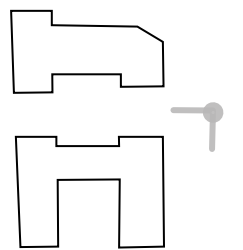


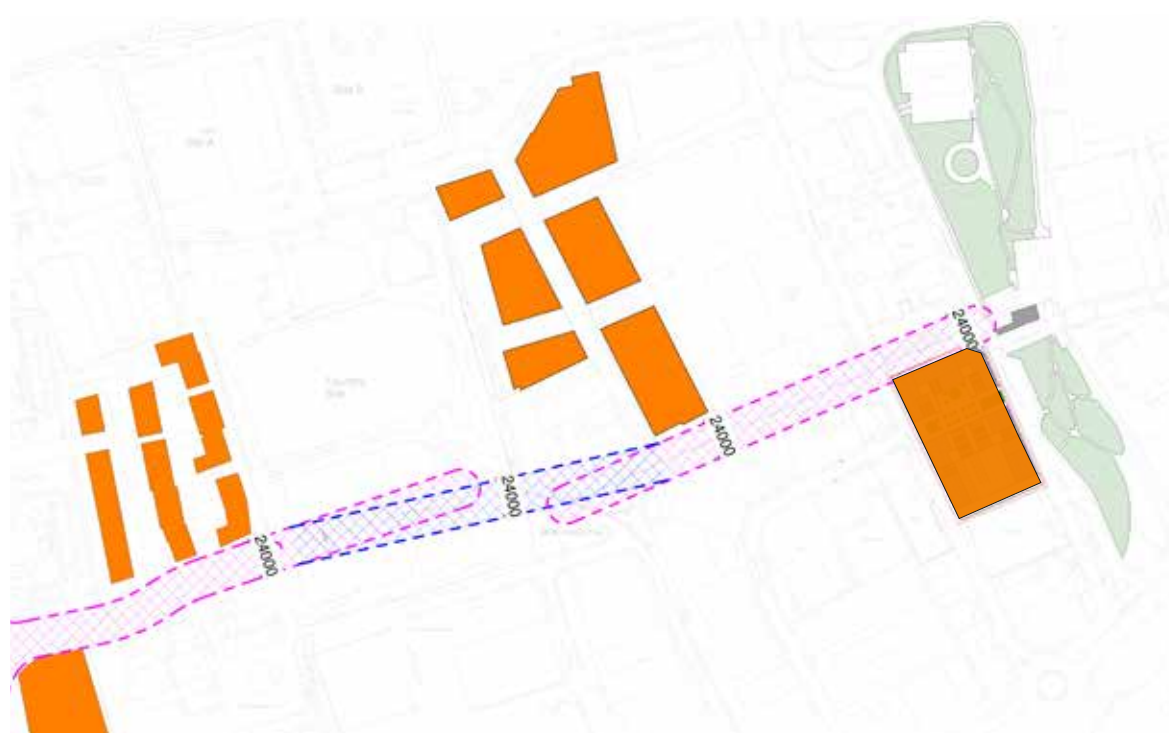
^ Proposed roof option 03

5.

DESIGN PROPOSAL

- 5.1 Layout
- 5.2 Scale
- 5.3 Massing Reduction
- 5.4 Schedule & Tenure
- 5.5 Plans
- 5.6 Materials
- 5.7 Elevations
- 5.8 Residential Entrances
- 5.9 Parking, access & servicing





- ^ Development of road layout

5.1 LAYOUT

East - West Route

Layout - Mirfield Street: East - West link

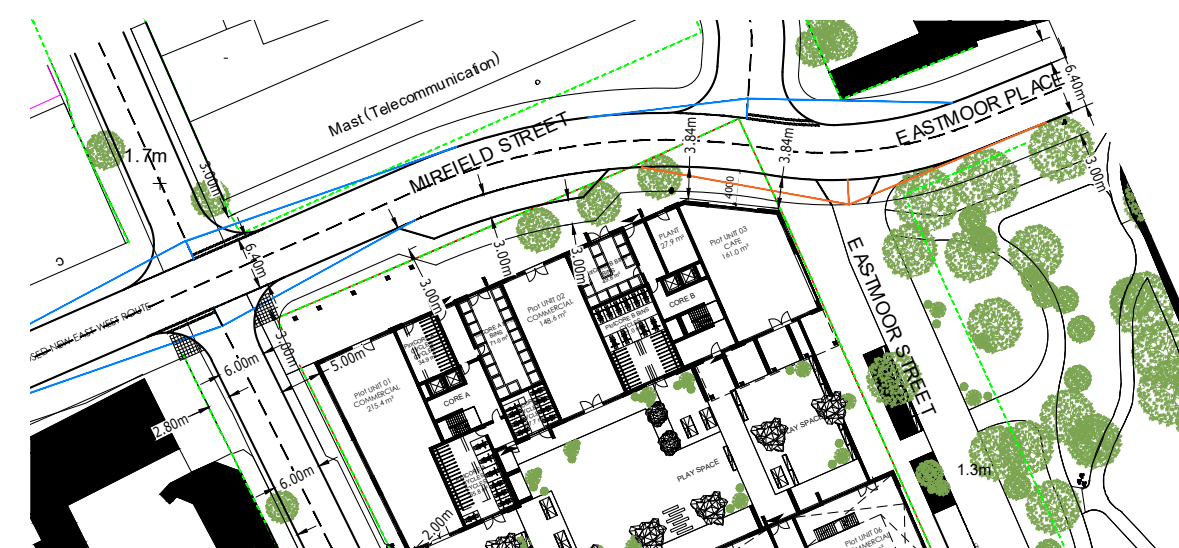
The Charlton Riverside SPD proposes a new East-West road link through the area. As discussed previously, several options for the road have been discussed with adjoining landowners, the RBG and TfL. The key design decisions have centred around :

The width of the road - whether the road will solely be a bus and servicing route?

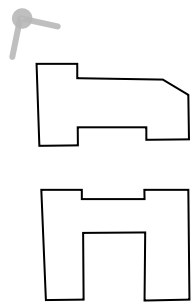
The position of the locally listed public house and its relationship to the road alignment.

The position of the road in relation to sites to the west namely, The VIP Trading Estate (Rockwell) and Stone Foundries (Montreaux)

Please see the adjoining images detailing the impact of the issues listed above. The preferred route has been agreed with the RBG and TfL. The route has a significant impact on the buildable site area. The buildings on the site have been set back to deliver the east-west link. The pavement width has been increased to 4m and a loading bay has been provided to safely service the commercial units of the Mirfield Street block. The remaining area will be landscaped with street trees and furniture to enhance the environment of the local hub.



^ East-west link proposed layout - Ardent



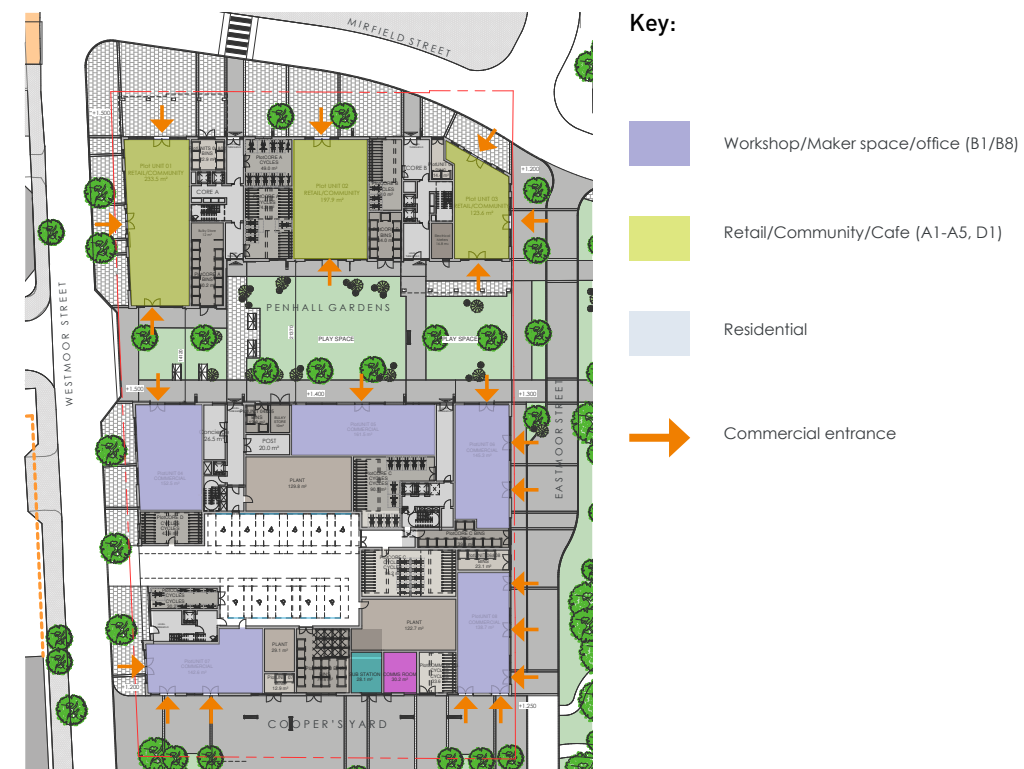


^ Ground Floor Isometric

Site Circulation

Use: The proposal has been designed to enable the sensitive co-location of residential and commercial uses on the site. The co-location of commercial workspace and residential units will help to create an active community on the site throughout the day. The type of commercial spaces offered, workspaces, retail, a cafe, provide the opportunity for residents to work, shop and relax close to home. The uses proposed also support the SPD's provision for a local centre

on the site. A variety of residential typologies are offered to attract a diverse new community including young families and professionals. A total of 149 homes have been created with 10% being affordable housing. Residential entrances on the Mirfield Street block are located on Mirfield Street linking through to Penhall Gardens. In the southern block, residential entrances are located along Penhall Gardens and also Westmoor Street. The residential entrances ensure a strong

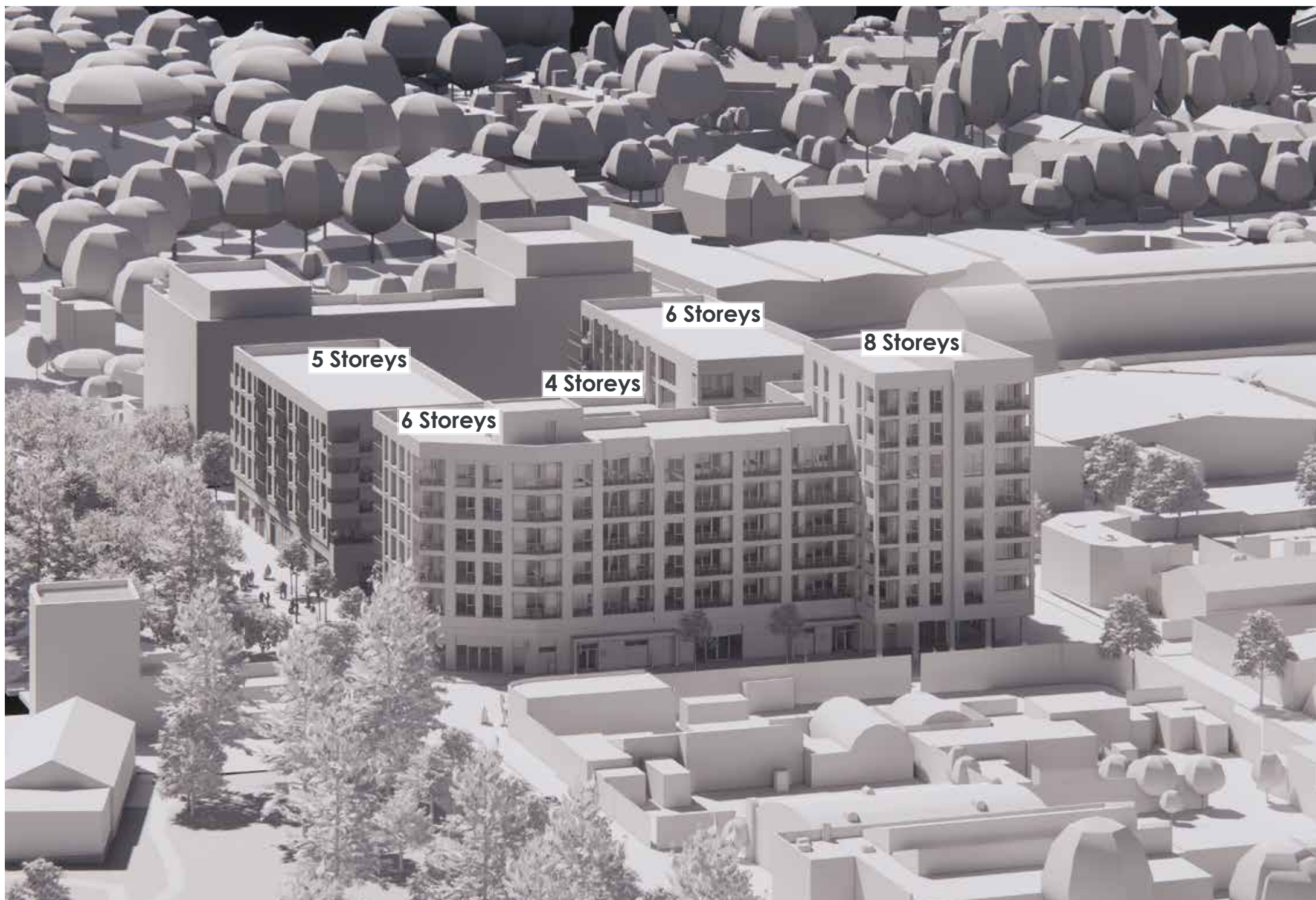


^ Ground floor plan showing commercial uses

connection to the public realm and shared garden space of the green link. Residential cores also link to the public realm provided at first floor within the southern building. Further amenity is provided via roof terraces ensuring all residents have access to communal amenity. Private amenity is provided for each home via either a balcony or roof terrace.

At ground floor a total of 1,820 m² of flexible, commercial space is proposed. Due to flood risk, residential accommodation cannot be provided at ground floor. The Mirfield Street building has the

potential to provide a cafe/community use at the prominent intersection of Mirfield Street and Eastmoor Street which, being dual aspect, can also overlook the quieter gardens of the green link. Two further units provide the opportunity to create retail and workspace within this building. A small, local convenience store will provide for the day- to -day shopping needs of new residents. In the southern building a variety of sizes of commercial units have been provided with the potential to create mezzanine level office space.



^ Proposed isometric

5.2 SCALE

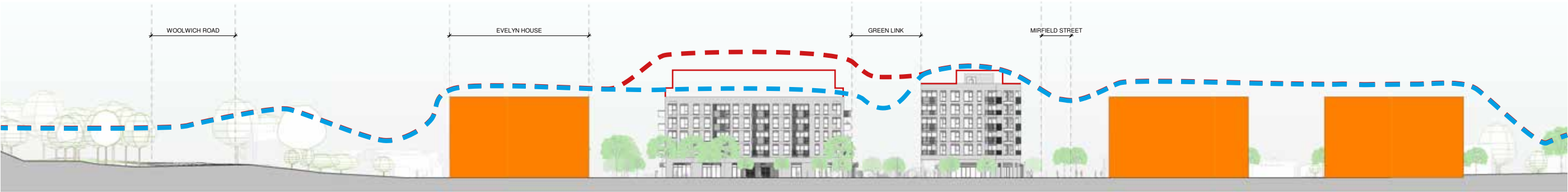
The massing has been developed to respond to the emerging context of the Charlton Riverside Area and the SPD. The building heights have been reduced as part of this iterative process and the previous refusal at planning. The proposed heights have been assessed to ensure amenity spaces, the green link and the courtyard space, receive adequate daylight and sunlight. The massing of the Mirfield Street building creates a defined taller block on the intersection of Mirfield Street and Westmoor Street, a key focal point along Mirfield Street. The massing lowers from 8 storeys to 4storeys across the site.

There are elements of a single storey along the pedestrian links.

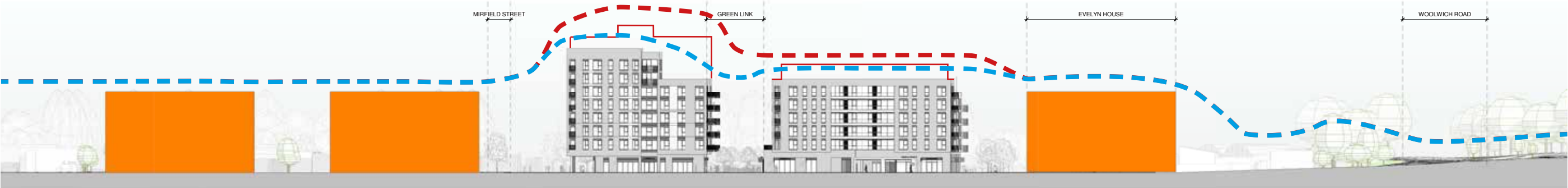
The southern courtyard building sits a minimum of 15 metres from the Mirfield Street building increasing to 30 metres across the green link. A large south facing courtyard, 23.5 metres wide is formed at first floor level. The landscaping of the courtyard creates an intimate, relaxing feel in contrast to the external city and river views from the homes.

The taller element at the north-west corner rises to 8 storeys, acting as a wayfinder for the wider area.

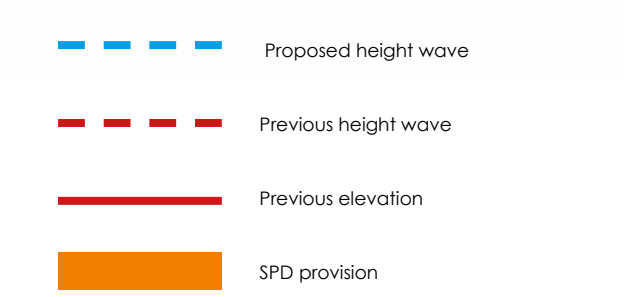
5.3 MASSING REDUCTION



^ Eastmoor Street elevation



^ Westmoor Street elevation



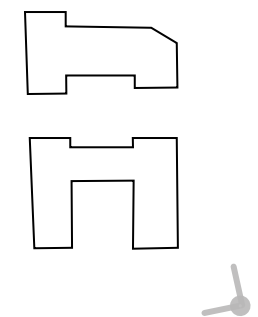


^ View looking north west from Moore Park

Massing Reduction - Views

Following concerns from the local residents on the impact of the proposal on Moore Park, the massing on the Eastern elevation has been substantially reduced by 3 storeys on the southern block and a single storey on the northern block.

The existing foilage mostly covers the massing on Eastmoor Street from view. This revised massing reflects the aspirations of the SPD for the creation of human scale.



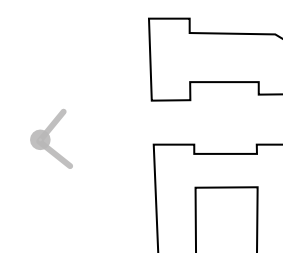
Current massing outline

Previous massing



^ View looking east from Penhall Road

Looking east along Penhall road the revised massing responds to concerns from residents and committee members of the height of the northern block. In the amended proposal, the 8 storey tower drops down to 6 storeys, a substantial reduction from the previous 10 storey tower.



Current massing outline

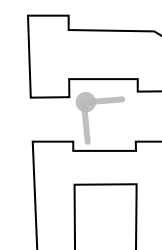
Previous massing



^ View looking east from the green link

The reduction in massing from the previous scheme is most evident in the green link.

The subtracted height creates a human scale sought after by the SPD, with the new mass reminiscent of a London suburban street between 1 - 5 storeys. The lower massing to the south allows higher levels of daylight in which will increase usage and plant growth in the space.



Current massing outline

Previous massing

OVERALL SCHEDULE

UNIT TYPE	AREA	NUMBER	%
Studio	37m²	9	6
1B2P	50m²	30	20
1B2P WCH	65m²	10	7
2B3P	61m²	31	20.2
2B3P WCH	75m²	5	3.4
2B4P	70m²	49	33
3B4P	74m²	14	9.4
3B5P	86m²	1	1
Total:		149 homes	100%

WHEELCHAIR SCHEDULE

UNIT TYPE	AREA	NUMBER	%
1B2P WCH	65m²	10	7
2B3P WCH	75m²	5	3.4
Total		15	10

TENURE SCHEDULE

TENURE TYPE	1B	2B	3B	NUMBER	%
LAR	0	9	2	11	7.4
S/O	0	2	2	4	2.7
Total				15	10.1

5.4 SCHEDULE & TENURE

Residential Tenure

- London Affordable Rent accommodation
- Shared Ownership accommodation
- Private accommodation
- Wheelchair adaptable accommodation

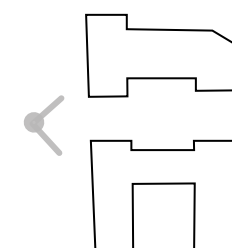
The proposal provides a total number of 149 new homes, 10% of which are affordable. The affordable housing is then split 73% affordable rent / 27% shared ownership. Affordable rent accommodation is located in core A of the northern, Mirfield Street block. Wheelchair adaptable flats are located in each block, with access to two lifts per core. The number of homes accessible per floor from each core does not exceed 8no.

The site building layout has been designed to maximise the number of dual aspect flats. All three bedroom dwellings are located on corner plots, where they can enjoy views and daylight via two aspects or more.

The emerging London Plan, London Housing Design Guide and Building Regulations have been followed in the design of the residential layouts and individual flat layouts. Other site specific factors have also influenced the design such as : daylight, noise and air pollution.

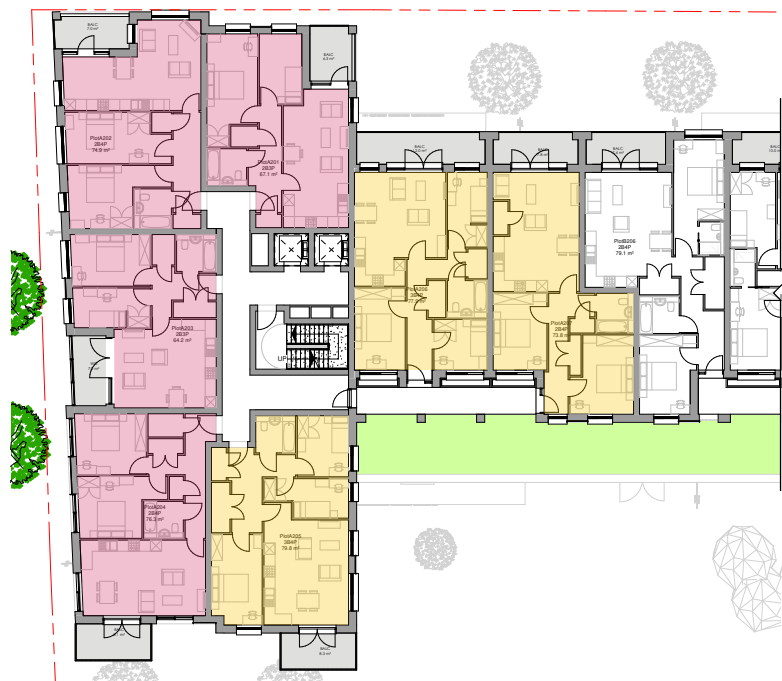
Occupancies across the site range from one bedroom homes to three bedroom duplexes. Dwellings have been designed using the London Housing Design Guide, in line with the emerging London Plan, and Part M of the Building Regulations.

Each core has access to communal amenity - either a roof terrace with extensive views or the landscaped courtyard in the southern block. All homes have their own private amenity in the form of a balcony or terrace.

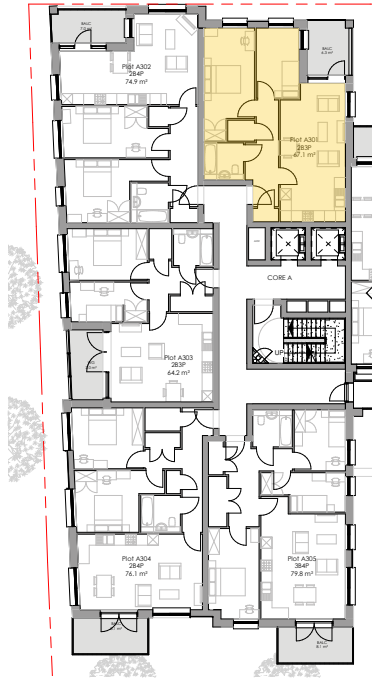




^ Proposed first floor plan - Core A



^ Proposed second floor plan - Core A



^ Proposed third floor plan - Core A

Tenure Allocation

The scheme provides 10% affordable provision which is located on the first, second and third floor on Core A.

This is split 73 / 27 between London affordable rent and shared ownership.

The scheme is tenure blind, with the affordable units having full access to the amenity areas on the North block.

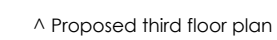
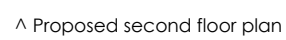
The affordable homes are a mixture of 2 Bed and 3 Bed flats.

[illegible]

88 | ACA | EASTMOOR STREET | DESIGN & ACCESS STATEMENT



ACA | DESIGN PROPOSAL | 89

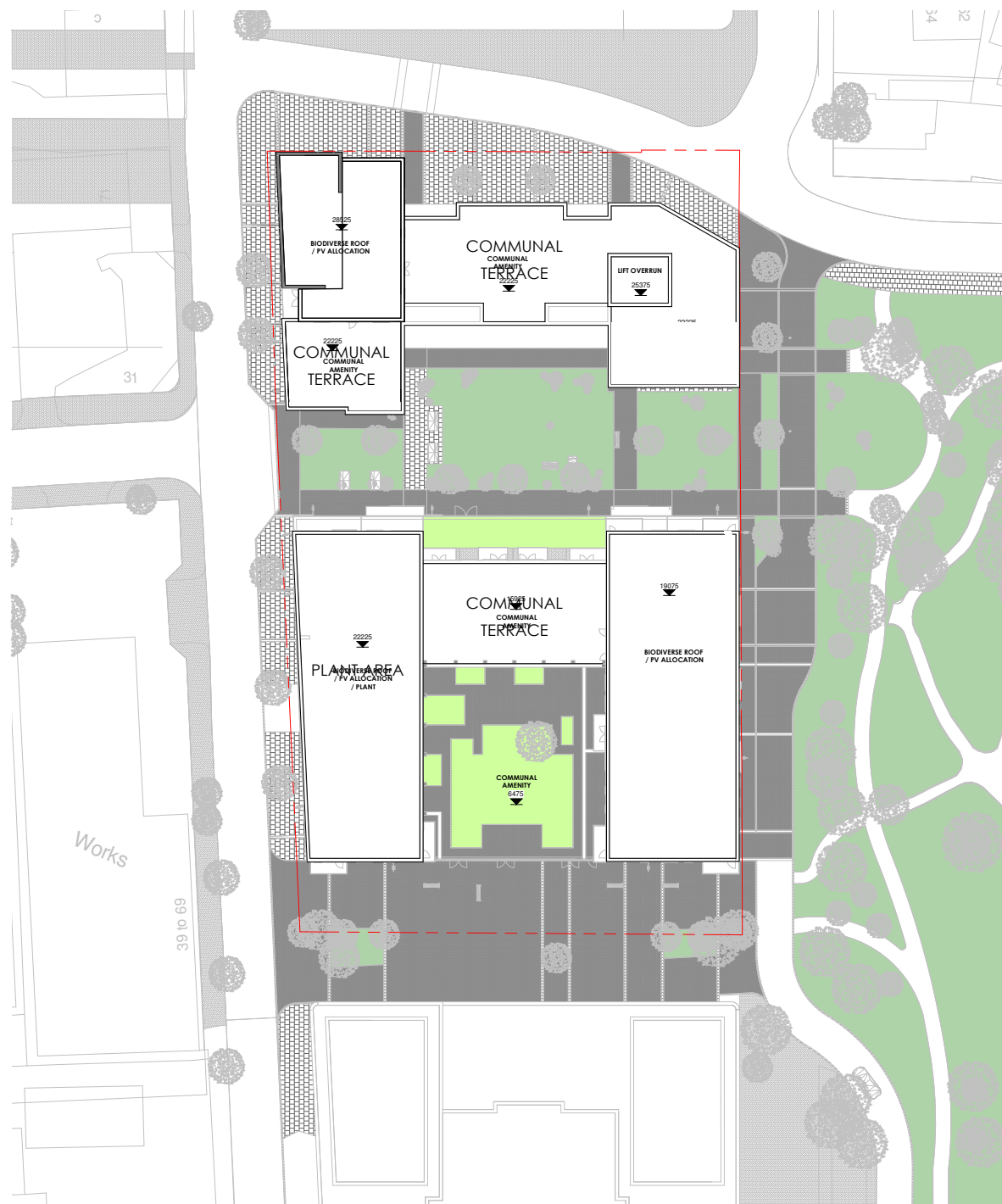




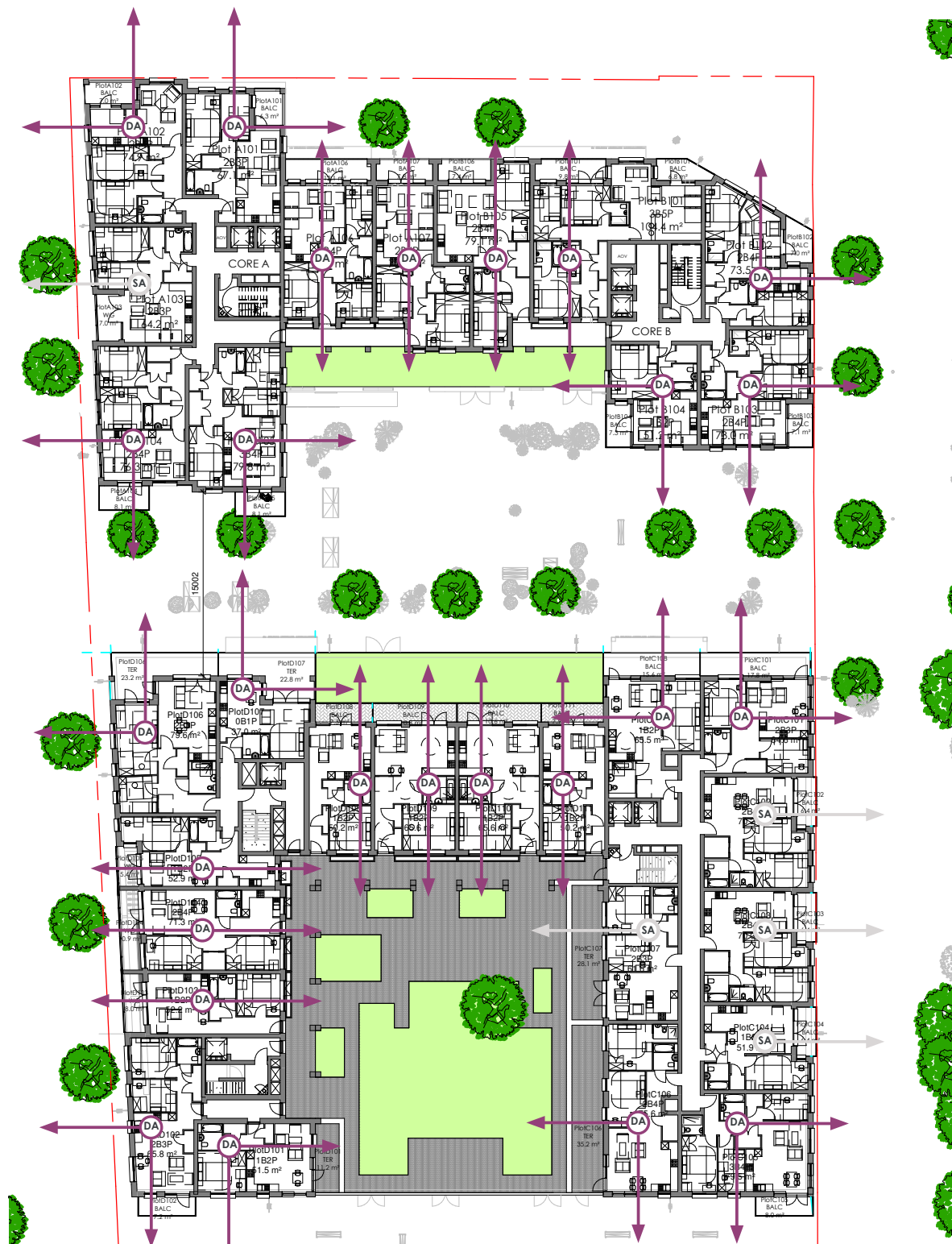
^ Proposed fourth floor plan



^ Proposed fifth floor plan



^ Proposed roof plan



^ Proposed first floor plan, showing dual aspect

Dual Aspect

Residential dwelling orientation

The building layout proposed ensures that flats are positioned to take advantage of south, east and west orientation. This maximises the amount of daylight and sunlight within the flats for residents.

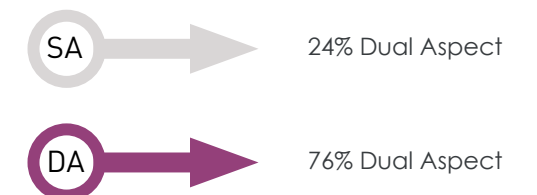
Dual aspect

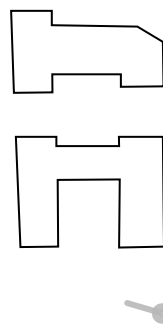
A total of 149 homes, 76% of units, are dual aspect, meaning they benefit from the ability to cross ventilate and achieve a greater range of sunlight/daylight. These flats are a range of one, two and three bedroom homes in the form of flats and two storey duplexes.

The Mirfield Street building has been designed to optimise views to the river, the adjoining Moore Park and Gilbert's Pit/Maryon Park to the south. The building has been planned to provide as many dual aspect dwellings as possible and ensure there are no north facing, single aspect homes. The building utilises a deck access colonnade which also provides natural surveillance to the adjoining Penhall Gardens and helps to foster a sense of community via social interactions. The dual aspect homes benefit from private terraces with river views. Duplexes are provided on upper, set-back floors with living, kitchen and dining areas located on the upper floor to enjoy extensive views to the north and south. Larger

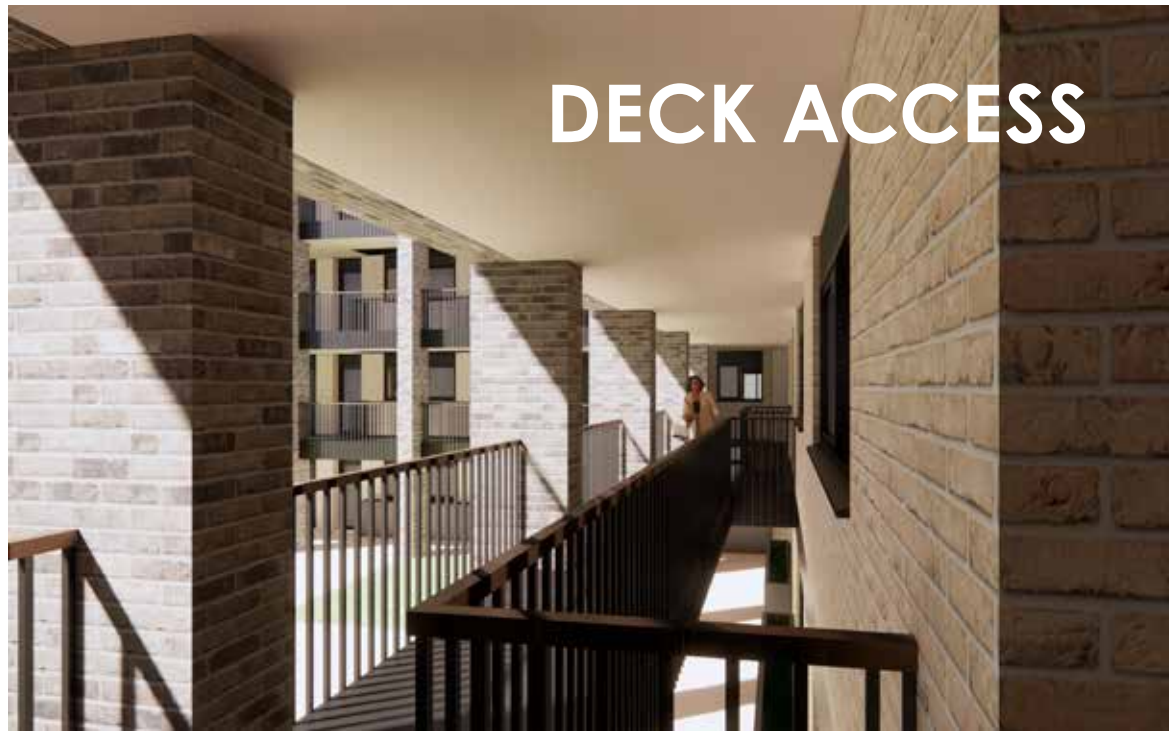
family units also have corner aspect to benefit from dual orientation.

In the southern courtyard block, apartments are accessed off the central courtyard, creating an activated social space. Landscaping elements have been introduced to create subtle transitions from the communal, public courtyard to private dwellings. These thresholds are also reflected in the facade, which uses depth to create lobbies to each home. The sheltered colonnades also provide access to communal lifts and stairs ensuring natural daylight and ventilation to these spaces. The colonnades allow for social interactions as residents enter and leave their homes. The colonnade also provides natural surveillance to the landscaped courtyard. The deck access typology ensures the majority of dwelling are dual aspect.





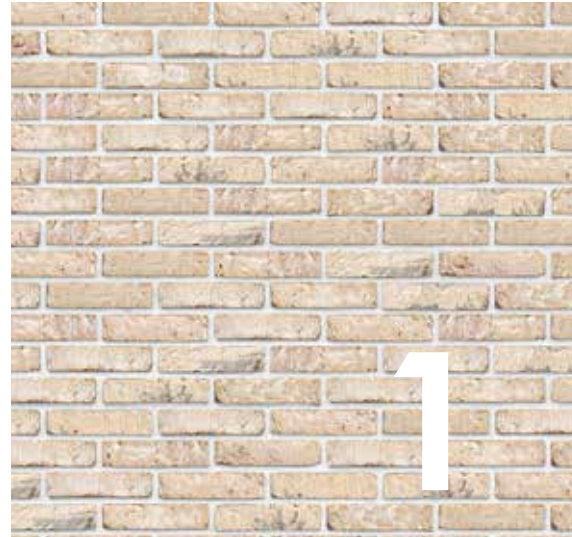
V View, deck access South block



^ View, deck access North block



^ Proposed second floor plan South block showing deck access and dual aspect



^ 1. Light buff brick



^ 1. Grey brick



^ 3. Blue glazed brick



^ 4. Metal panel

5.6 MATERIALS

The palette of materials has been developed in response to the surrounding industrial heritage. The predominant building material proposed is brickwork in two complimentary tones and glazed bricks for use within the commercial frontages.

1. Light brickwork
2. Grey brickwork
3. Blue glazed brickwork
4. Metal panels

The grey brickwork is used to form the commercial plinth to the buildings providing unity to the ground floor pedestrian areas. Blue glazed brickwork highlight residential and commercial entrances. The light brickwork highlights the residential accommodation. Upper set-back floors use the grey brickwork. The metal panels reference the industrial heritage whilst adding detail and depth to the elevation. Windows are proposed as dark black polyester powder coated aluminium double glazed units. Balustrades to balconies mixed between the dark black polyester power coating and the copper metal finish.

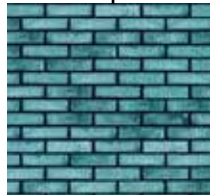
5.7 ELEVATIONS



^ Proposed elevation



^ Light buff brick



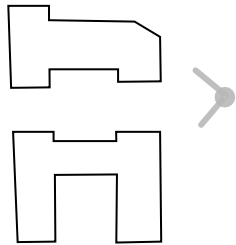
^ Blue glazed brick



^ Grey brick

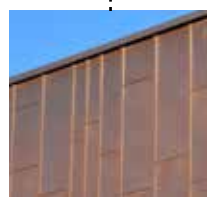


^ Metal cladding

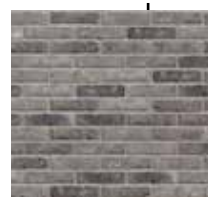




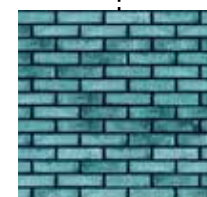
^ Proposed elevation



^ Metal cladding



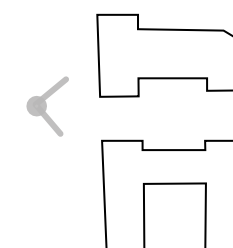
^ Grey brick



^ Blue glazed brick



^ Light buff brick

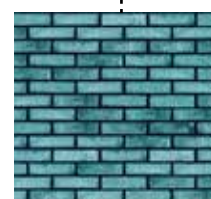




^ Proposed elevation



^ Light buff brick



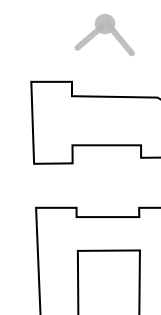
^ Blue glazed brick



^ Grey brick



^ Metal cladding

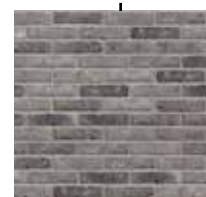




^ Proposed elevation



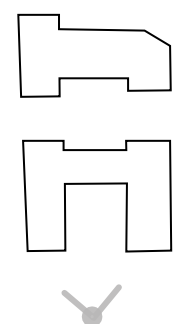
^ Light buff brick



^ Grey brick



^ Metal cladding



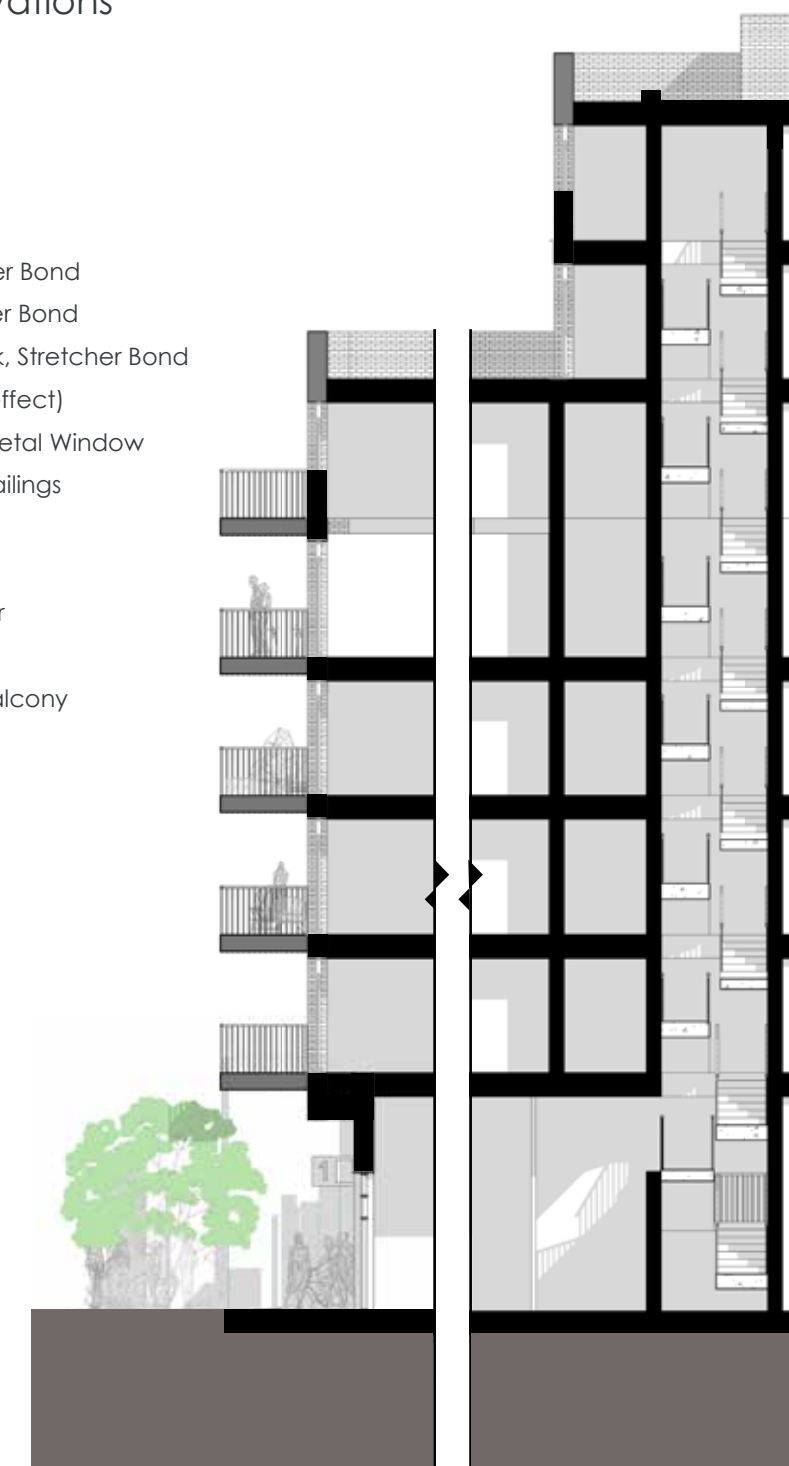
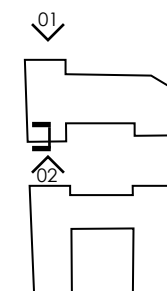


^ Proposed elevation

North block bay elevations

Materials Key

1. Grey Brickwork, Stretcher Bond
2. Light Brickwork, Stretcher Bond
3. Glazed Green Brickwork, Stretcher Bond
4. Metal Panels (Copper effect)
5. PPC Aluminium Black Metal Window
6. PPC Dark Grey Metal Railings (Copper handrail)
7. Dark Grey Metal Louvre
8. Dark Grey Louvred Door
9. Signage Zone
10. Solid 'bronze colour' balcony



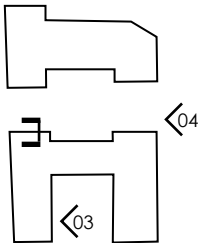
^ Proposed section

South block bay elevations



Materials Key

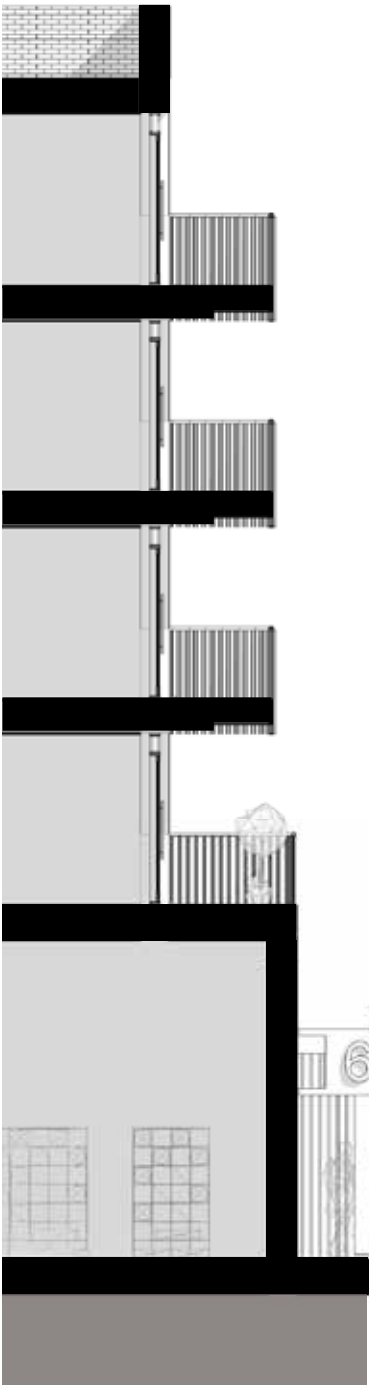
- 1. Grey Brickwork, Stretcher Bond
- 2. Light Brickwork, Stretcher Bond
- 3. Glazed Green Brickwork, Stretcher Bond
- 4. Metal Panels (Copper effect)
- 5. PPC Aluminium Black Metal Window
- 6. PPC Dark Grey Metal Railings (Copper handrail)
- 7. Dark Grey Metal Louvre
- 8. Dark Grey Louvred Door
- 9. Signage Zone
- 10. Solid 'bronze colour' balcony



^ Proposed South block courtyard bay elevation



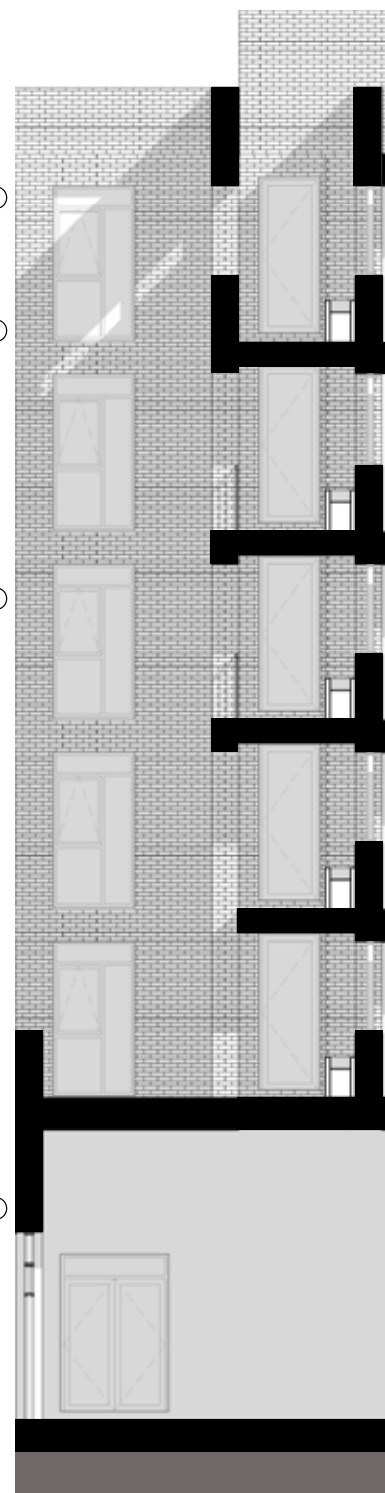
^ Proposed Eastmoor Street bay elevation



^ Proposed section



^ Proposed North block courtyard bay elevation

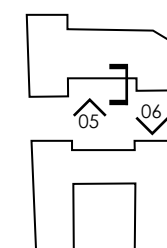


^ Proposed section

South block bay elevations

Materials Key

1. Grey Brickwork, Stretcher Bond
2. Light Brickwork, Stretcher Bond
3. Glazed Green Brickwork, Stretcher Bond
4. Metal Panels (Copper effect)
5. PPC Aluminium Black Metal Window
6. PPC Dark Grey Metal Railings (Copper handrail)
7. Dark Grey Metal Louvre
8. Dark Grey Louvred Door
9. Signage Zone
10. Solid 'bronze colour' balcony



^ Proposed South block courtyard bay elevation

V Proposed residential entrance Core C



5.8 RESIDENTIAL ENTRANCES

Residential entrances are highlighted by the use of a metal frame incorporating a canopy to protect residents from inclement weather whilst entering. Signage for each core is formed from cut out metal lettering mounted on the metal frame. Glazed brickwork is used to further highlight the entrances. Adequate lighting will be provided in the underside of the canopies to ensure safety for residents.



^ Proposed residential entrance Core B



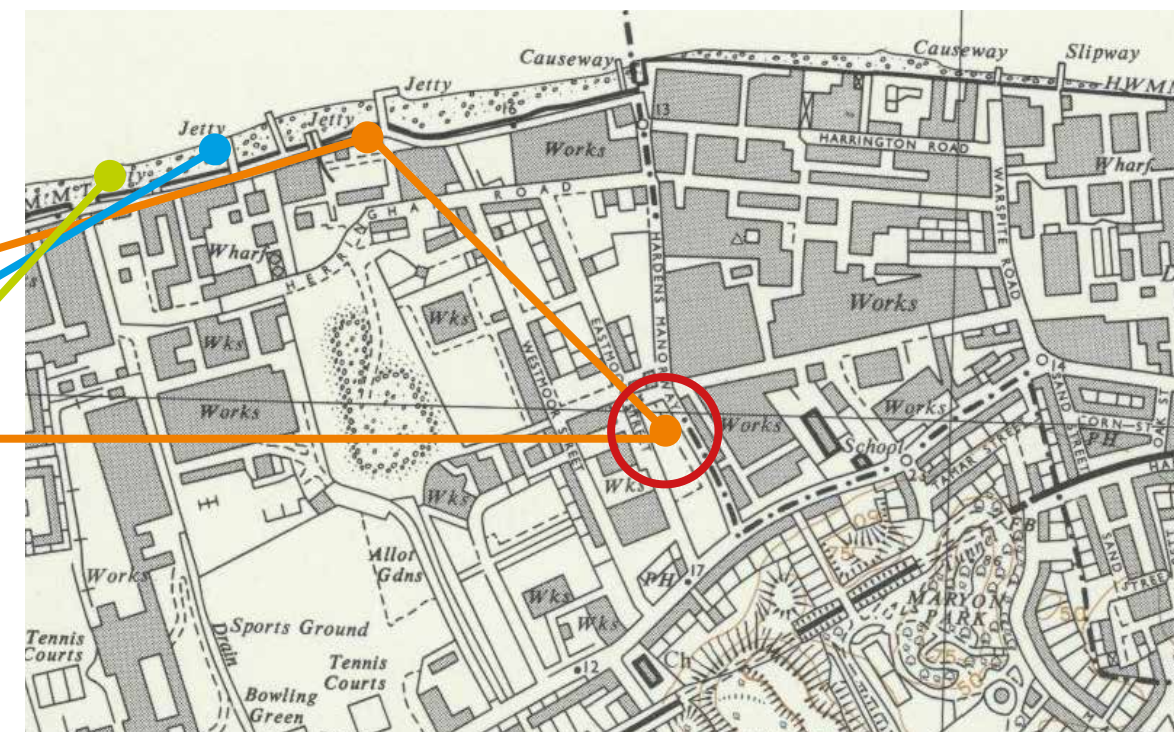
^ Proposed residential entrance Core D



^ Proposed residential entrance Core A



^ Proposed residential entrance Core C



^ Historical map 1950

Heritage and design

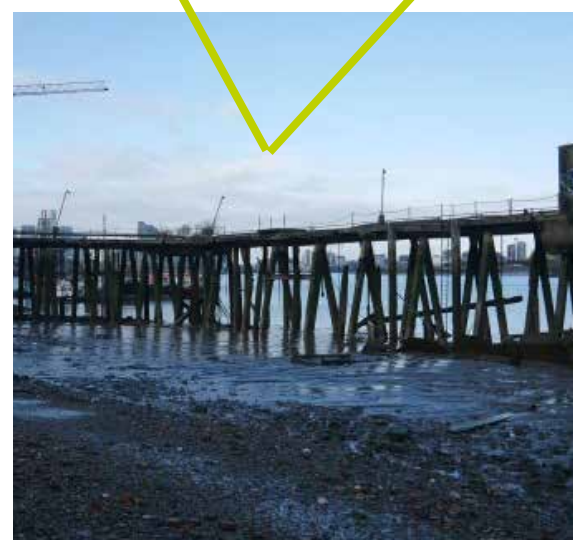
The residential entrances draw inspiration from the heritage of the Charlton riverside area.

The glazed blue/green bricks reference the early glass works by Johnsen and Jorgensen on the near Flint Glass Jetty and Thames Wharf north of site.

The dark metal surround references the industrial wharfs past and present, with the dark grey tone mimicking the colour of aged timber on the river.



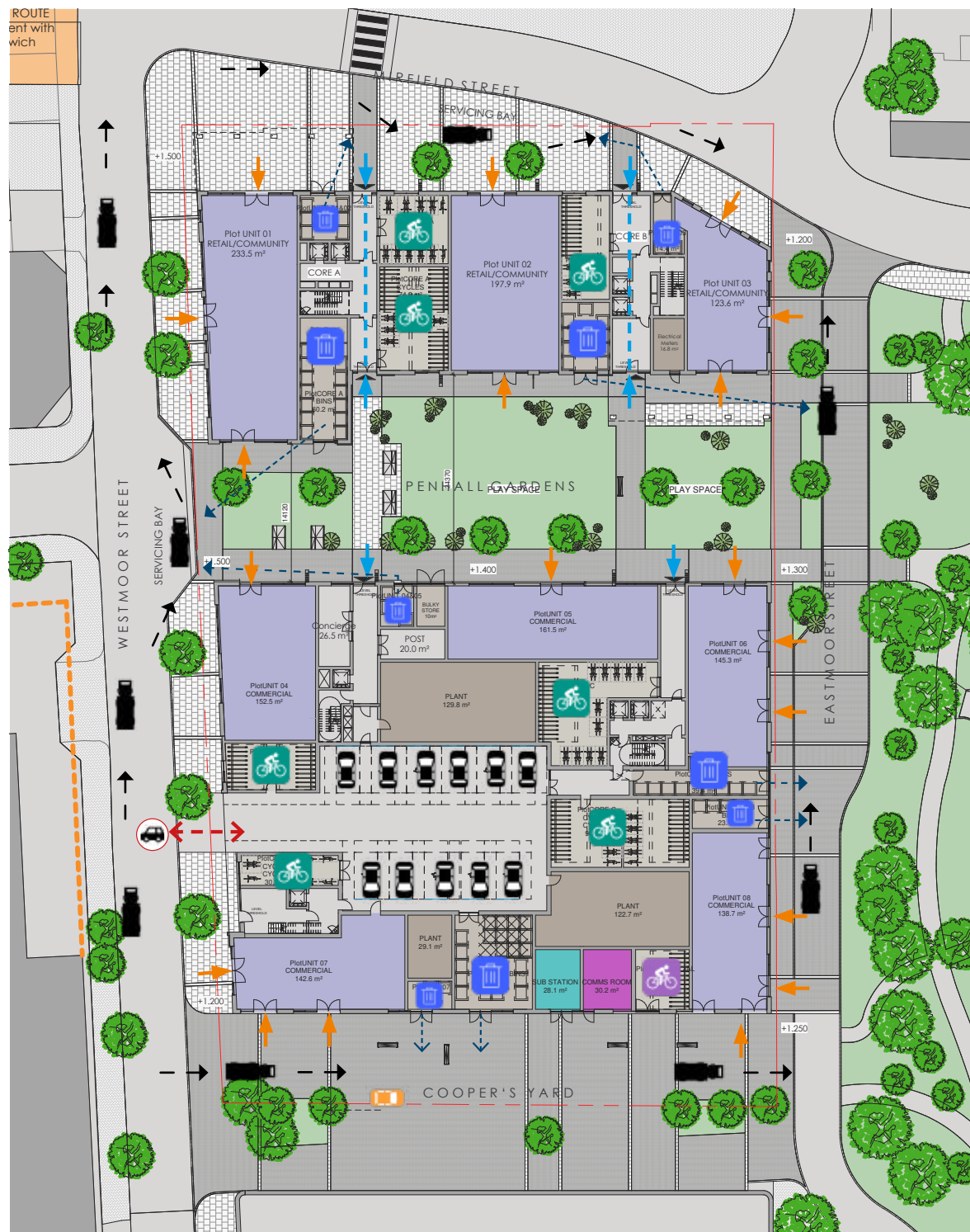
^ Riverside wharf. Source: Charlton Champion



^ Aged wharf timber Source: Charlton Champion







^ Proposed Ground Floor Plan

5.9 PARKING, ACCESS & SERVICING

Vehicular access & servicing

The layout of the site has been considered to ensure access for vehicles, pedestrians, cyclists, deliveries and servicing. The following areas have been considered:

- Access
- Car parking
- Car Club Spaces
- Long term Residential Cycle Parking
- Visitor Cycle Parking
- Commercial Cycle Parking
- Fire Vehicle Access
- Servicing
- Residential Post & Deliveries
- Site Refuse Strategy
- Refuse Vehicle Access
- Residential Refuse
- Commercial Refuse

Access

The residential entrances on the northern block have been designed to link through to the green link. This provides easy access for residents to the landscaped gardens. The residential entrances to the southern block are located directly opposite within the green link. This entrance arrangement will help to foster a sense of community, with residents stopping to chat whilst children play in the gardens. A further entrance is located on Westmoor Street, this core links on the upper floors to the entrance located on Penhall Gardens.

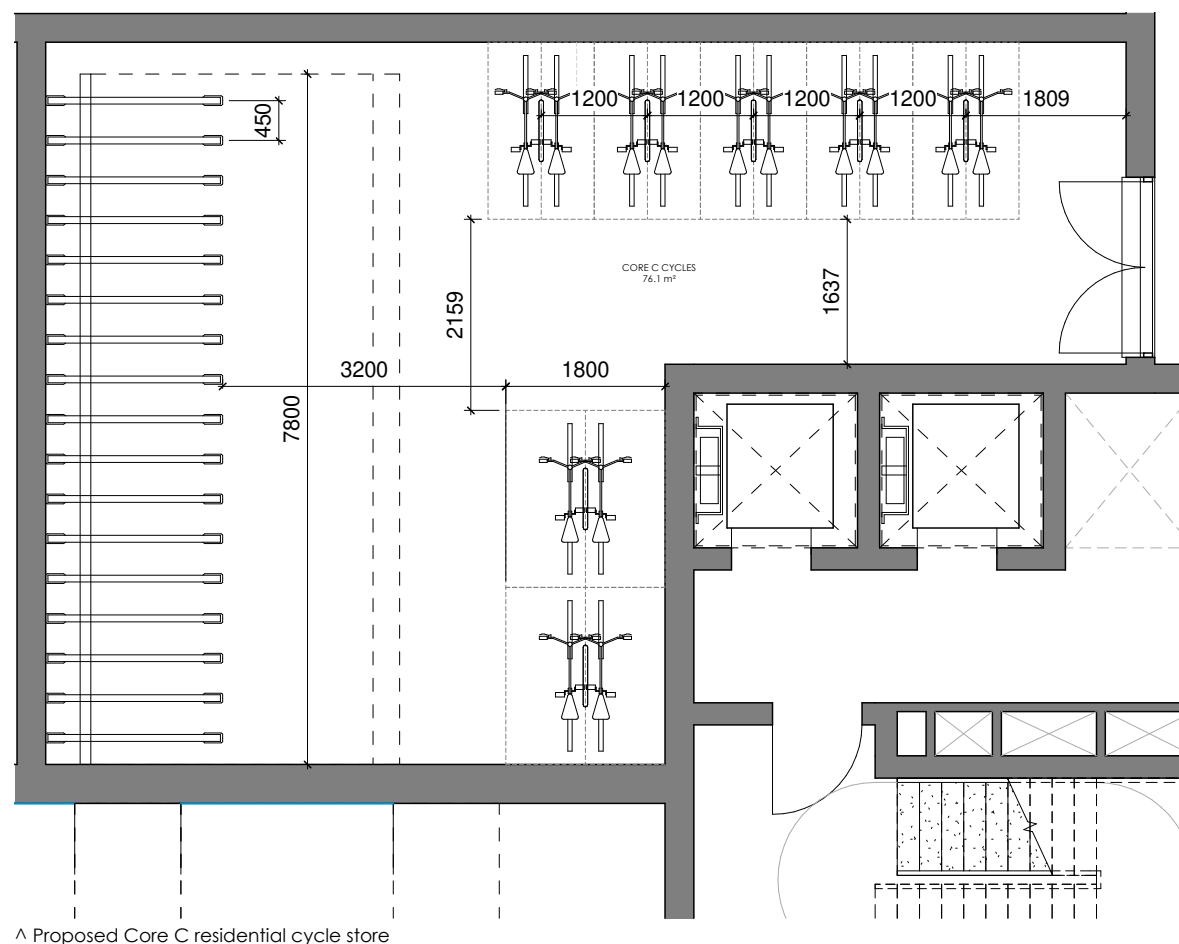
Car Parking

Covered car parking accessed via Westmoor Street is provided within the southern courtyard block. A total of 14no. blue badge managed car parking spaces are provided. From the outset, 20% of spaces will have 'active' charging facilities and the remaining spaces will have 'passive' provision which can be activated when the demand arises. This will be monitored through the Parking Design and Management Plan.

Car Club Space

A car club space will be provided in Coopers Yard. The location of the space means the vehicle is easily accessible to all local residents. Please refer to the full transport assessment carried out by Ardent submitted in support of the planning application.

- Key:
- Blue badge car parking
 - Car club parking bay
 - Residential cycle store
 - Commercial cycle store
 - Refuse store
 - Refuse vehicle path
 - Car park entrance & exit
 - Commercial entrance
 - Residential entrance



Cycle parking

Residential Cycle Parking

In line with the London Plan cycle spaces have been provided at a rate of:

- 1B 1P 1 space per unit
- 1B 2P 1.5 space per unit
- 2B + 2.0 spaces per unit

A total of 83no. spaces/ 25%, are provided in Sheffield stands spaced at 1.2m. A total of 14no spaces/ 5%, are provided on Sheffield stands spaced at 1.8m to accommodate larger cargo/ exceptional bikes. The remainder of spaces, 191 no. spaces / 70% are provided on Josta two tier bicycle stands. 5 no. residential spaces are provided for short stay/visitors. In line with Secured by Design principles, bicycle stores do not exceed 60no. spaces.

Commercial Cycle Parking

Commercial cycle parking for staff use will be provided within the commercial units and a central commercial cycle store accommodating 26. Visitor cycle parking is provided on Copper's Yard. A total of 34no. spaces are provided for retail and 13 space for commercial.

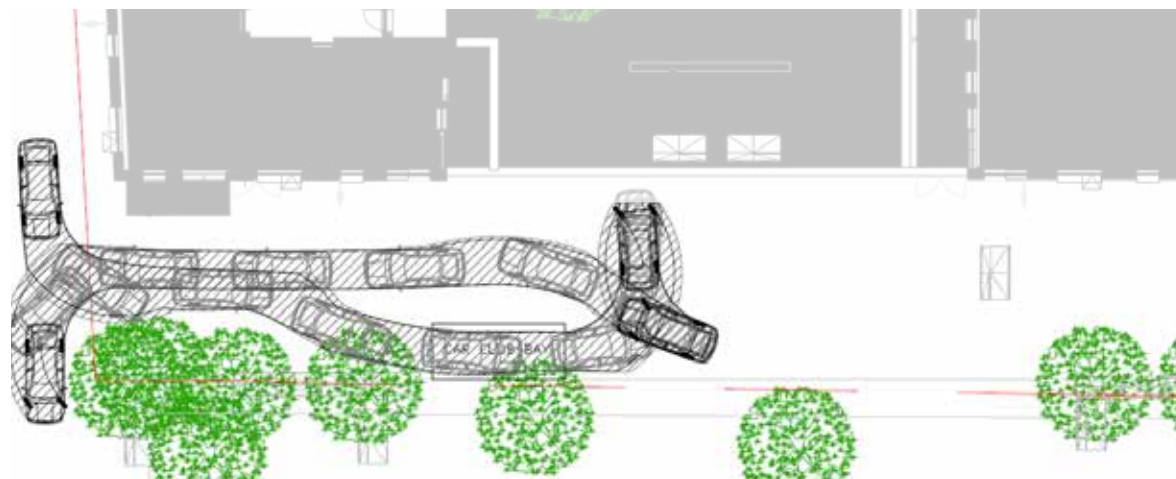
Please refer to the full transport assessment carried out by Ardent submitted in support of the planning application.

Residential Cycle Storage Provision	No.	%
Sheffield Stand 1200mm spacing	69	25.2
Sheffield Stand 1800mm spacing	14	5.1
Josta Two Tier Stand	191	69.7
Total	274	100

V Refuse turning circle Coopers Yard



^ Panel van turning circle Coopers Yard



^ Car club vehicle turning circle

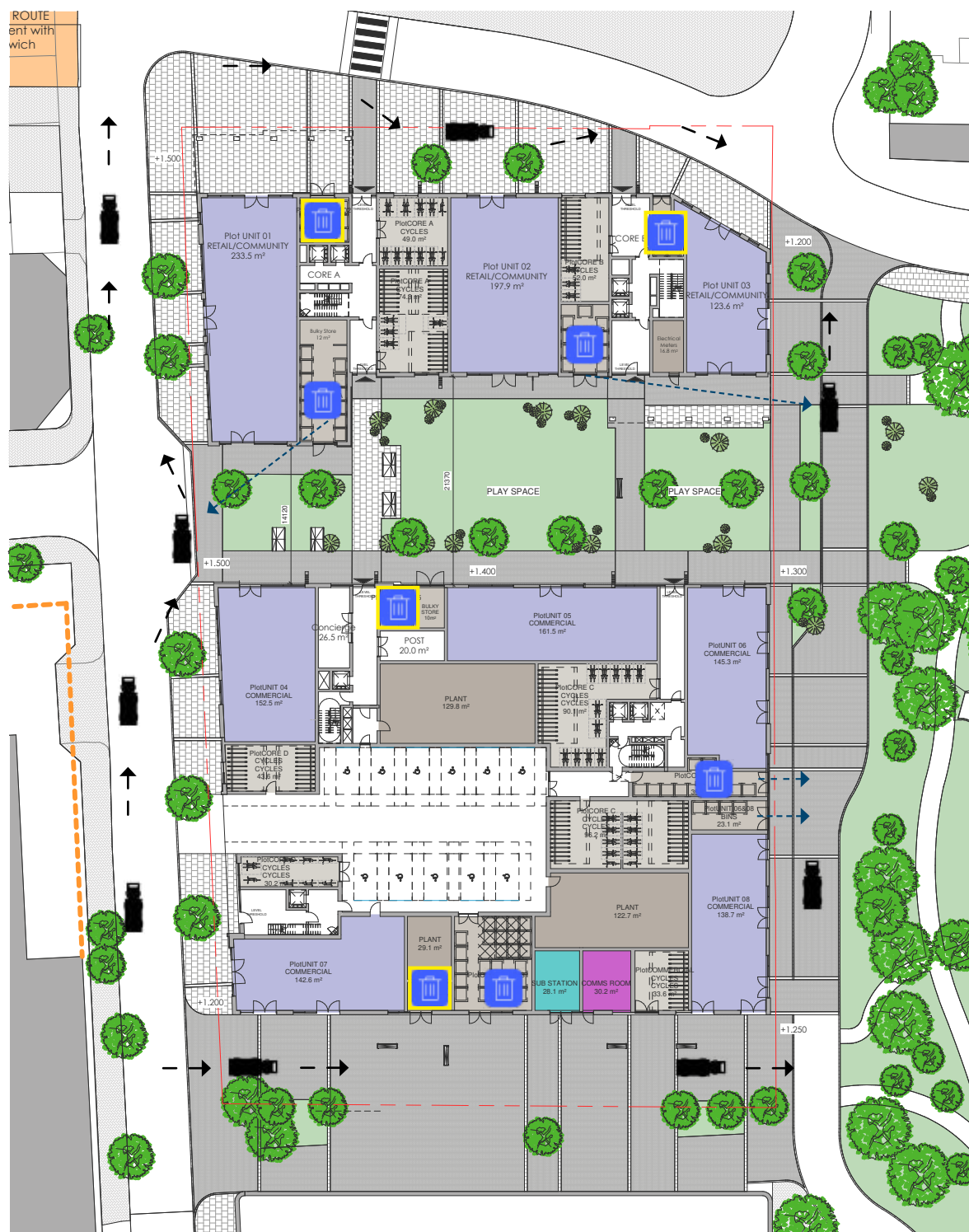
Fire & Service Vehicles

Fire Vehicles: A pump appliance can access the buildings and cores via Mirfield Street, Eastmoor Street and Westmoor Street.

Servicing: A servicing bay has been provided outside the northern block for commercial units 01-03. This space will also serve as a drop-off taxi space and delivery space for residents. The commercial units along the green link, units 04 & 05, can be serviced via a bay located on Westmoor Street. This bay can also be used for residential deliveries/post. Servicing to units 05 & 06 can take place from Eastmoor Street. Commercial unit 07 can be serviced from Westmoor Street, vehicles will be able to enter the landscaped yard and exit in forward gear via Eastmoor Street or turn and exit via Westmoor Street. Unit 08 can be serviced via Eastmoor Street or the landscaped yard.

Residential Post: Post for residential properties will be delivered to letterboxes at the ground floor of each core. Postal vehicles can use the loading bays located on Westmoor Street and Mirfield Road. A managed postal room for deliveries is located in the southern block and can be accessed via Westmoor Street.

Refuse Strategy : The design of both the residential and commercial bin stores has been achieved using RBG's 'Guidance notes for the storage and collection of waste and recycling materials', May 2018. The bin store dimensions are based on the number of bins required per core in relation to the number of units served by it. The stores will cater for recycling, residual waste, organics, textiles and waste electrical and electronic equipment. Separate stores have been allocated for larger items i.e. bulky waste. Residential and commercial waste has been kept separate.



^ Proposed Ground Floor Plan

Refuse Bins

Refuse Vehicles: Refuse vehicles can access refuse stores located on Mirfield Street via a enlarged paving area in front of the building. On the southern block, a refuse vehicle will use the hard landscaped yard space to access the refuse store and exit in a forward gear via Eastmoor Street. A further refuse store is accessed via Eastmoor Street. Eastmoor Street will be closed to cars but can be used by servicing/emergency vehicles. If Eastmoor Street becomes closed to all vehicles then all refuse and recycling can be collected from the southern store and the refuse vehicle can turn within the yard space to exit via Westmoor Street. Wheelie bins will be moved from the store on Eastmoor Street through the car park to the larger southern bin store on collection day and returned after collection.

Residential Refuse: Cores A & B have refuse and recycling stores located on Mirfield Street. Residents exit the entrance core and the refuse store is within close proximity. Resident travel distances do not exceed 30m. On collection day refuse bins are collected and emptied via Mirfield Street, the maximum bin drag distance is 15m or less. Core C has a refuse and recycling store located on Eastmoor street. Residents exit the stair core and access the refuse store via a ventilated lobby. Core D has a refuse and recycling store located on the landscaped yard space accessed via the car park area. Residents will personally arrange bulky waste collections via RBG. Battery, textile and WEE recycling bins have also been provided. Bin stores have been provided with 1500mm wide double doors and level access routes.

Commercial Refuse: Commercial units will each have their own refuse stores provided with level access and 1500mm wide double doors.



Refuse store (Commercial)



Refuse store



LANDSCAPING

6.1 Landscape Strategy

6.1 LANDSCAPE STRATEGY



1 Eastmoor Street

The existing arrangement is reduced to a one way street and a dedicated cycle lane adjacent to the park boundary. The layout introduces a change in direction to slow the speed of vehicles and cyclists at the connection to the East-West green route. The principle of extending the east-west routes into Eastmoor street to slow vehicular movement could be replicated on plots to the south, subject to further detailed discussions.

2 Penhall Gardens

A new pedestrian connection linking Westmoor Street and Moore Park. The proposed trees on the northern side of the space frame views through to the surrounding context. A planted drainage swale provides a playful edge to the route with stepping stones creating a sense of adventure and exciting circulation between the spaces. At each end of the east-west route two entrance spaces create opportunities for users of the surrounding commercial units to break out into the space.

Framed by native species planting and hedging at the centre of the east west route; the gardens celebrate the widening of the green route between the buildings. Planting divides the garden into a series of intimate, multifunctional, playful spaces that can be used by all residents of the development. Views from the residential cores into the garden are framed by multi-stem trees and low level planting.

3 Cooper's Yard

The interface between the site and the Optivo land to the south will create a landscaped east west pedestrian route. Moveable seating will create a multifunction, flexible space, accommodating servicing requirements whilst also creating usable space.

4 Mirfield Street

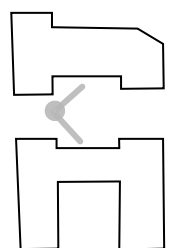
New extension to the existing pedestrian footpath to provide a connection to the proposed commercial spaces. Tree planting will continue the rhythm of the architectural colonnade on the western corner creating a new civic space adjoining the commercial units.

6 Moore Park

The proposals seek to provide additional crossing points to the western boundary and realign some of the pathways to create stronger north- south and east- west pedestrian connections. Wayfinding and play equipment will help to activate the park throughout the day.

5 Westmoor Street

The predominantly hard paved street creates a 4m paved interface with banding taking queues from the architectural fenestration. The built form along the boundary is broken up by large scale tree planting. A loading bay is located along the street as part of the shared space pavement.





Landscape Strategy

Emphasis will be placed on bringing the local community, both new and existing, into contact with nature throughout the seasons. New trees and species rich planting will bring a softer, green edge to the existing, harsh industrial environment. The unique location of the Site provides the opportunity to improve the underused Moore Park. Natural surveillance throughout the day will improve safety for pedestrians and cyclists.

Character Areas

The landscaping has been divided into a series of character areas. The characters of the spaces respond to or promote the intended identity and function of the areas.

1 Eastmoor Street

2 Penhall Gardens

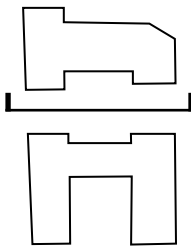
3 Cooper's Yard

4 Mirfield Street

5 Westmoor Street

6 Moore Park

Section



^ Continuous section

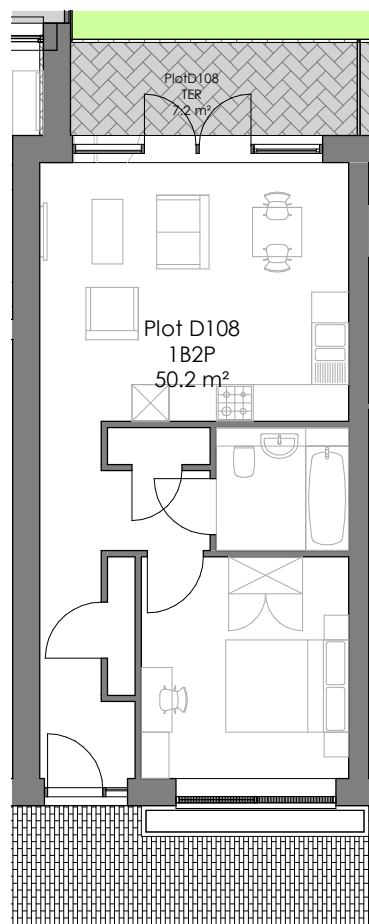


QUALITATIVE REVIEW

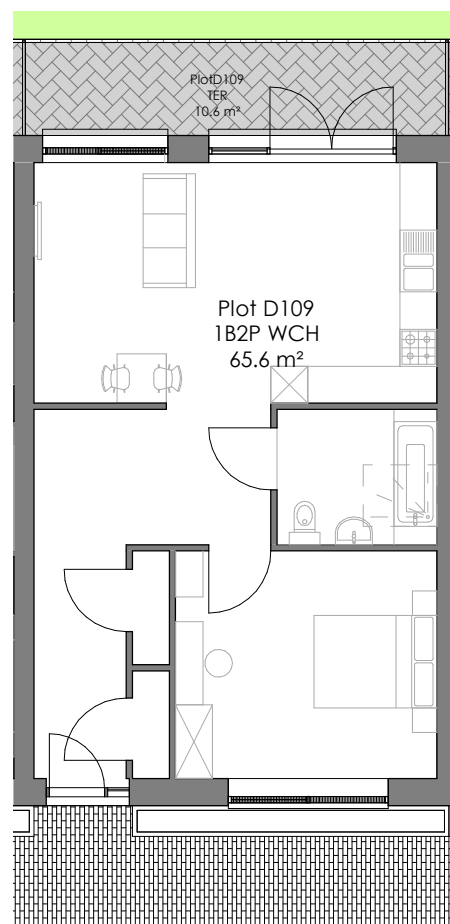
7.1 Residential Typologies

7.2 Wheelchair Flat Design

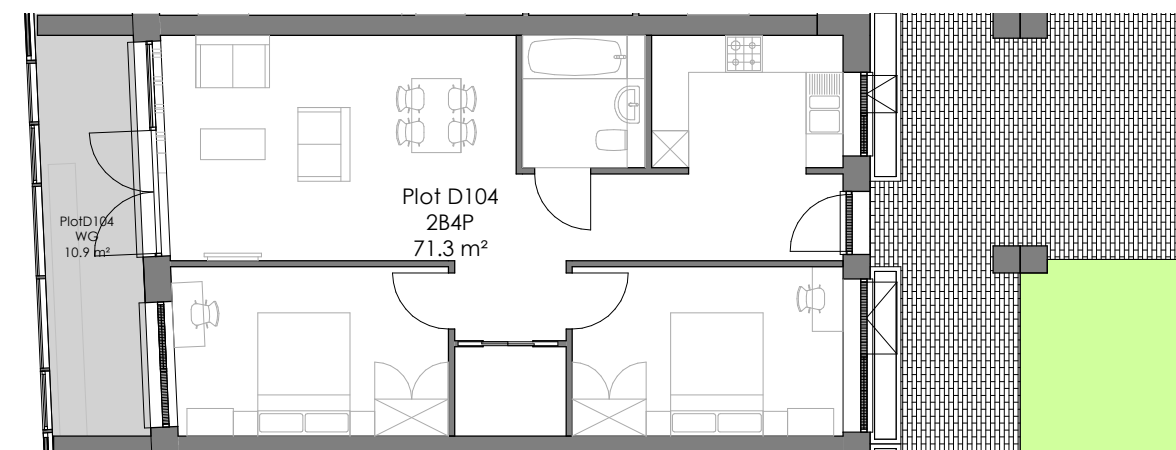
7.3 Secured by Design



^ 1 Bed 2 Person (South Block)



^ 1 Bed 2 Person Wheelchair (South Block)



^ 2 Bed 4 Person (South Block)

7.1 RESIDENTIAL TYPOLOGIES

Flat Types

The buildings provide a high quality mixture of one, two and three bedroom family homes. A variety of typologies have been provided including deck access homes and two storey duplexes. The mix provided will help to establish a new community within the Charlton Riverside area attracting both families and young professionals. Play space for varying ages has been designed into the landscape proposals and roof terraces. In line with London Plan guidance, 10% of the units will be wheelchair adaptable.

The dwellings have been arranged to a maximum of 8 homes per core per floor. The majority of dwellings are dual aspect or single aspect with east or west orientation. In each block the larger two bed and three bed family dwellings have been designed to be dual aspect. On upper floors the massing of the blocks has been stepped to create further dual aspect flats. All apartments have been designed to meet the space requirements of the emerging London Plan and the Housing Supplementary Planning Guidance 2016.

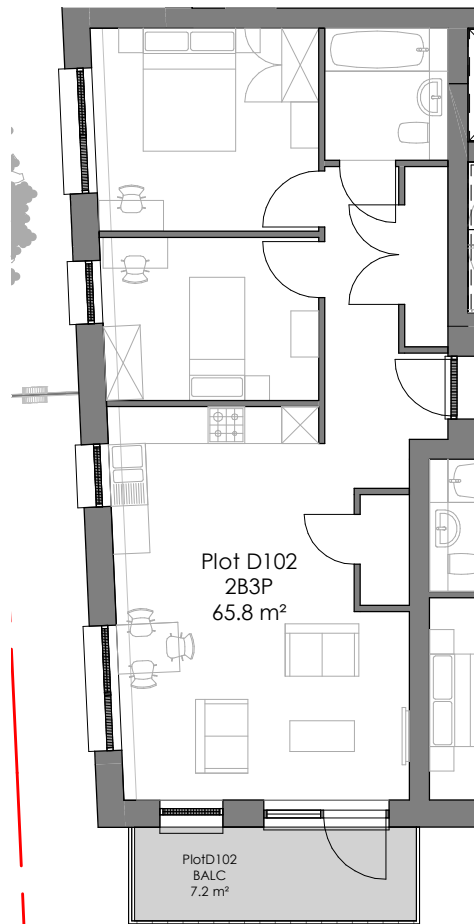
Apartment layouts

Apartment layouts for the private and shared ownership homes will be open plan to maximise the sense of space. Affordable rent flats have a more traditional layout with rooms located off a

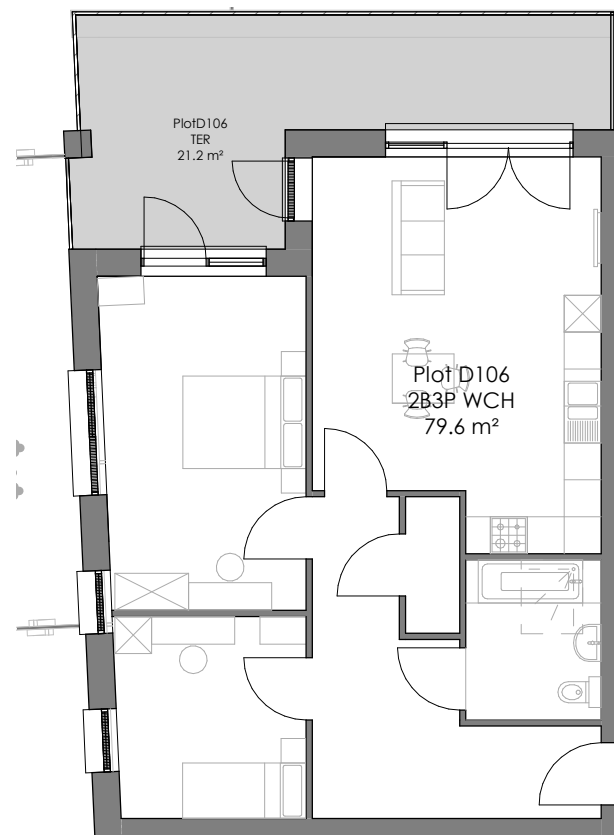
main entrance lobby. Floor to ceiling windows are proposed to all dwellings to provide optimal daylight/sunlight to residents.

Indicative furniture layouts have been shown to all flats to show that rooms are well proportioned and suitably designed for their function as either a single or double bedroom or living /kitchen/ dining room. Storage has been provided in line with the emerging London Plan requirements.

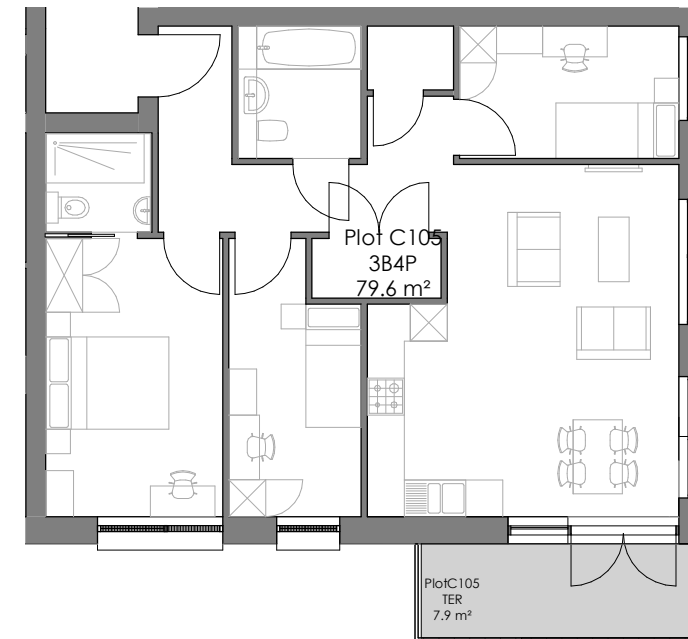
All homes have been provided with a private terrace or balcony accessible from the living/ kitchen/dining room; in some units the master bedroom also has direct access to the terrace.



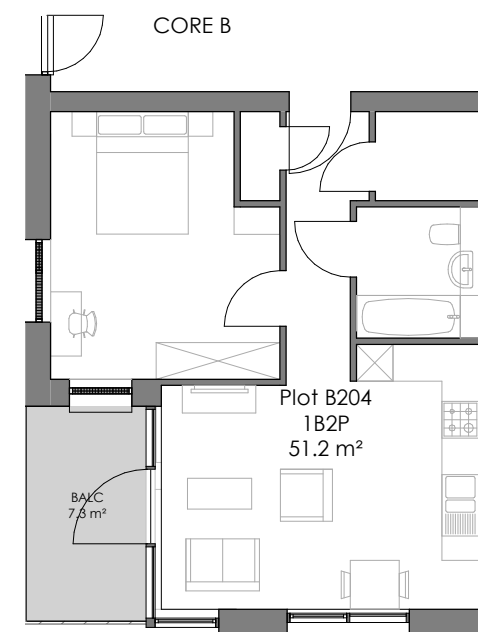
^ 2 Bed 3 Person (South Block)



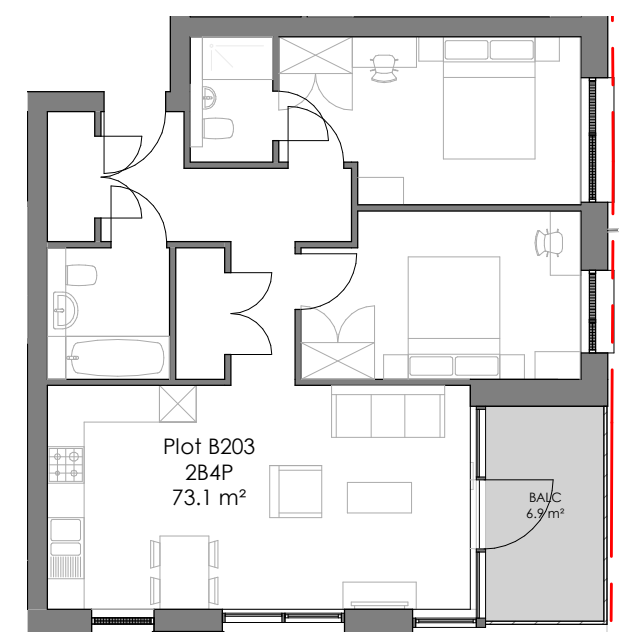
^ 2 Bed 3 Person Wheelchair (South Block)



^ 3 Bed 4 Person (South Block)



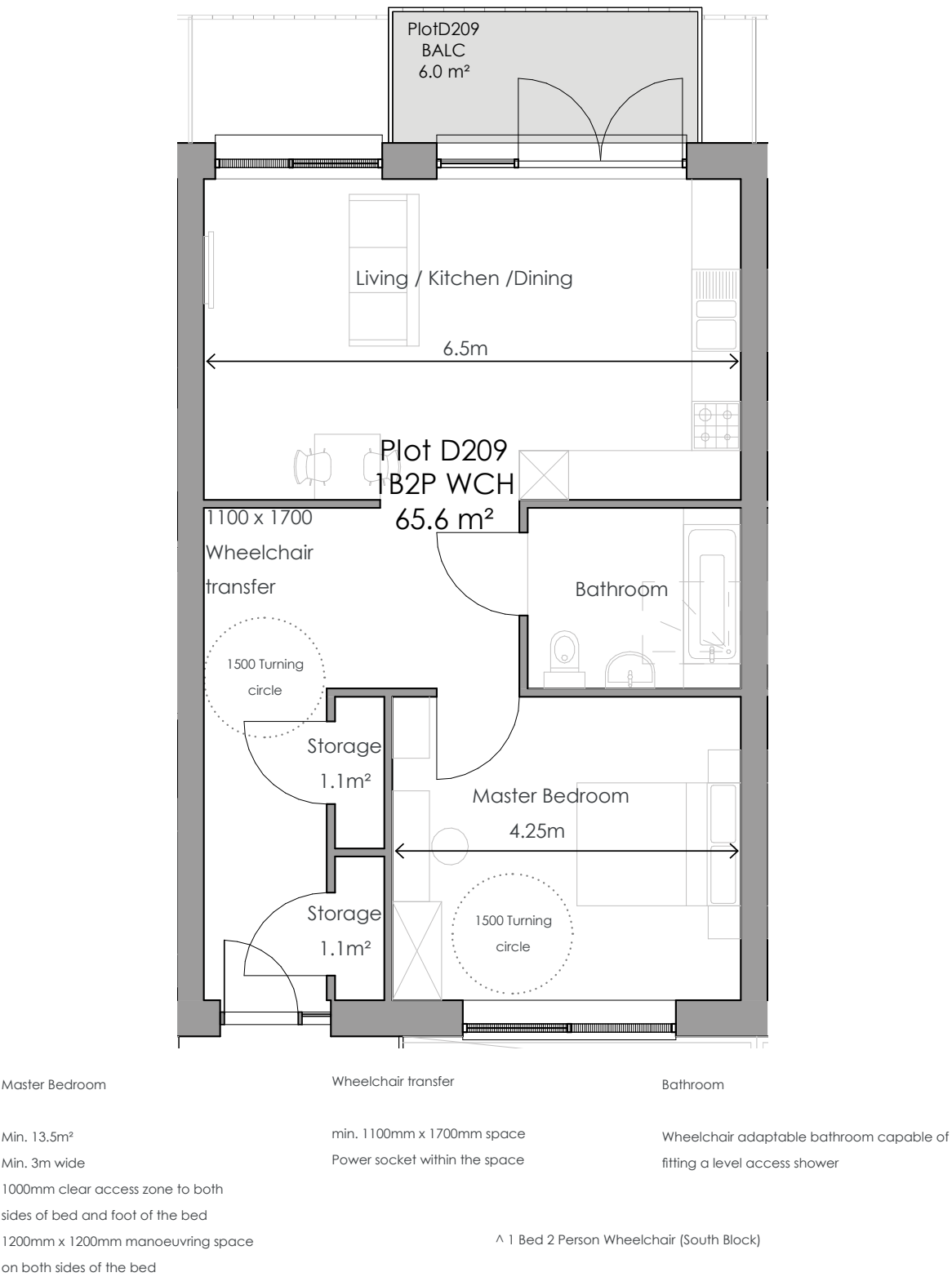
^ 1 Bed 2 Person (North Block)



^ 2 Bed 4 Person (North Block)

Living/kitchen/dining

min. 27m²
open plan kitchen & dining
min. clear access zone of 1500mm in front of and between
kitchen units and appliances



7.2 WHEELCHAIR FLAT DESIGN

Example Layout

Wheelchair flat design

In accordance with the emerging London Plan, 10% of the residential dwellings have been designed as wheelchair adaptable units based on Building Regulations Part M(2015 incorporating 2016 amendments): Access to and use of buildings, Volume 1 : Dwellings. Wheelchair flats have been provided across all tenures on Site A. The adjoining flat layouts describes the principles of wheelchair adaptation for a typical two bedroom, three person flat.

Adaptable units will have the following:

- Provision of wheelchair charging/transfer zones close to front entrance doors
- Door openings having at least 850mm clear width
- 1500mm clear turning circles in every room
- Larger access space between furniture, using the recommended dimensions from Part M
- Wheelchair accessible bathroom layouts, with potential for level access showers
- Door nibs to be a minimum of 300mm to the leading edge

Wheelchair car parking provision

A total of 14no. blue badge parking spaces have been provided within the southern building.

Accessibility

The landscaping and buildings the site have been designed to ensure accessibility for all users and residents. Potential users and visitors regardless of age or any disabilities will be able to access and navigate through the buildings and the proposed public realm.

The design has been developed with consideration of national legislation including The Building Regulations Part M (2015 Edition incorporating 2016 amendments), the Equality Act 2010 (Disability) Regulations and BS8300:2001.



7.3 SECURED BY DESIGN

The safety and security of residents, staff, guests and Moore Park users is fundamental to the development of the site. The guidance to be used for the scheme has been recommended as the Secured by Design Homes 2019 and Secured by Design Commercial Developments 2015.

The ground floor layout of the site has been designed to provide maximum passive surveillance of pedestrian routes via active commercial frontages. Mirfield Street is the main proposed east-west axis road. The proposed commercial use at ground floor in Mirfield Street is a cafe, convenience store and workshop/office space. These uses will provide natural pedestrian footfall along this street in the daytime and also into the evening. Cores A & B front directly on to Mirfield Street and link through to Penhall Gardens.

Penhall Gardens is a public pedestrian route from Penhall Road to Eastmoor Street and Moore Park. Residential entrances have been located along the length of the route in both the Mirfield Street block and the southern courtyard block, the entrances are well overlooked by residential units. The commercial units of the Mirfield Street block are dual aspect so also overlook Penhall Gardens. The southern courtyard block has access to residential cores C and D and further commercial units. The proposed uses for the

commercial units are workspaces which will be open until early evening each day.

A further access to residential core D is located on Westmoor Street providing increased footfall along the street. Additional commercial units are located along Westmoor Street providing an active frontage that continues around the south west corner to the yard space. Eastmoor Street also has a series of commercial units along its length providing overlooking to the park.

All entrance lobbies will be well lit and incorporate video call access. Access from ground floor into the apartments has been considered to restrict access for residents and visitors to the correct floors. All external entrance doors and windows at ground floor will be Secure by Design accredited.

The car park entrance will be fitted with a vehicle and pedestrian gate controlled via fob access. Cycle stores have been limited to a maximum of 60 spaces per store.

The cores must be compartmentalised in order to prevent unauthorised access. A double fob entry system will be used whereby residents will need to fob once at the main external entrance and then again at either the lift or to exit an air lock on upper storeys.



COMMERCIAL

- 8.1 Commercial units
- 8.2 Commercial elevations
- 8.3 Commercial entrances



^ Proposed ground floor plan showing commercial

8.1 COMMERCIAL UNITS

Commercial Provision

The commercial spaces of the scheme have an important role to play in animating the ground floor and making the area a safe and welcoming pedestrian space.

Eastmoor Street

Eastmoor Street will become car free creating a safe, pedestrian zone with an improved cycle route. The park edge will be vastly improved with the current car parking removed. The area will create a social space where workers and residents can interact. The pavement can be used as break- out space from the units. Street furniture will reference the Site's industrial heritage.

Penhall Gardens

The green link is another pedestrian zone accommodating green amenity for residents and employees. The space contains play space and outdoor seating space for the cafe. The area is animated by pedestrian entrances and the active frontages of commercial units.

Cooper's Yard

Cooper's Yard is conceived as a working yard. The landscaping is intentionally robust to reflect the Site's heritage. The yard acts as a functional space serving the ground floor commercial units.

The workspaces provide natural surveillance to the space via large framed windows.

Westmoor Street

Retail and workshops will provide an active frontage to the street. The pavement will be widened and street trees introduced to create a pleasant pedestrian environment.

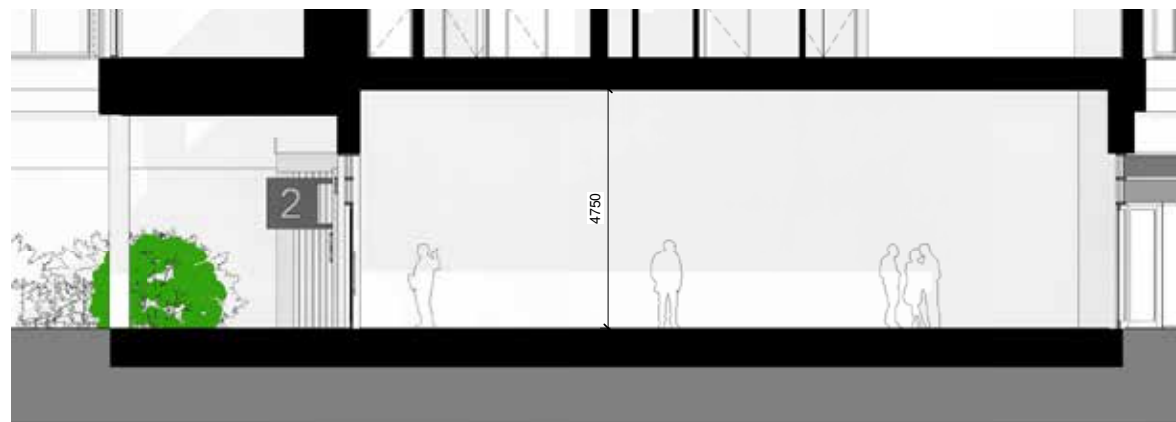
Mirfield Street

Mirfield Street is proposed as a local hub with a small retail unit and cafe/community space. The space immediately outside the building is designed as a landscaped civic space with trees and street furniture.

Unit	Proposed Use	GIA m²
Unit 01	Retail/Community A1-A5, D1	233.5
Unit 02	Retail/Community A1-A5, D1	197.9
Unit 03	Retail/Community A1- A5, D1	123.6
Unit 04	Light industrial/office B1/B8	152.5
Unit 05	Light industrial/office B1/B8	161.5
Unit 06	Light industrial/office B1/B8	145.3
Unit 07	Light industrial/office B1/B8	142.6
Unit 08	Light industrial/office B1/B8	138.7
Total		1295.6



^ Proposed ground floor plan showing commercial (North block)



^ Section through commercial (North block)

North Block - Commercial

Mirfield Street - Local Hub

The potential ground floor uses proposed for the Mirfield Street building create:

Retail

The quantum of development on the site will support the inclusion of a convenience retail offer. This could include a metro/express style small supermarket such as Simply Fresh. The aim is to meet the day-t-day needs of new residents for small, frequent shops.

Cafe/Community Use

A cafe/community use provides a place for residents and employees to meet and helps to foster a new living/working community on the Site.

The commercial units are single storey with tall glazing combined with triple aspect design to maximise the daylight entering the spaces. The internal height is 4750mm which creates a sense of space and height.

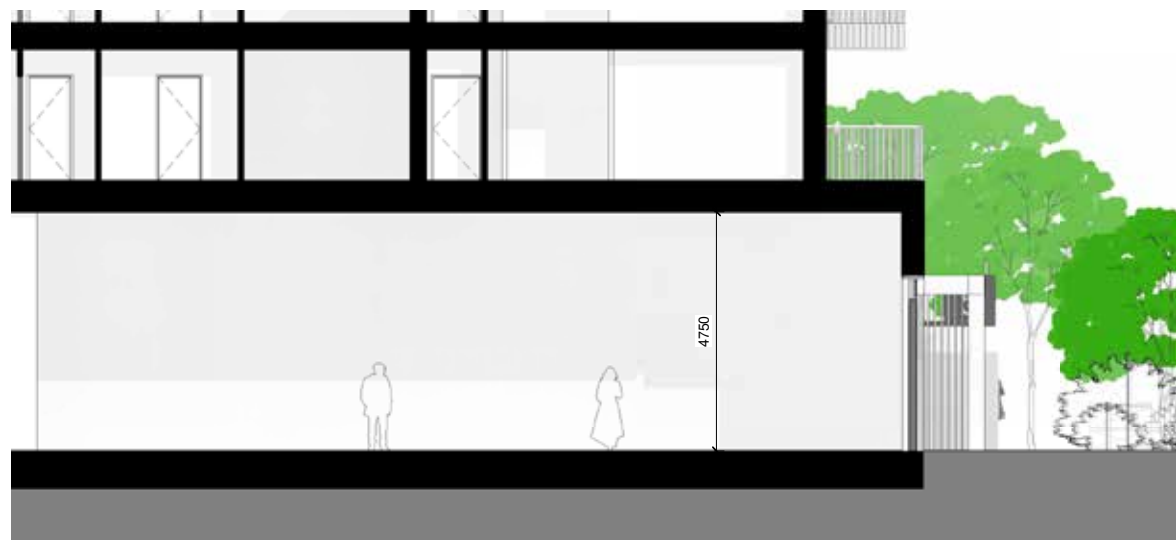
Servicing

A loading bay is provided on Mirfield Street to serve the cafe, retail and workshop unit. The workshop unit can also utilise the loading bay located on Westmoor Street. Separate commercial bin stores are provided.



^ Design intent of commercial areas





^ Section through commercial (South block)

South Block - Commercial

The ground floor uses proposed for the southern building create:

Light Industrial/maker Space Office

Self contained workshop units for small to medium businesses have been provided. These offer the opportunity to house light industrial uses or co-working office space. The units can be sub-divided and space planned by end user ensuring maximum flexibility.

Tall glazing- maximising daylight

Clear internal height = 4750mm

Servicing

The units can be serviced from the loading bay on Westmoor Street, Cooper's Yard and Eastmoor Street. Separate commercial bin stores are provided.



^ Design intent of commercial areas



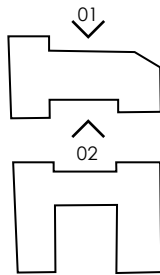
8.2 COMMERCIAL ELEVATIONS



^ 01 Commercial bay elevation - North block north elevation



^ 02 Commercial bay elevation - North block south elevation

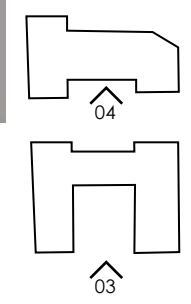




^ 03 Commercial bay elevation - South block north elevation



^ 04 Commercial bay elevation - South block south elevation

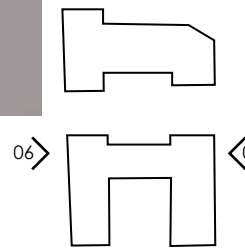




^ 05 Commercial bay elevation - West elevation



^ 06 Commercial bay elevation - East elevation



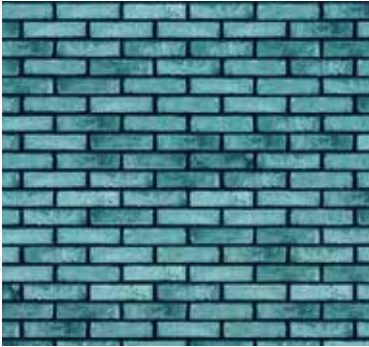
8.3 COMMERCIAL ENTRANCES



^ Dark Brick



^ Light Brick



^ Glazed Blue and Green Brick



^ Dark Metal Panels



SUSTAINABILITY

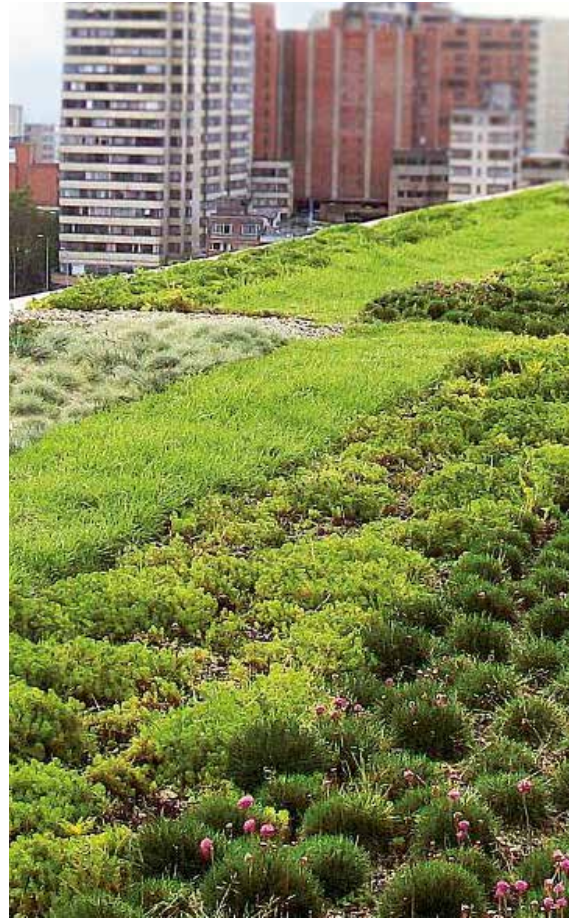
9.1 Energy and Sustainability

9.2 Daylight & Sunlight

9.3 Overheating



^ Solar Panels



^ Green Roof



^ Cycle parking



^ Charging points for electric cars

9.1 ENERGY & SUSTAINABILITY

Sustainability

As well as setting the tone for development in Charlton in terms of design, we are proposing a range of carefully considered technical and environmental measures. These include:

Energy

Air Source Heat Pumps

Solar PV panels

A central energy centre serving entire development passively reducing carbon production through an efficient building envelope

Optimised facade design will be implemented to reduce energy demand whilst achieving good internal daylight and sunlight and mitigating overheating risk in the summer months.

External balconies will be designed to support solar shading strategy and provide private amenity

Dual aspect flats, allowing for cross ventilation and good daylight and sunlight levels

Transport

Cycle parking for all users including residents, employees and visitors

Improved pedestrian routes

Improved cycle routes

Low car parking on site to deter car usage

Car club space

Travel plans to encourage a carfree development

Provision of electric car charging points - 20% of spaces will have 'active' charging facilities from the outset with the remaining spaces having 'passive' provision, in accordance with policy.

Landscape and Ecology

Enhancement of existing Moore Park

SUDS drainage incorporated in landscaping design

New green route to provide new wildlife corridors and connection to Moor Park

Tree lined streets and terraces to provide canopy cover, wind mitigation and seasonal interest

Urban greening factor 0.32

Community infrastructure

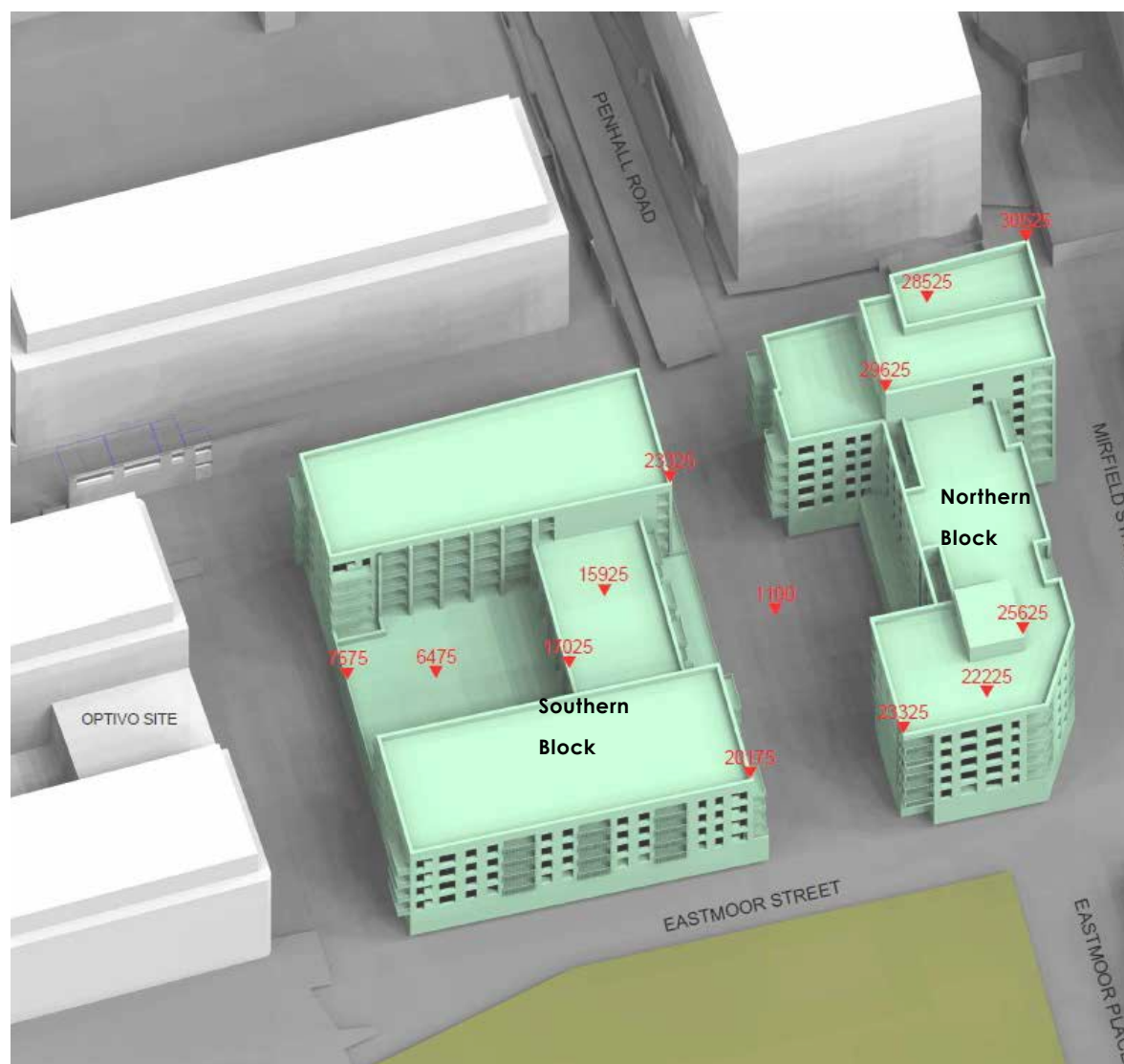
Spaces for communal social interaction

Designated areas for civic use such as cafe

Tenure blind scheme

Mix of dwelling occupancies

Please refer to the sustainability report produced by JAW Sustainability in support of the Planning Application.



^ Scenario 02 modelling- Avison Young

9.2 DAYLIGHT & SUNLIGHT

The daylight/sunlight assessment consists of three assessments:

The impact of the proposals on the existing, surrounding properties

Scenario 1 - surrounding baseline conditions as existing and the emerging Optivo site massing

Scenario 2 - the emerging Optivo site massing and hypothetical future massing in an attempt to reflect the emerging context and aspirations of the Charlton Riverside Masterplan SPD

Daylight/sunlight levels to proposed and existing amenity spaces

Daylight/sunlight levels within the proposed residential units

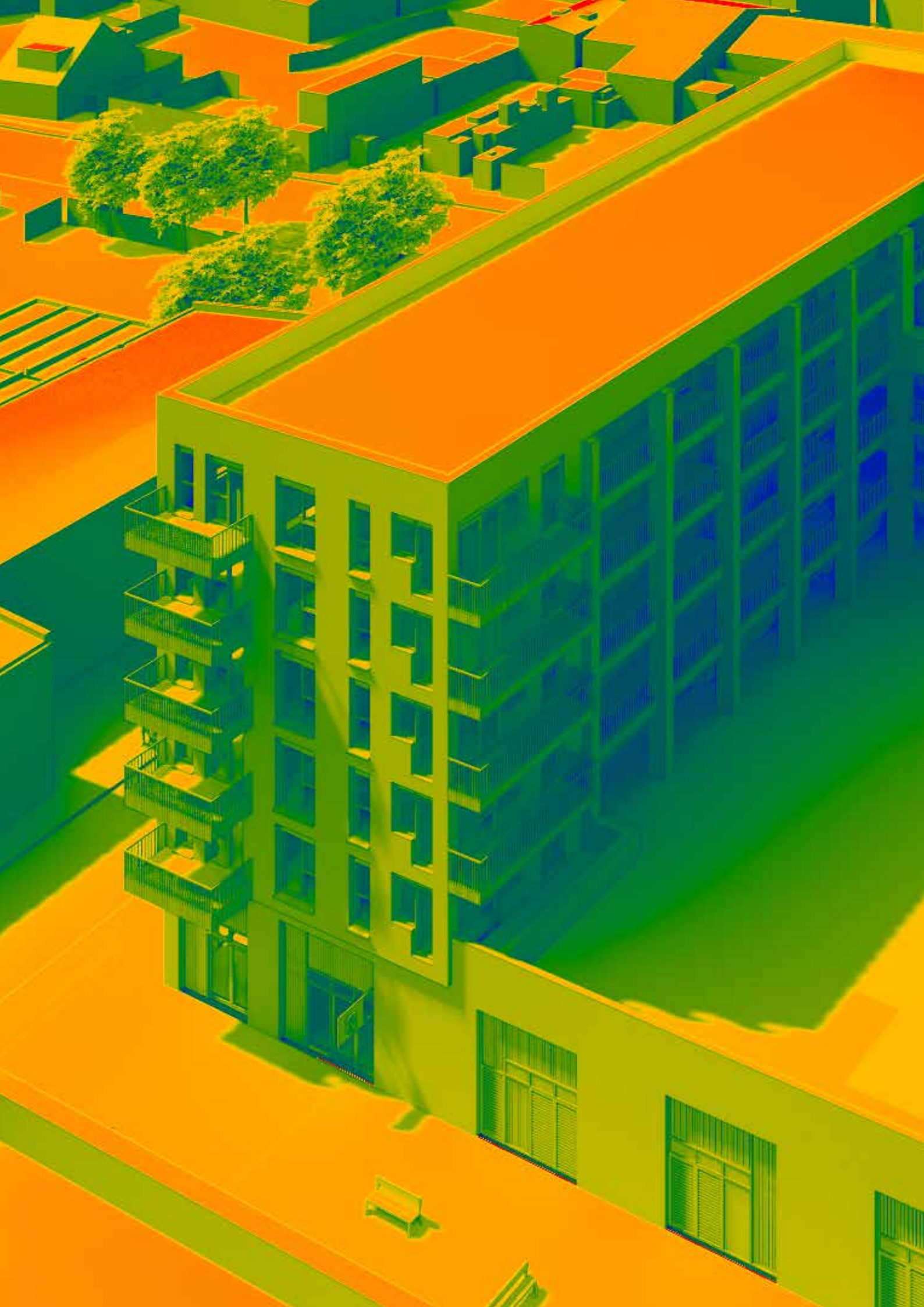
been considered within the daylight/sunlight assessments and the Eastmoor site massing does not have a detrimental affect on the daylight/sunlight levels of homes proposed on the Evelyn House site.

Daylight/Sunlight within Landscaped Spaces

Daylight and sunlight to the proposed green link, Penhall Gardens, has been considered. The northern and southern buildings are set a minimum of 15m apart increasing to 30m along the green link. The southern courtyard also receives good daylight and sunlight levels. The buildings have been set 23.5m apart across the courtyard. The impact of the scheme on the existing Moore Park has also been considered as part of the assessment. The scheme has no adverse impact on the daylight/sunlight levels to Moore Park

The impact of the proposals on the existing surrounding properties

The Site sits within an existing industrial zone, no residential properties are currently located near the site and therefore a daylight/sunlight assessment is not necessary. The plot to the south of the site however, is about to be submitted as a Planning Application providing commercial space at ground floor and residential accommodation above. The proposal has



Daylight/Sunlight within the Proposed Residential Units

Daylight, sunlight and overshadowing have been a key design consideration throughout the design process. The massing layout of site has been carefully considered to ensure good daylight levels to homes. The taller 8 storey massing has been located at the north-west corner of the Mirfield Street building. This taller element provides dual aspect homes as it projects beyond the adjoining block on both its north and south facade. The north-east corner is also designed to provide dual aspect units. The central section of this building uses deck access to create dual aspect units with individual front doors accessed via a sheltered colonnade. Overall the proposed development is considered to make very good use of the daylight available to the Site with a high pass rate. In relation to sunlight, the levels of sunlight amenity are comparable with schemes of a similar typology. Furthermore, residents will have access to well sunlit amenity areas internal and external to the proposed development.

Please refer to the Daylight/Sunlight report produced by Avison Young in support of the Planning Application.

Overheating

During the design process the area of south facing glazing has been carefully considered. The deck access colonnades and projecting balconies help to provide natural shading to south facing windows. These shading techniques have been assessed to ensure daylight/sunlight levels within the homes throughout the year are not adversely affected.

An overheating report has been produced by JAW Sustainability in support of the planning application.

10.

CONCLUSION

10.1 Concluding comment



10.1 CONCLUDING COMMENT

The re-development of 61-81 Eastmoor Street will regenerate 0.592 hectares of the Charlton Riverside industrial estate. The area has a huge role to play in London's capacity to deliver new homes for the next generation of residents. The Charlton Riverside SPD outlines the vision for the area to deliver 5000-7500 new homes and 4,400 additional jobs. Eastmoor Street will in total deliver 149 homes. The delivery of this site together with the adjacent Evelyn House site will act as a catalyst to drive inclusive, sustainable development in the area. As London evolves, it is imperative that it is inclusive to all, 10% of the total homes proposed on the site will be affordable (shared ownership and London affordable rent tenure).

The key location of the Eastmoor Street site ensures that the phased delivery of the east-west link road is achievable; it also delivers the first phase of the east-west green link to Moore Park. The re-design of Eastmoor Street and its closure to cars will vastly improve the park access for pedestrians. The narrowing of the carriageway and removal of car parking will allow the park to expand and soften the boundary to the site. The park will have the added safety of active commercial frontages overlooking the space at ground floor and residents overlooking the space above. The presence of local residents will ensure the park is used throughout the day and

into the evening on summer nights. The current lack of pedestrian activity makes the park feel unsafe and uninviting. New public realm around the site has been designed to act as an extension to the park. The harsh, industrial landscape will be softened by green landscaping and street trees which will also increase the biodiversity of the area. Penhall Gardens is proposed as a lively, green and sustainable environment where residents can relax whilst children play. Residential entrances are located along its length providing a pleasant transition from street to front door for residents. The landscaping will also help to foster a local community where residents recognise and chat to neighbours whilst entering and leaving the buildings.

The proposed commercial spaces further help to nurture a sustainable community. A local cafe is proposed where residents can enjoy a drink overlooking the gardens to the rear. A small convenience store is also proposed to ensure new residents can locally pick up a loaf of bread or a pint of milk. The proposed workshops/office spaces mean new residents can potentially both work and live on the site creating a sustainable community and cater for the new working from home arrangements many firms have in place due to the COVID-19 pandemic. Residents will also have easy access to the surrounding cycle network.



Throughout the design process, the community, local council and stakeholders have been consistently consulted and engaged via public consultation, pre-application meetings, GLA meetings and TfL meetings; ensuring an inclusive and iterative design process. The design has evolved in response to this involvement and the proposed scheme has been greatly enhanced as a result. The proposals have also been scrutinised by the Design Council. Following the refusal in earlier in 2021, the scheme has been amended to substantially reduce the mass and height as these were the main concerns. The scheme has reduced to 4-6 storeys at the rear on the south block, and 6-8 on the north block (the previous application was 8-10 storeys); now more aligned with the aspirations of the SPD and the expectations of local residents.

The proposed development is safe and secure for residents, whilst being open and accessible to the wider community. The implementation of the SPD links will improve connectivity within the area for pedestrians, cyclists and motorists. The affordable homes will ensure the existing community will be able to remain connected to Charlton. Potential improvements to public transport provisions, bus routes, will create further links to the wider city for residents. Parking and vehicular use has been restricted on the site to promote a sustainable dependence on public transport, cycling and walking.

Dwellings vary from one bedroom apartments to three bedroom family homes, ensuring a

diverse community. The 1295.6m² of commercial space being delivered will accommodate new businesses. These spaces will provide local employment and increase footfall and use of the surrounding spaces.

The proposal conforms to the framework of the Charlton Riverside SPD, encapsulating its key principles to create a compliant yet visionary scheme. Buildings are restricted to ten storeys maximum, a varied massing of 4-8 storeys is proposed creating a development with affordable family homes. The scheme responds to the need for future flood protection and references the site's industrial heritage; following the recommendations and requirements of the SPD.

The key ambition of the Site, is to develop a scheme that makes the best, most efficient use of the Site by delivering a significant number of homes, whilst also creating a sustainable, inclusive and active community.

Following the amendments post refusal and the extended period of consultation with the local residents and planning authority, we firmly believe this is the most suited proposal for this site and community.

This scheme will act as a catalyst for the area of Charlton Riverside, and drive forward the much needed regeneration to alleviate the requirement of housing in the area.

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Land at 61-81 and Coopers Yard Eastmoor Street,
and 6 & 10 Westmoor Street, Charlton, London, SE7 8LX

Planning Statement

Eastmoor Street 81 LLP

November 2021

BPTW brings together specialisms in Architecture and Planning to transform not just physical spaces, but people's lives. 30 years experience with many of the UK's leading developers and housing providers to create desirable new homes and places. Our work tells our story. We are bold. We are innovative. We care.

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Document Control Sheet

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Prepared by	Gabrielle Delorme	Planner	19/11/2021
Reviewed by	David Akam	Senior Planner	22/11/2021
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For and on behalf of BPTW

Revision	Date	Description	Prepared	Reviewed	Approved

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Appendices

Appendix 1. Site Location Plan

Appendix 2. Charlton Riverside Masterplan SPD (2017) Tracker

Appendix 3. Statement of Community Involvement

1.0 Introduction

- 1.1. This Planning Statement has been prepared on behalf of Eastmoor Street 81 LLP (**the Applicant**), in support of a full planning application submitted to the Royal Borough of Greenwich (**the Council**) for the redevelopment of 61-81 and Coopers Yard on Eastmoor Street, and 6-10 Westmoor Street, Charlton (**the Site**).
- 1.2. The purpose of this Statement is to set out the development proposal and how it relates to the statutory development plan and other material considerations. The Planning Statement should be read in conjunction with the supporting documents listed below in para.1.9.
- 1.3. Full Planning Permission is sought for:

Demolition of existing structures and erection of buildings between 4 and 8 storeys in height (including roof access level areas with a maximum total height of 29.5m AOD), comprising 149 residential units, 740.6 sqm flexible employment floorspace (Use class E(g) and B8) and 555 sqm flexible retail and community uses (Use class E and F2) with associated landscaping and new public realm, access and infrastructure works, refuse and recycling storage, car parking and cycle parking and associated development.
- 1.4. The Site is located within the Royal Borough of Greenwich, 400m south from the River Thames, on Eastmoor Street, within the Charlton Riverside Opportunity Area and Masterplan SPD area. It is situated approximately 800m northeast of Charlton Railway Station and the adjacent parade of shops constituting the closest local centre (15min walk). The closest town centre lies 1.62km east of the Site: Woolwich Town Centre.
- 1.5. With a total area of 0.59ha, the Site itself comprises single storey industrial warehouses and open storage space used for storage, vehicle repair/car breaker, scrap metal yard/dealer, and metal castings (Use Classes B2 / B8 / Sui Generis).
- 1.6. The proposal is for the redevelopment of the Site for a residential-led mixed use scheme including flexible commercial units to the ground floor. The proposal follows the Council's aspirations to regenerate the wider Charlton Riverside area into a thriving mixed-use community.
- 1.7. The main elements and key benefits of this proposal are:
 - > Mixed-use development delivering 149 high-quality new homes, with 10% affordable rent tenure with a focus on family accommodation;
 - > Green link at ground level, courtyard at podium level and accessible roof terraces providing 2778.8 sqm of communal amenity space including play space;
 - > A high-quality human scale development between 4 and 8 storeys in height;
 - > Total of 1295.6sqm flexible employment and retail and community uses on ground floor;
 - > Active frontages on all streets; and
 - > Concealed Blue Badge and commercial units parking below podium.
- 1.8. This planning application follows an earlier application ('Scheme A') submitted for the Site in July 2020, which was refused planning permission in October 2021 on the grounds of its height and massing being deemed incommensurate with 'human scale.' Accordingly, the Applicant has revised the height and of its proposal for the Site and is submitting this 'Scheme B' for full planning permission.

Submission Documents

- 1.9. In addition to this Planning Statement, the application is accompanied by the following documents.

Report	Author
Application form and CIL form	BPTW
Air Quality Assessment, including: <ul style="list-style-type: none"> - Air Quality Neutral Assessment - Ventilation/Extraction Strategy 	Ardent
Archaeological Assessment (desk-based)	RPS
BREEAM Pre-assessment	JAW Consulting
Car Park management Plan	Ardent
Circular Economy Statement	JAW Consulting
Construction Logistics Plan (outline)	Ardent
Daylight Sunlight Overshadowing Assessment	Avison Young
Delivery and Servicing Management Plan	Ardent
Design and Access Statement, including: <ul style="list-style-type: none"> - Accessibility Statement - Accommodation Schedule - Designing Out Crime - Lighting Assessment - Materials Schedule (indicative) - Photographs / Photomontages - Sustainable Design and Construction Statement 	Alan Camp Architects
Ecological Appraisal and Roosts Assessment (preliminary)	The Ecology Partnership
Employment Statement and Relocation Strategy	Andrew Sissons Consulting
Energy Strategy	JAW Consulting
Fire Statement	Clarke Banks
Flood Risk Assessment, including Sustainable Urban Drainage System Strategy	Ardent Consulting
Health Impact Assessment	RPS
Heritage, Townscape and Visual Impact Assessment	Lichfields

Land Contamination Assessment (Desktop Study)	JOMAS
Landscape Strategy, including Play space strategy	Studio Bosk
Life Cycle Carbon Assessment	JAW Consulting
Noise Assessment	Ardent
Overheating Assessment	JAW Consulting
Planning Statement, including Statement of Community Involvement	BPTW
Schedule of Accommodation	Alan Camp Architects
Sustainability Statement	JAW Consulting
Transport Assessment	Ardent
Travel Plan	Ardent
Tree Survey and Arboricultural Impact Assessment	Keen Consultants
Utilities and Services Assessment	Ardent
Viability Assessment and Affordable Housing Statement (including build cost estimate & EUV)	Red Loft

Community Consultation

- 1.10. A Statement of Community Involvement (SCI), prepared by Lexington Communications, is appended to this report as **Appendix 3**. This SCI was initially produced and submitted in support of Scheme A, demonstrating that those proposals were subject to a comprehensive programme of pre-application engagement with the Charlton community and other key stakeholders, local groups, and interested persons. Due to the similarity of Scheme B to Scheme A, save for a reduction in height, and the recency of the Scheme A public engagement process, it was not considered necessary to conduct further community consultation on the Scheme B proposals.
- 1.11. The post-submission process will involve further dialogue and engagement with statutory consultees and other relevant stakeholders and presents a formal opportunity for any third-party groups or persons wishing to make representations on the proposals.

Structure of this Statement

- 1.12. This Planning Statement is structured as follows:
- > Section 2 describes the Site within the immediate local context;
 - > Section 3 summarises the pre-application advice received;
 - > Section 4 sets out the proposed development in greater detail;

- > Section 5 identifies the relevant national, regional and local policy context;
- > Section 6 addresses the material planning considerations of the proposal; and
- > Section 7 draws together the conclusions of the report.

1.13. The appendices referred to in the text are included at the end of this Statement.

2.0 Site and Surrounding Area

- 2.1. The Site is located between Westmoor Street and Eastmoor Street, within the Royal Borough of Greenwich (RBG). The Site is approximately 800m northeast of Charlton Railway Station.
- 2.2. The Site is not situated with a District or Major Town Centre. Charlton Railway station constitutes the closest local centre (15min walk). The closest town centre is Woolwich Town Centre lying 1.62km east of the Site.



Figure 1: Aerial view of the Site (source Google maps)

- 2.2. The Site is bounded by Eastmoor Street to the east, Westmoor Street to the west, Mirfield Street to the north, and industrial warehouses on the south side (Beaumont Beds). The surrounding area is predominantly industrial in character; however, it is part of the Charlton Riverside Masterplan that intends to transform the area into a thriving mixed-use community.
- 2.3. Moore Park lies adjacent to the Site to the east. It is designated as a Site of Importance for Nature Conservation (SINC) and Site Schedule nc27 in the policies map. It links Maryon Park to the south and the Thames Barrier Park to the north.
- 2.4. The Site is 0.59ha in area and comprises single storey industrial warehouses and open storage space used for storage, vehicle repair/car breaker, scrap metal yard/dealer, and metal castings (Use Classes B2 / B8 / Sui Generis).
- 2.5. There are no Tree Preservation Orders (TPO) on or adjacent to the Site. A Tree Survey of the Site confirmed that there are no trees on the site itself, but trees exist within the pavement on the adjoining roads. None of

the trees are of any particular merit, with most having some sort of defect. On the corner of Mirfield Street with Eastmoor Street is a London plane that is of some merit but is not an exceptional specimen.

- 2.6. The Site is not within a Conservation Area. It is however at the boundary of the Thames Barrier and Bowater Road Conservation Area. There are no statutory listed or locally listed buildings present on the Site. However, it is close to two locally listed former public houses: 'The Lads of the Village' is located 30m to the north east, and 'The Victoria' 80m to the south east; and nearby a few other locally listed heritage assets: within Maryon Park (150m to the south), at 25 Westfield Street (150m to the east), the Lithuanian Christian Church 696 Woolwich Road (200m to the south east), and the red brick wall on Hardens Manor Way (60m to the north east of the Site).
- 2.7. The Environment Agency's Indicative Flood Map shows that the Site lies within Flood Zone 3 (high risk) and benefits from flood defences.
- 2.8. The Site has a Public Transport Accessibility Level (PTAL) score of 2 (where 1 is low and 6 is high). There are no Controlled Parking Zone (CPZ) within or in the immediate vicinity of the Site.
- 2.9. The Site is within a borough-wide Air Quality Management Area (AQMA).

Planning History

- 2.10. The Site is subject to a previous planning application:

Reference	Development	Notes
"Scheme A" LPA Ref: 20/1924/F	Demolition of existing structures and erection of buildings between 6 and 9 storeys in height comprising residential units, flexible employment floorspace and flexible retail and community uses with associated landscaping and new public realm, access and infrastructure works, refuse and recycling storage, car parking and cycle parking and associated development.	Developer Eastmoor Street 81 LLP (the Applicant) Refused on 06/10/2021

- 2.11. An initial application for full planning permission (Scheme A) was submitted for the Site by the Applicant and validated on 17th July 2020. After continuous discussions and several design iterations, despite its recommendation for approval by RBG Planning Officers, RBG Planning Committee Members resolved to refuse it planning permission on 20th July 2021. This decision was approved by the GLA, and the decision notice was issued on 6th October 2021, the single reason being:

- > "The proposed development, by virtue of its height and massing, fails to comply with the Charlton Riverside Masterplan SPD and, in particular, would not deliver the medium-rise development on a human scale as outlined in the SPD. The proposal would therefore be contrary to policy D3 of the London Plan (2021), policy DH1 of the Royal Borough of Greenwich Local Plan Core Strategy with Detailed Policies (2014) and the Charlton Riverside Masterplan SPD (2017)."

- 2.12. A search of the Council's online Planning Register revealed no other recent planning history for the Site.

- 2.13. Surrounding applications in the Charlton Riverside Area include the following:

Address / Reference	Development	Notes
Stone Foundries		Developer Montreaux

669a Woolwich Road, Charlton, SE7 8SL LPA Ref: 20/0751/EIA	Scoping Opinion under Regulation 13(1) of the Town and Country Planning (EIA) Regulations 2017 (as amended) for a mixed-use, mixed-tenure residential-led development including up to 7 blocks with heights ranging from 4-10 storeys; Approximately 1,200 residential units, up to 130,000m2 Gross Internal Area (GIA); Approximately 15,000m2 GIA of commercial space including a mix of offices, light industrial use, shops, retail outlets, food and drinking establishments and community services and a new primary route running east-west along the southern boundary of the site.	This is not a planning application however 10-storey buildings are included in the proposal. Approved Decided 22/04/2020
Faraday Works 17-21 (Unity House), 18-32 (Wire Workshops), 25 (Junction Box), 37 (Faraday Building), Bowater Road, Westminster Industrial Estate, Charlton Riverside, Woolwich, SE18 5TF LPA ref: 19/430/F	Redevelopment of the north western parts of Westminster Industrial Estate through a residential-led mixed use scheme comprising 492 residential units, 10,439 sqm(GIA) of commercial floorspace (including up to 181sqm of retail (Use Classes A1-A5), 5,272sqm of business space (Use Classes B1(a) to B1(c)) and 4,986sqm of B1(c)-B8), the refurbishment and extension of both 18-32 Bowater Road (The Wire Workshops) and 25 Bowater Road (The Junction Box), the external and ground floor refurbishment of 17-21 Bowater Road (Unity House) and the demolition of 37 Bowater Road, altogether with new public realm, hard and soft landscaping, parking, access and servicing arrangements, plant and associated works.	Developer U+I Under consideration (Validated: 30/01/2020)
Herringham Quarter 9, 40-45 Herringham Road, 55 New Lyndenber Street, units 1-32 New Lyndenber commercial estate, SE7 LPA ref: 19/3456/F	A hybrid planning application comprising: - Outline planning application (appearance, landscaping, layout and scale reserved) for the proposed demolition of buildings and structures, the construction of buildings between 4-10 storeys in height, comprising up to 530 residential units (Class C3), 3,559 sqm (GEA) flexible employment (Use Classes B1), retail (Use Classes A1-A4), community and leisure (Use Classes D1 and D2) floorspace; associated infrastructure; new streets, open spaces, landscaping and public realm; car, motorcycle and bicycle parking spaces and servicing and other works incidental to the proposed development. - Detailed planning application for the proposed demolition of existing buildings and structures, the construction of buildings between 4-10 storeys in height, comprising 762 residential units (68 x Studios, 268 x 1-bed, 273 x 2-bed and 153 x 3-bed) (Class C3), 3,592 sqm (GEA) of flexible employment (Use Classes B1), retail (Use Classes A1-A4); community and leisure (Use Classes D1 and D2) floorspace; associated infrastructure; new streets, open spaces, landscaping and public realm, car, motorcycle and bicycle parking spaces and servicing; the construction of new flood defence wall and delivery of	Developer Hyde Housing Under consideration (Validated: 17/10/2019)

	ecological habitat adjacent to the River Thames, and other works incidental to the proposed development.	
Herringham Road & New Lyndenberg Street, Charlton, SE7 LPA ref: 19/1926/EIA	Scoping Opinion for the Redevelopment of the site to provide a mixed-use scheme of up to 1350 residential units (Class C3), up to 7,000 sqm GEA of commercial floorspace (use classes A1, A2, A3, A4, B1, D1 and D2) in buildings up to 10 storeys in height including basements. Associated new publicly accessible open space, new pedestrian and vehicle routes and landscaping, car and cycle parking and new river wall.	This is not a planning application however 10-storey buildings are included in the proposal, adjacent to the river. Approved Decided: 03/07/2019
Flint Glass Wharf 3 Herringham Road, Charlton, SE7 8NJ LPA ref: 18/0732/F	Hybrid Application: - Detailed Planning Permission for the demolition of existing buildings & structures on site and redevelopment of the eastern area (Phase 1) for the erection of 2 buildings between 7 & 9 storeys to provide 146 homes (8xStudio, 82x1-bed, 34x2-bed & 22x3-bed) & 482 sqm GEA of non-residential floor space Classes A1-A4 & Class B1 (Energy Centre 240 sqm) & new public open space & public realm, delivery & servicing space, 35 car parking spaces and 217 Cycle Spaces - Outline planning permission (landscaping, scale & appearance reserved) to provide up to 45,000 sqm floorspace GEA, comprising up to 354 residential units and up to 1,300 sqm of non-residential floor space within Classes A1-A4, Class B1, Class D1 & D2 use	Developer Komoto EIA development Under consideration (Validated: 27/07/2018)
VIP Trading Estate Anchor and Hope Lane, Charlton, SE7. LPA ref:16/4008/F	Demolition of existing buildings and erection of 11 buildings ranging from 2 to 10 storeys in height for Class C3 residential use (771 units), with flexible uses comprising Class B1 (Business), Class A1- A3 Retail / Restaurant), Class D1 (Community) and Class D2 (Leisure)	Developer Rockwell Appeal dismissed: 03/06/2020 Within a designated 10 storey area. Initially the application included plans for a 28 storey however this was reduced to SPD standards. Objections were poor design, effect on existing surrounding buildings, lack of space for existing local businesses, lack of S106 agreement for affordable housing and other mitigation of impact. (Source: Chamberlain, 2019, in the Charlton Champion)

Land North of Woolwich Road opposite 71-101 Barney Close, Charlton, SE7 LPA ref: 16/1447/F	Construction of a new 5-storey B1(c) development with alternative use as B1(a) to provide 14 new workspaces.	Approved Decided: 16/11/2018
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2.14. Surrounding planning applications within the wider Greenwich area include the following:

Address / Reference	Development	Notes
40 Victoria Way, Charlton, SE7 7Q5 LPA ref: 17/1795/F	Demolition of existing warehouse and redevelopment of the site to provide 330 residential units in blocks ranging from 2-10 storeys, creation of 439 sqm of community floorspace (Use Class D1) in a part 1/2 storey block, 999 sqm (Use Class B1) in a 3 storey block, 144 car parking spaces and associated cycle parking, refuse and landscape.	Approved Decided: 27/03/2018 Officers noted the site did not fall within an area designated appropriate for tall buildings. However, Officers did not consider the scale of development would be harmful to the character of the area.
81-88 Beresford Street, SE18 6BG LPA ref: 16/1975/F	Demolition and redevelopment of the former Woolwich Catholic Club for a part 7/part 11 storey building providing a mixed-use development comprising gym use (Class D2) at basement and ground level and 59 flats above, together with associated works.	Approved Decided: 22/12/2017 Officers considered acceptable in terms of height as precedent in the area had achieved similar heights.

3.0 Pre-Application Advice

- 3.1. In accordance with the National Planning Policy Framework (2021), the Applicant has engaged with the Local Planning Authority (LPA) and a number of other statutory consultees to ensure an integrated design process in relation to residential-led redevelopment of the Site.
- 3.2. The Applicant has discussed the evolving proposals with the Royal Borough of Greenwich (RBG) Planning, Highways, Conservation and Regeneration Officers, the Greater London Authority (GLA) Officers, Transport for London (TfL) Officers, the Environment Agency and the Design Council's Design Review Panel (DRP).
- 3.3. In addition, the Applicant has sought to engage the local community. A virtual Public Consultation exhibition was organised through the scheme's dedicated website and provided the opportunity for the community to explore the proposals. A live webinar event was also held to give the opportunity to discuss the proposals with the Applicant Team.
- 3.4. A Statement of Community Involvement prepared by Lexington Communications is submitted in support of this application. It sets out the programme of community consultation that was tailored for the project and adapted to the current social distancing situation.
- 3.5. Key comments provided by RBG and GLA, along with the input from the DRP, TfL and the EA throughout the pre-application process are set out below. The way in which the Applicant team has responded to these comments and the scheme has evolved accordingly is summarised.

Royal Borough of Greenwich

- 3.6. The following meetings took place with the RB Greenwich Planning Officers during the pre-application phase:
 - > Initial Pre-Application Meeting on 8th October 2019;
 - > Follow-up Pre-Application Meeting on 27th January 2020; and
 - > Third Pre-Application Meeting on 22nd April 2020.
- 3.7. The scheme presented to the initial pre-application meeting, included 232 homes (35% affordable housing) with approximately 1,350m² of commercial space at ground floor. The built form created a landscaped podium at first floor enclosed by blocks ranging from 7 to 10 storeys, with car parking at grade under the landscaped podium.
- 3.8. In summary, a number of points were raised:
 - > Officers noted that the proposal exceeded the heights indicated in the Masterplan SPD and suggested that the proposal was too tall but did not indicate what height they considered acceptable.
 - > Officers suggested that there could be more variation in height between different massing elements and felt that further design/townscape justification was required.
 - > Officers accepted that the SPD indicates that the north west of the Site is intended to contribute to a new Local Centre.
 - > Officers confirmed that the recent adoption of the Conservation Area and the sensitive receptors that are the two Locally Listed Buildings were key factors that should be considered when proposing the height of the proposal. Officers did not confirm the aspects of the Locally listed Buildings that would be most important to be protected.
 - > Officers supported the principle of redevelopment but required further justification on the height/density of the proposed development.

- > Officers confirmed that they considered that the proposed site layout was well thought out and appropriate for this Site. Officers supported moves to step away at the north eastern corner to celebrate where Mirfield Street meets Eastmoor Place.
- > Officers appreciated that the width of the new east-west green link to the south of the Site is difficult to confirm due to the scale of the Masterplan information. BPTW confirmed that they had scaled it to circa 40m separation between the facing elevations. The proposal provides circa 12m measured from the southern facing elevation. It is envisaged that the site to the south will provide the same separation distance to provide a route of circa 20-24m.
- > Officers accepted that there is limited detail in the Masterplan regarding the function or design of the new green route. They understood the justification for this space to narrow as the green route meets the Barrier Park to help define the different characters of both areas.
- > Officers agreed to provide contact details for the EA representative to explore the requirements for access along Eastmoor Street.
- > Officers urged Eastmoor Street 81 LLP to open discussions with the neighbouring landowners but understood the confidentiality and sensitivities currently at play.
- > Officers confirmed that the Core Strategy seeks 35% Affordable housing. The GLA will seek 50% as the Site is former SIL land. Mix sought is 70/30 in favour of London Affordable Rent tenure.

3.9. The design was developed with the project team following the initial pre-application meeting with RBG. The revised scheme provided 231 homes, 2,261m² commercial space and 44 car parking spaces. The Pre-application scheme was developed as follows:

- > E-W road link considered and buildable area of the site adjusted accordingly in line with SPD provision;
- > Green route through the site aligned to Penhall Road to ensure route is feasible in terms of surrounding land ownerships;
- > Formation of two distinct buildings, a northern block fronting Mirfield Street and a southern courtyard block; and
- > Massing proposed ranged from four to ten storeys.

3.10. On 7th January 2020 the scheme was presented to RBG Regeneration. The focus was on the east – west route, the EA requirements over Eastmoor street, and ensuring the applicants in the area work together to ensure that development is ‘joined up’.

3.11. The revised scheme was discussed at a 2nd pre-application meeting with RBG. In summary, the following points were raised:

- > Height: The height proposed for the site exceeds the 3-5 storeys allocated by the SPD, any deviation in height will need to be justified.
- > Commercial Space: GLA seeks business relocation and uses to be retained where possible, there is no relevance in retaining current businesses on this site (car breakers etc). B8 space will need to be designed appropriately.
- > Urban Design & Layout: more thought required on the differentiation between each face of the development Eastmoor and Westmoor Streets should have different character. Variation to flat layout required explore deck access. Green link key feature to be developed. Active frontages required to southern façade.
- > Parking: Car free scheme cannot be supported until local infrastructure is improved.
- > Amenity: More informal playspace at ground floor with connection to the park and adjoining the Evelyn House/ Optivo scheme.

- > Housing Mix: Family housing provision of 19.5% considered acceptable, this provision should be balanced in accordance with the play space availability on site. 35% affordable provision acceptable, still requires a viability review.

3.12. The proposals were refined to address the comments received:

- > The massing to the north east corner was reduced to create a better articulated marker tower. The proposals aims to create a taller, nodal element on the junction of Mirfield Street and Westmoor Street marking the district centre. Height has also been influenced by viability with the Applicant always aiming to provide a minimum of 35% Affordable Housing.
- > The site is earmarked as forming part of a new district hub within the SPD document. The commercial units proposed consist of a cafe/community use and retail units. The remainder of units are being proposed as high-quality flexible workspace.
- > Upper floor setbacks were introduced to each block to reduce massing and improve natural light in the courtyards.
- > A curved facade was added to the north-east corner to complement the proposed road alignment.
- > The green link was developed further with play space moved from first floor to ground floor making it accessible to all.
- > The commercial spaces were re-designed to provide active frontages to the southern facade.
- > Car parking was consolidated within the southern block and removed from the northern block. Parking spaces provided as blue badge spaces only.
- > Landscaped amenity that was proposed at first floor in the northern block was dropped to ground floor level making it accessible to all.
- > The Applicant again committed to aiming for 35% affordable housing provision. Family housing provision to target 20%.

3.13. At a 3rd pre-application meeting with RBG the following points were raised:

- > Exact number and location of affordable homes should be confirmed prior to submission.
- > The southern 10 storey block appears bulky and incongruous and it is recommended that the main parapet should be reduced to 6 storeys with the potential to explore a two storey set-back element above.
- > There are also concerns about the form of the northern 10 storey block. It is considered that the corner 10 storey block should be expressed more legibly as the local marker on the site and the rest of the building simplified and possibly lowered down by a storey, with a more subtle marker at the junction with Eastmoor Place, towards the locally listed pub. The tallest element of the scheme reads as too broad and bulky and it is suggested that the option of subdividing this block into two slightly shifted elements, with a taller element at the street junction and a lower one towards the green link, is considered.
- > It is proposed that the non-residential floorspace would be for flexible use and it is noted that the commercial units within the southern block would have floor to ceiling heights of 6m to allow for a range of users and the insertion of mezzanine floors. Whilst the desire to provide flexible spaces is understood, further information should be provided with regard to the detailed layout / specification of the units to demonstrate that they will meet the needs of accommodating employment generating uses (B1).

3.14. The proposals were again refined to address the comments received. The proposals are set out in the Proposed Development section and the Design and Access Statement.

- 3.15. On 17th June 2020, the project team had a virtually meeting with the Leader of the Council, the Chair of the Planning Committee, the Cabinet Member for Regeneration & Housing, Regeneration Officers, and high level RBG Officers. The scheme was presented including a summary of the pre-application advice and consultation responses received and how the proposals had evolved. The Leader then asked other attendees to comment and ask questions which the Applicant team addressed. The Leader of the Council noted that the proposals offered clear benefits.
- 3.16. The presentation included slides presenting the east – west route proposals to the north of the site and the Applicant team summarised meetings and advice to date. No objection was raised to the route presented and no questions were asked of the Applicant team.

Design Review Panel

- 3.17. The Applicant team initially presented the proposals to the Design Review Panel (DRP) on 28th February 2020. The below summarises the comments received:
- > Develop further detail on how proposed route and green links will connect to existing and proposed routes. Width of green link and play space needs increasing to improve daylight/ sunlight, further landscaped space could be provided adjacent to the Evelyn House/Optivo site.
 - > Investigate alternative route for EA to the Thames Barrier so Moore Park can be improved.
 - > Greater understanding of the heritage is required to inform the design, add variation to the elevations to respond to context.
 - > Re-consider the vehicular entrance to the south of the site, potential for vehicles to access directly off Westmoor Street.
 - > Consider utilising mezzanine levels for cycle storage to improve the size of the commercial units.
 - > Create further variety to height on the site and reduce the height, ensure green spaces receive adequate daylight/sunlight.
 - > Look to increase the number of dual aspect homes and consider the positioning of family homes and their access to amenity. Avoid double loaded access corridors explore different typologies.
 - > Simpler expression of building on corner of Mirfield Street and Eastmoor Street, removal of curve. Possibility to create civic space in this location. Further consideration of building materials, perhaps a lighter brick rather than the red brick proposed.
 - > Hierarchy of spaces private/public needs further exploration.
 - > Biodiversity to be considered to increase character of landscaping proposed and the adjacent Moore Park.
- 3.18. In response to the above the scheme was revised as follows:
- > The massing of the northern block was amended so the central green link could be widened and opened up to the park with improved play space designed in. The link proposed between the site and the Evelyn House site is a hard-landscaped space that also serves the commercial units.
 - > At this time the EA insisted on access along Eastmoor Street, although this message has since changed and further investigation into the feasibility of the EA using Westmoor Street is taking place. In response to advice from the EA and RBG Officers at the time the landscape design included the narrowing of the carriageway for service vehicles only ensuring the landscaping of the edge of the park could be improved.
 - > A study of local vernacular including the local warehouse buildings was carried out and reflected in the proposed materials.
 - > The car park entrance was re-positioned to Westmoor Street.
 - > Investigations at this time into the use of Mezzanines to comply with TfL guidance showed that this would require the provision of two lifts to the mezzanine level and the cost implications associated

with this were considered too great. As the design entered the final stages of design refinement the decision was taken to propose a mezzanine level in the core C cycle store as the core C residential lift can provide access to the mezzanine level if the cycle store platform lift is temporarily out of service, guaranteeing convenient access.

- > Deck access was introduced to both building blocks to increase the number of dual aspect homes and larger duplex homes were designed on upper floors.
- > The curve to the building was removed to simplify the architecture and the positioning of balconies was amended. Materials were amended to three types of brickwork - a light brick, a grey brick and a black brick.
- > The green link was further animated with increased active frontages and residential entrances.
- > Deck access to the southern courtyard block was re-positioned to the courtyard to encourage social interaction between residents.
- > The landscape design was developed further to include the narrowing of the carriageway on Eastmoor Street and introduction of additional planting and trees along the edge of the park.

3.19. The Applicant team presented the revised proposals to the DRP on 1st May 2020. The DRP acknowledged that positive progress had been made. The below summarises the comments received:

- > The DRP recognised the positive progress that had been made both in the design of the scheme itself and in critical elements relating to its wider masterplan context, which support an improved scheme overall.
- > The DRP noted that due to the site context its relationship to the wider context and a coherent, integrated design approach is crucial, particularly in relation to the streetscape and landscape, the height and massing, and the character of the architecture.
- > The DRP recommend lowering the height across the scheme to improve the quality of light to homes and spaces and to ensure that the scheme both on its own and alongside adjacent and future schemes creates the character that is envisaged for Charlton. They suggested introducing greater variety to break up the mass and create light in certain places across the scheme, stepping the height down from the north-west corner towards the east to address the scale of the park, lowering the southern building so that the height responds appropriately to the adjacent Optivo site and undertaking shadow studies to understand the impact of overshadowing on green link and park.
- > The DRP welcomed the significant progress made on landscape and streetscape and encouraged the Applicant to work collaboratively with emerging developments to ensure a holistic approach to this across the area, to ensure this space works as part of a cohesive neighbourhood.
- > The DRP welcomed the addition of gallery access, which increased the proportion of dual aspect and the quality of accommodation, and the introduction of duplex flats, which creates a greater diversity of housing types.
- > The DRP commended the ongoing discussions with the Environmental Agency and the Royal Borough of Greenwich regarding the change in the emergency access requirement to the Thames Barrier from Eastmoor Street to Westmoor Street as significant and positive step for the quality of place at Charlton Riverside and this scheme.
- > The DRP welcomed the revised street layout to Mirfield Street and its junction with Eastmoor Street to the north of the site and support the further set back of the buildings to allow for a wider pavement.

3.20. The proposals were again refined to address the comments received. The proposals are set out in the Proposed Development section and the Design and Access Statement.

Greater London Authority

3.21. The following meetings took place with the GLA Planning Officers during the pre-application phase:

- > Initial Pre-Application Meeting on 30th January 2020 ; and
- > Follow-up Pre-Application Meeting on 31st March 2020.

3.22. The scheme presented to the initial meeting proposed two U-shape buildings separated by a 15m wide green access link. The scheme comprised 231 residential homes and the maximum height across both blocks was 10 storeys, with the ground floor occupied by commercial floor space, cycle and car parking covered by podiums providing amenity space at first floor level.

3.23. The key points raised in the GLA's response letter can be summarised as follows:

- > The principle of mixed-use redevelopment was supported.
- > Further engagement with TfL and the Council, regarding the alignment of the east-west access road to the south of the Site, was required.
- > Direct engagement with TfL was also advised to identify necessary measures for the improvement of transport provision for future residents.
- > The proposed extent of car parking provision was not supported as the expectation is for car-free residential development.
- > The existing and emerging land use context should be taken into consideration within the design process, referencing the agent of change principle.
- > The Applicant should engage with existing occupiers and inform the GLA of plans for relocation.
- > The re-provision of light industrial use space (B1 Use Class) was queried, and demand should be tested, and requirements should be discussed with potential future occupiers.
- > Access to and design of the industrial units was queried, and the Applicant was requested to refer to the GLA's Industrial Land Intensification Primer.
- > The applicant should engage with the Council in order to identify required social infrastructure needs.
- > Further details on affordable housing provision were requested e.g. the proposed tenure split.
- > Details of the play strategy for older children should be provided. Provision of play space on the podium was supported.
- > Given the site's PTAL, a high percentage of smaller homes is generally considered inappropriate. The Applicant should discuss the dwelling mix with the Council.
- > Play space provision for 5+ years on site was supported, with precise quantum justification.
- > Heights and overall design were supported, providing the impact on Westmoor Street is assessed in the context of emerging development, as well as the relationship with the Optivo proposal to the south.

3.24. A revised scheme was presented to the follow-up meeting. The key points raised at the meeting can be summarised as follows:

- > The main point of concern for RBG remains the height. The London Plan has changed tall building policy, and therefore tall buildings proposed outside allocated areas need to be justified against the SPD.
- > East-west alignment of proposed road still needs further work and discussion with TfL and RBG.
- > Commercial spaces proposed should not be generic. Could some of the existing businesses (B1 use) in the Charlton area be re-located in new spaces? Units must be designed to accommodate B1 light industrial use. Could Whitton Castings be retained on the site as they have a certain level of protection being a creative industry. Social uses would be equally welcomed. Site is ear-marked as new local centre, uses should also reflect this.
- > A car parking free development with 10% disabled parking is a positive and is supported. Vehicle access to the Evelyn House/Optivo site must be considered.
- > GLA has produced a document 'Making London Child-friendly'. More details are required on how the design aims to comply with this document. The scheme also needs to consider the GLA's private and public space charter to ensure amenity space is accessible.

3.25. At the time of writing a written response letter has not been received from the GLA.

Environment Agency

- 3.26. A meeting took place with the Environment Agency (EA) on 16th January 2020. The meeting focussed on the EA's requirements for Eastmoor Street and Barrier Park to the north. Since this meeting the EA is considering using Westmoor Street instead. The proposed scheme has been designed to facilitate the EA's requirements.

Transport for London

- 3.27. The scheme was presented to TfL Officers on 16th April 2020. The below summarises the comments received:
- > The alignment of the east west route, to support a rapid transit system, through the Site is not in line with the drawings included in the Masterplan SPD.
 - > It must be demonstrated that the public realm created on Westmoor Street is sufficient to support anticipated pedestrian flows and does not create any pinch points.
 - > A coherent approach to improving Westmoor Street is needed to ensure that a high-quality pedestrian and cyclist environment is created and that modifications will not be at a detriment to introducing bus services into the area.
 - > The Masterplan SPD includes an aspiration for Eastmoor Street to be closed off to through traffic. The Applicant should engage with RGB to see how the proposed development can support this aspiration.
 - > The development should ensure that a strong connection with Woolwich Road is created.
 - > Disabled persons' parking should be provided equivalent to only 3 per cent of residential dwellings.
 - > Active electric vehicle charging points should be installed for all spaces.
 - > A Travel Plan for each land use should be submitted with the application.

4.0 Development Proposal

- 4.1. This section provides a description of the proposed development. Further details are provided in the accompanying Design and Access Statement, prepared by Alan Camp Architects, and within the specialist reports submitted in support of the Planning Application.
- 4.2. The redevelopment of the Site is proposed in the context of Charlton Riverside's masterplan and contributes to redeveloping the local industrial area into a sustainable mixed-use community.
- 4.3. The scheme will provide 149 residential dwellings, with a mix of 1-bed, 2-bed, and 3-bed flats, as set out below. It seeks a maximum viable provision of affordable housing of 10%, with a proposed tenure split of 70% London Affordable Rent and 30% Shared Ownership units.

- 4.4. Planning permission is sought for the following description of development:

Demolition of existing structures and erection of buildings between 4 and 8 storeys in height (including roof access level areas with a maximum total height of 29.5m AOD), comprising 149 residential units, 740.6 sqm flexible employment floorspace (Use class E(g) and B8) and 555 sqm flexible retail and community uses (Use class E and F2) with associated landscaping and new public realm, access and infrastructure works, refuse and recycling storage, car parking and cycle parking and associated development.

- 4.5. The development proposal submitted respond to comments made by RB Greenwich, the GLA, TfL, the EA and the Design Review Panel throughout the pre-application phase, as well as the local community's comments made during the online public consultation. The proposal has also evolved following the refusal of the initial Scheme A at Planning Committee, addressing comments made by Committee Members.

- 4.6. The key features of the proposal are:

- > Mixed-use development delivering 149 high-quality new homes, with 10% affordable rent tenure with a focus on family accommodation.
- > Green link at ground level, courtyard at podium level and accessible roof terraces providing a total of 2778.8 sqm of communal amenity space including play space.
- > A high-quality human scale development between 4 and 8 storeys in height.
- > Total of 1295.6 sqm flexible employment floorspace, retail and community use at ground floor.
- > Active frontages on all streets.
- > Concealed Blue Badge and commercial units parking below podium.

Layout and Use

- 4.7. The proposal has been designed to enable the sensitive co-location of residential and commercial on the site. The co-location of commercial workspace and residential units will help to create an active community on the site throughout the day.
- 4.8. The proposed development includes two distinct buildings, the northern block facing Mirfield Street and the southern block with courtyard. The buildings are separated by a generous new East-West green linking Westmoor Street to Moore Park. The northern block is set back from the Site boundary to enable the creation of a new East-West Route as proposed within the Charlton Riverside Masterplan, providing active frontage to both the new road and the new green link. The southern building provides active frontages to the green link, Moore Park, Westmoor Street, and the new servicing yard adjoining the neighbouring site to the south.

- 4.9. At ground floor, a total of 1295.6 sqm of flexible commercial space is proposed, including retail, community and employment uses. Due to flood risk, residential accommodation cannot be provided at ground floor. The northern building has the potential to provide a cafe at the prominent intersection of Mirfield Street and Eastmoor Street which, being dual aspect, can also overlook the quieter gardens of the green link. Two further larger units provide the opportunity to create retail and workspace within this building. In the southern building a variety of sizes of commercial units have been provided. The commercial space has been designed with regard to the GLA's Industrial Land Intensification Primer's guidance to provide flexibility of use.

Westmoor Street

- 4.10. The proposal is recessed from the site boundary to provide an enhanced environment for Westmoor Street. The pavement width has been increased to 3750mm allowing for the introduction of landscaping, street trees, furniture, and delivery bays. The overall street width follows the proposals set by Hyde in their masterplan for Herringham Quarter as discussed with TfL.
- 4.11. Three independent commercial units form an active frontage to the street, a residential entrance also provides further natural surveillance. On the upper floors, winter gardens are provided as private amenity to protect residents from traffic noise along the street.

Eastmoor Street

- 4.12. The proposal has been designed to respond to the changing layout of Eastmoor Street. Initial discussions with the EA confirmed that maintenance access for the barrier will still be required along Eastmoor Street. The scheme has been designed to allow a single, one-way carriageway along Eastmoor providing emergency and servicing access only. This has created the opportunity to improve and extend the park boundary and create a pedestrian friendly environment. Meetings with RB Greenwich Regeneration have suggested that vehicular access to Eastmoor Street will eventually be removed, and the soft landscaping of the park extended. The proposal ensures that in the interim EA emergency and servicing vehicles have access and the park edge is significantly improved.

Southern servicing yard

- 4.13. The proposal has been discussed with OPTIVO and BPTW Architects, the Applicant and designers of the neighbouring Evelyn House site. In order to ensure adequate daylight/sunlight and privacy to residential windows, it has been agreed that each site owner will set the buildable plot boundary 9m from the shared boundary line. The 18m zone between building faces at ground floor has been designed as a hard landscaped, pedestrian yard space with commercial units providing natural surveillance to the space.
- 4.14. The commercial units face south to activate this area and generous tree planting will provide a welcoming environment.

Scale and Massing

- 4.15. The massing has been developed to respond to the emerging context of the Charlton Riverside Area. The heights of the buildings have been considered to ensure amenity spaces, the green link and the courtyard space receive adequate daylight and sunlight.
- 4.16. The proposed heights of the buildings have dominated the design process, in particular the additional height proposed for the site in comparison to the heights specified within the SPD. The building heights have been reduced as part of this iterative process during pre-application stage and following the determination of Scheme A application (LPA Ref.20/1924/F) refused on the ground of height and massing. The proposed heights have been assessed to ensure amenity spaces, the green link and the courtyard space, receive adequate

daylight and sunlight, as well as to further address the concerns over human scale raised by Committee Members.

- 4.17. The massing of the northern building creates a defined slender taller block of 8 storeys at the intersection of Mirfield Street and Westmoor Street, a key focal point along Mirfield Street the future East-West Route. The massing lowers to 6 storeys towards Moore Park.
- 4.18. The southern building sits 14 to 21 metres from the northern building, across the green link. It sits in a U-shape around a large south facing courtyard of 489 sqm at first floor level. The landscaping of the courtyard creates an intimate, relaxing feel in contrast to the external city and river views from the homes. It is surrounded by three blocks of 4, 5 and 6 storeys.
- 4.19. Both buildings have flat roofs that accommodate either communal amenity space, including play space for children, or biodiverse green roofs and photovoltaic panels at various levels.

Appearance and Materials

- 4.20. The palette of materials has been developed in response to the surrounding industrial heritage. The predominant building material proposed is brickwork in three complimentary tones – light buff and grey brickwork, plus an additional glazed brick for use at ground floor – blue glazed brickwork.
 - > The grey brickwork is used to form the commercial plinth to the buildings providing unity to the ground floor pedestrian areas.
 - > Blue glazed brickwork highlights residential and commercial entrances at ground floor.
 - > The light buff brickwork highlights most of the residential accommodation on upper floors.
 - > The grey brickwork is also used on upper floors to introduce variation and visual interest and to highlight the 8-storey block.
 - > Metal cladding is also used punctually to create layering and depth to the facades.
 - > Windows are proposed as double glazed PPC aluminium black metal.
 - > Balustrades to balconies are proposed as PPC dark grey metal railings (copper handrail).

Housing Quality and Mix

- 4.21. The building layout ensures the flats are positioned to take advantage of the east, south and west aspects of the site in order to maximise the amount of daylight and sunlight within the flats for residents. 76% of homes are dual aspect, which means that they benefit from the ability to cross ventilate and achieve a greater range of sunlight/daylight.
- 4.22. The northern building has been designed to optimise views to the river, the adjoining Moore Park and Gilbert's Pit/ Maryon Park to the south. The building has been designed to provide as many dual aspect homes as possible and ensure there are no north facing single-aspect homes. The building utilises a deck access colonnade which also provides natural surveillance to the adjoining Penhall Gardens and helps to foster a sense of community via social interactions. The dual aspect homes benefit from private terraces with river views. Larger family homes also have corner aspect to benefit from dual orientation.
- 4.23. In the southern building, apartments are accessed off the central courtyard, creating an activated social space. Landscaping elements have been introduced to create subtle transitions from the communal, public courtyard

to private dwellings. These thresholds are also reflected in the facade, which uses depth to create lobbies to each home. The sheltered colonnades also provide access to communal lifts and stairs ensuring natural daylight and ventilation to these spaces. The colonnades allow for social interactions as residents enter and leave their homes. The colonnade also provides natural surveillance to the landscaped courtyard. The deck access typology ensures the majority of homes are dual aspect.

- 4.24. Each core has access to communal amenity – either a roof terrace with extensive views or the landscaped courtyard in the southern building. All homes have their own private amenity in the form of a balcony or terrace.
- 4.25. Affordable rent accommodation is located in core A of the northern building. Wheelchair adaptable flats are located in each block, with access to two lifts per core. The number of homes accessible per floor from each core does not exceed 8.
- 4.26. The tenure and dwelling mix are set out below.

Unit size	Private	Shared Ownership	London Affordable Rent	TOTAL
Studio	9	0	0	9
1-Bedroom	40	0	0	40
2-Bedroom	74	2	9	85
3-Bedroom	11	2	2	15
TOTAL no. units	134	4	11	149
TOTAL no. hab. rooms	355	14	35	404

Car and Cycle parking

- 4.27. In accordance with the London Plan, the residential aspect of the development is car free, with 11 disabled parking bays provided for both the commercial and residential aspects of the scheme in an undercroft car park. 20% of the bays, i.e. 3no., will have active Electric Vehicle Charging Points (ECVPs), and the remaining will be passive.
- 4.28. A total of 274no. cycle parking spaces are provided for residential use, which includes 269no. long-stay spaces within secure storage rooms within the building and 5no. visitor spaces. A total of 69no. spaces (25.2%) are provided as Sheffield stands spaced at 1.2m, 14no. spaces (5.1%) as Sheffield stands spaced at 1.8m to accommodate larger cargo/ exceptional bikes, and the remaining 191no. spaces (69.7%) provided on Josta two tier bicycle stands.
- 4.29. A total of 34 no. cycle parking spaces are provided for flexible retail/community use units, including 3 long-stay spaces and 31 short-stay spaces.
- 4.30. A total of 13 no. cycle parking spaces are provided for the flexible commercial use units, including 11 long-stay spaces and 2 short-stay spaces.

Landscape, Communal amenity and Play space

- 4.31. The public realm includes a green link and a shared servicing yard at ground level. Both spaces respond to the aspiration of the SPD. They provide significant amount of semi-public green space and enable views through

the Site, permeability for pedestrians, as well as active frontage for the scheme. The green link also contributes to play space provision and biodiversity enhancement by linking the wider masterplan area to the existing green infrastructure of the park.

- 4.32. More communal amenity space is provided on the first-floor podium courtyard and on roof terraces. A total of 2778.8 sqm of communal amenity space is provided.
- 4.33. Play space is provided within the green link which has a strong connection with the park and on upper floor roof terraces. All under 5's and older children play space is provided within the application's red line for a total of 1105 sqm.
- 4.34. All homes have been provided with a private balcony or garden to meet the minimum London Plan standards.

Sustainability

- 4.35. The development employs an efficient building fabric, including well insulated walls and highly efficient glazing, efficient systems, air source heat pumps and PV panels are specified to maximise carbon savings for the site.
- 4.36. The development follows the energy hierarchy, heating hierarchy and cooling hierarchy. The development will install ASHP. In addition, the PV system is the largest that the roof can accommodate. The proposals therefore represent the best on site savings. The development will further achieve 'net-zero carbon' through an offset payment in line with the London Plan guidance.

5.0 Planning Policy Framework

- 5.1. This section considers the relevant planning policy framework for the Site and material considerations. The policies considered most relevant to this application are set out below.

Development Plan

- 5.2. The current development plan for the Royal Borough of Greenwich comprises:
- > The London Plan (2021)
 - > Mayor's Housing SPG (2016)
 - > Mayor's Affordable Housing and Viability SPG (2017)
 - > Mayor's Play and Informal Recreation SPG (2012)
 - > RB Greenwich Local Plan: Core Strategy with Detailed Policies (2014)
 - > RB Greenwich Core Strategy with Detailed Policies Proposals Map (2014)
 - > RB Greenwich Greener Greenwich SPD (2014)
 - > RB Greenwich Planning Obligations SPD (2015)
- 5.3. The RBG Site Allocations Local Plan is currently being prepared. Consultation on the Site Allocations Proposed Submission is currently taking place from Monday 8th November to Monday 20th December 2021. This document will support the Core Strategy policies and provide additional detail on specific sites that will help deliver the Core Strategy's priorities. RBG will publish the adopted Site Allocations Local Plan on 20th June 2022. Prior to its adoption, the Saved UDP Site Proposals schedule contains the site allocations for RB Greenwich.
- 5.4. The Charlton Riverside area has been designated as an Opportunity Area in the London Plan, and its development is guided by the Charlton Riverside Masterplan SPD (2017).

Site Designations

- 5.5. The Site is situated within the Charlton Riverside Opportunity Area (as designated in the London Plan) and as such is designated by RB Greenwich as a Strategic Development Location. RB Greenwich has subsequently adopted the Charlton Riverside Masterplan SPD (2017) which sets out the policies covering this area. Within the Masterplan SPD, the Site is allocated for mixed use, with B1 uses at ground floor and residential development on upper floors.
- 5.6. The Site is subject to a site-specific allocation (Ref: C4 Thames Barrier approach and Eastmoor Street) in the adopted RB Greenwich Site Allocations DPD (2016) which allocates the Site for '*Community open space to include playing pitch which could be dedicated for school use, replacing playing pitch north of UTC building if the existing playing pitch is needed for school expansion. Remove Eastmoor Street to enable one area of open space to Westmoor Street. Westmoor Street to be primary access to Thames Barrier. This site is in an area of extreme flood hazard. As a flood storage area, free from inappropriate development, it will be able to act as a protection against flooding to neighbouring site (C5) west of Westmoor Street, enabling the eastern part of site C5 to be developed to its maximum potential.*'
- 5.7. This community open space allocations is clearly in conflict with the mixed-use allocation set out in the aforementioned Masterplan SPD. In this regard, Officers at RBG have confirmed that the Masterplan SPD supersedes the Site Allocations DPD with regard to sites in the Charlton Riverside area.
- 5.8. The RB Greenwich Policies Map shows the Site is located adjacent to Moore Park (to the east) which is designated as a Site of Important Nature Conservation (SINC), Green Chain and Metropolitan Open Land (MOL), and adjacent to the Thames Barrier and Bowater Road Conservation Area, the boundary of which lies across Eastmoor Street.

- 5.9. There are several locally listed assets located in proximity of the Site: the 'Victoria' Public House (ref no. 757) at the corner between Woolwich Road and Eastmoor Street, the Barrier Animal Care Clinic (ref no.32, former 'Lads of the Village' Public House) at the corner between Eastmoor Place and Hardens Manor Way, the red brick wall on Hardens Manor Way, Maryon Park to the south of Woolwich Road, no.25 Westfield Street, and the Lithuanian Christian Church at 696 Woolwich Road.
- 5.10. The Site is within a borough-wide designated Air Quality Management Area (AQMA).

National Planning Policy and Guidance

- 5.11. Regard has been given to the following national planning policy and guidance:

- > The National Planning Policy Framework (2021)
- > The National Planning Practice Guidance (2018, as amended)

NPPF (2021)

- 5.12. The National Planning Policy Framework (NPPF) sets out the Government's planning policies for England, and how these are expected to be applied and taken into consideration in planning decisions. The following policies are considered most relevant to this application.

Chapter 4: Decision-taking

- 5.13. At the heart of the NPPF is a 'presumption in favour of sustainable development'. **Para.11** states that for decision-taking, this means:

a) Approving development proposals that accord with an up-to-date development plan without delay;

or

b) Where there are no relevant development plan policies, or the policies which are most important for determining the application are out-of-date, granting permission unless:

- i) The application of policies in this Framework that protect areas or assets of particular importance provides clear reason for refusing the development proposed; or*
- ii) Any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework as a whole.*

Chapter 5: Delivering a Sufficient Supply of Homes

- 5.14. The NPPF requires a local housing needs assessment to be conducted using the standard methodology and sets out that the Local Planning Authority should apply the weighted test set out in paragraph 11 (d) unless it can demonstrate a five-year supply of deliverable housing sites.
- 5.15. **Para. 60** states: "*to support the Government's objective of significantly boosting the supply of homes, it is important that a sufficient amount and variety of land can come forward where it is needed, that the needs of groups with specific housing requirements are addressed and that land with permission is developed without unnecessary delay.*"
- 5.16. **Para. 63** states: "Where a need for affordable housing is identified, planning policies should specify the type of affordable housing required, and expect it to be met on-site unless:
- a) Off-site provision or an appropriate financial contribution in lieu can be robustly justified; and*
 - b) The agreed approach contributes to the objective of creating mixed and balanced communities".*

Chapter 8: Promoting Healthy and Safe Communities

- 5.17. **Para. 92** states that planning policies and decisions should aim to achieve healthy, inclusive and safe places which promote social interaction. Para. 91c further states that the aim to: “...enable and support healthy lifestyles, especially where this would address identified local health and wellbeing needs – for example through the provision of safe and accessible green infrastructure, sports facilities, local shops, access to healthier food, allotments and layouts that encourage walking and cycling.”
- 5.18. **Para. 93** states that to provide social, recreational and cultural facilities and services to the community, planning policies and decisions should plan positively for shared spaces and community facilities, including sports venues and open space..

Chapter 9: Promoting Sustainable Transport

- 5.19. **Para. 110** states that when considering development proposals, it should be ensured that appropriate opportunities to promote sustainable transport modes can be – or have been – taken up, given the type of development and its location.
- 5.20. **Para. 111** states that development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residential cumulative impacts on the road network would be severe.

Chapter 11: Making Effective Use of Land

- 5.21. **Para. 119** states that planning policies and decisions should promote an effective use of land in meeting the need for homes and other uses, while safeguarding and improving the environment and ensuring safe and healthy living conditions.
- 5.22. **Para. 120.d)** states that planning policies and decisions should: “*promote and support the development of under-utilised land and buildings, especially if these would help meet identified needs for housing where land supply is constrained, and available sites could be used more effectively...*”
- 5.23. **Para. 122** states that planning policies and decisions need to reflect changes in the demand for land and should be informed by regular reviews. It further states: “*Where the local planning authority considers there to be no reasonable prospect of an application coming forward for the use allocated in a plan:*
- a) They should, as part of plan updates, reallocate the land for a more deliverable use that can help to address identified needs; and*
 - b) In the interim, prior to updating the plan, applications for alternative uses on the land should be supported, where the proposed use would contribute to meeting an unmet need for development in the area.*
- 5.24. **Para. 124** states that planning policies and decisions should support development that makes efficient use of land, taking into account:
- a) the identified need for different types of housing and other forms of development, and the availability of land suitable for accommodating it;*
 - b) local market conditions and viability;*
 - c) the availability and capacity of infrastructure and services – both existing and proposed – as well as their potential for further improvement and the scope to promote sustainable travel modes that limit future car use;*

- d) *the desirability of maintaining an area's prevailing character and setting (including residential gardens), or of promoting regeneration and change; and*
- e) *the importance of securing well-designed, attractive and healthy places.*

- 5.25. **Para. 125.c)** states that: “Local Planning Authorities should refuse applications which they consider fail to make efficient use of land, taking into account the policies in the NPPF. In this context, when considering applications for housing, authorities should take a flexible approach in applying policies or guidance relating to daylight and sunlight, where they would otherwise inhibit making efficient use of a site”.

Chapter 12: Achieving well-designed places

- 5.26. **Para.130** states that planning decisions should ensure that developments are visually attractive as a result of good architecture, layout and appropriate landscaping; they should be sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (such as increased densities); and development should optimise the potential of the site to accommodate and sustain an appropriate amount and mix of development and support local facilities and transport networks.

Chapter 16: Conserving and enhancing the historic environment

- 5.27. **Para. 194** states that applicants are required to describe the significance of any heritage assets potentially affected by development proposals, including any contribution made by their setting, and that the level of detail should be proportionate to the assets' importance.
- 5.28. **Para. 197** states that in determining planning applications, LPAs should take account of the desirability of sustaining and enhancing of heritage assets; the positive contribution that conservation of heritage assets can make to sustainable communities including economic vitality; and the desirability of new development making a positive contribution to local character and distinctiveness.
- 5.29. **Para. 199** states that great weight should be given to a heritage asset's conservation when considering the impact of proposed development on the significance of a heritage asset.
- 5.30. **Para. 201** states that where proposed development will lead to substantial harm of a designated heritage asset, LPAs should refuse consent unless it can be demonstrated that the substantial harm is necessary to achieve substantial public benefit that outweighs the harm caused. **Para. 202** considers that where development will lead to less than substantial harm to the significance of a designated heritage asset, this harm should again be weighed against the public benefits.
- 5.31. **Para. 203** states that the effect of development proposals on the significance of non-designated heritage assets should also be taken into account. When considering any effect, a balance judgement will be required having regard to the scale of any harm or loss and the significance of the heritage asset.
- 5.32. **Para. 206** states that LPAs should look for opportunities for new development in conservation areas and within the setting of heritage assets, to enhance or better reveal their significance. Proposals preserving those elements of the setting that make a positive contribution to the asset should be treated favourably

Regional Planning Policy

- 5.1. The London Plan (2021) was published on 2nd March 2021 and sets out the spatial development strategy for Greater London. The London Plan (2021) is a key material consideration in the determination of this planning application.

- 5.2. In addition to the London Plan, a series of SPG documents provide further detailed guidance for the implementation of strategic policies, including the Mayor's Affordable Housing and Viability SPG (2017), the Housing SPG (2016) and, the Mayor's Play and Informal Recreation SPG (2012), being most relevant to this scheme.

The London Plan (2021)

- 5.3. The London Plan provides a strategic plan for London, setting out an integrated economic, environmental, social, and transportation framework for the development and growth of London. The London Plan focuses on optimising brownfield sites, particularly for new housing development, removing the restrictive density matrix and placing the emphasis on design-led sustainable development.
- 5.4. The following strategic policies are considered most relevant to the application:
- 5.5. **Policy GG2 (Making the Best Use of Land)** sets out the criteria that must be addressed in order to create successful sustainable mixed-use places that make the best use of land. This includes:
- > enabling the development of brownfield land, on surplus public sector land, and sites within and on the edge of town centres, as well as utilising small sites;
 - > prioritising sites which are well-connected by existing or planned public transport;
 - > proactively explore the potential to intensify the use of land to support additional homes and workspaces, promoting higher density development, particularly in locations that are well-connected to jobs, services, infrastructure and amenities by public transport, walking and cycling; and
 - > applying a design-led approach to determine the optimum development capacity of sites.
- 5.6. **Policy GG4 (Delivering the Homes Londoners Need)** sets out the criteria that must be addressed in order to create a housing market that works better for all Londoners. This includes:
- > ensuring that more homes are delivered; and
 - > supporting the delivery of the strategic target of 50 per cent of all new homes being genuinely affordable.
- 5.7. **Policy D3 (Optimising Site Capacity through a Design-led Approach)** requires all development to make the best use of land by following a design-led approach. This policy contains a large number of design principles which developments should incorporate, which whilst not repeated verbatim, are summarised below:
- > Enhance the local context by delivering buildings which respond to local distinctiveness and character through their layout, orientation, scale, appearance and shape, whilst having regard to the existing and emerging street hierarchy, building types, forms and proportions;
 - > Be of high design quality, using attractive and robust materials;
 - > Be street-based with a clearly defined public and private environment;
 - > Deliver appropriate outlook, privacy and amenity.
- 5.8. **Policy D4 (Delivering Good Design)** states that:
- > Design analysis and development certainty – Masterplans design codes should be used to help bring forward development and ensure it delivers high quality design and placemaking. Where appropriate, visual, environmental and movement modelling/assessments should be undertaken to analyse potential design options for an area, site or development proposal.
 - > Design scrutiny - Design and access statements submitted with development proposals should demonstrate that the proposal meets the design requirements of the London Plan. The design of

development proposals should be thoroughly scrutinised by borough planning, urban design, and conservation officers. Applicants should make use of the design review process to assess and inform design options early in the planning process. The format of design reviews for any development should be agreed with the borough and comply with the Mayor's guidance on review principles, process and management.

5.9. **Policy D5 (Inclusive Design)** states that that development proposal should achieve the highest standards of accessible and inclusive design. They should

- 1) be designed taking into account London's diverse population
- 2) provide high quality people focused spaces that are designed to facilitate social interaction and inclusion
- 3) be convenient and welcoming with no disabling barriers, providing independent access without additional undue effort, separation or special treatment
- 4) be able to be entered, used and exited safely, easily and with dignity for all
- 5) be designed to incorporate safe and dignified emergency evacuation for all building users. In all developments where lifts are installed, as a minimum at least one lift per core (or more subject to capacity assessments) should be a suitably sized fire evacuation lift suitable to be used to evacuate people who require level access from the building.

5.10. **Policy D6 (Housing Quality and Standards)** includes standards for private internal and external space, which can be summarised as follows:

Private internal space:

- > A dwelling with 2 or more bedspaces must have at least one double bedroom that is at least 2.75m wide.
- > Every other additional double bedroom must be at least 2.55m wide.
- > One bedspace single bedroom must have a floor area of 7.5sqm and be at least 2.15m wide.
- > The minimum floor to ceiling height must be 2.5m for at least 75% of the Gross Internal Area of each dwelling.

Private outside space:

- > A minimum of 5sqm of private outdoor space should be provided for 1-2 person dwellings; an extra 1sqm should be provided for each additional occupant.
- > Minimum depth and width for private external spaces (e.g. balconies) should be 1.5m.

5.11. It also requires: high quality design; the maximisation of dual aspect units; and the provision of sufficient daylight and sunlight to new and surrounding housing, whilst avoiding overheating, minimising overshadowing and maximising the usability of outside amenity space.

5.12. **Policy D7 (Accessible Housing)** states that residential development must ensure that:

- 1) At least 10% of new dwellings should meet Building Regulation requirement M4(3) 'wheelchair user dwellings' designed to be wheelchair accessible users.
- 2) All other dwellings should meet Building Regulation requirement M4(2) 'accessible and adaptable dwellings'.

- 5.13. **Policy D12 part B (Fire Safety)** states that all major development proposals should be submitted with a Fire Statement, which is an independent fire strategy, produced by a third party, suitably qualified assessor. The statement should detail how the development proposal will function in terms of:
- 1) the building's construction: methods, products and materials used, including manufacturers' details
 - 2) the means of escape for all building users: suitably designed stair cores, escape for building users who are disabled or require level access, and associated evacuation strategy approach
 - 3) features which reduce the risk to life: fire alarm systems, passive and active fire safety measures and associated management and maintenance plans
 - 4) access for fire service personnel and equipment: how this will be achieved in an evacuation situation, water supplies, provision and positioning of equipment, firefighting lifts, stairs and lobbies, any fire suppression and smoke ventilation systems proposed, and the ongoing maintenance and monitoring of these
 - 5) how provision will be made within the curtilage of the site to enable fire appliances to gain access to the building
 - 6) ensuring that any potential future modifications to the building will take into account and not compromise the base build fire safety/protection measures.
- 5.14. **Policy H1 (Increasing Housing Supply)** sets out the housing targets for London Boroughs over the next 10 years, with a strategic target to deliver 522,870 homes across London in total. The Borough of Hounslow has been assigned with a 10-year target of 17,820 (or 1,782 homes per annum) over 2019/20 - 2028/29. To optimise the potential for housing delivery, the Plan promotes delivery of housing on small sites, brownfield land, in areas with PTAL equal or superior to 3, on car park and low-density retail park sites, on public sector owned sites, and on identified industrial sites.
- 5.15. **Policy H4 (Delivering Affordable Housing)** states the strategic target is for 50% of all new homes delivered across London to be genuinely affordable. Specific measures to achieve this aim include:
- > requiring major developments which trigger affordable housing requirements to provide affordable housing through the threshold approach;
 - > using grant to increase affordable housing delivery beyond the level that would otherwise be provided;
 - > all affordable housing providers with agreements with the Mayor delivering at least 50% affordable housing across their development programme, and 60% in the case of strategic partners.
 - > public sector land delivering at least 50% affordable housing on each site and public sector landowners with agreements with the Mayor delivering at least 50% affordable housing across their portfolio;
 - > industrial land appropriate for residential use in accordance with Policy E7 Industrial intensification, co-location and substitution, delivering at least 50% affordable housing where the scheme would result in a net loss of industrial capacity.
- 5.16. **Part B of Policy H4** states that affordable housing should be provided on site. Affordable housing must only be provided off-site or as a cash in lieu contribution in exceptional circumstances.
- 5.17. **Policy H5 (Threshold Approach to Applications)** expands the threshold approach to viability first introduced by the Affordable Housing & Viability SPG (2017), which applies to all major development proposals which trigger affordable housing requirements. The threshold level of affordable housing on gross residential development is set at:

- 1) a minimum of 35%; or
- 2) 50% for public sector land where there is no portfolio agreement with the Mayor; or
- 3) 50% for Strategic Industrial Locations, Locally Significant Industrial Sites and Non-Designated Industrial Sites appropriate for release (as per Policy E7) where the scheme would result in a net loss of industrial capacity.

- 5.18. **Part C of Policy H5** states that to follow the Fast Track Route, applications must: meet or exceed the relevant threshold without public subsidy; be consistent with the relevant tenure split; meet other relevant policy requirements and obligations; and, demonstrate that they have taken account of the 50% strategic target for London and have sought grant to increase the level of affordable housing.
- 5.19. **Part D of Policy H5** states that developments which provide 75% or more affordable housing may follow the Fast Track Route where the tenure mix is acceptable to the borough or the Mayor where relevant.
- 5.20. **Part E of Policy H5** states that fast tracked applications are not required to provide a viability assessment at application stage. However, an Early-Stage Review will be triggered if an agreed level of progress on implementation is not made within two years of the permission being granted (or a period agreed by the Borough), to ensure an applicant fully intends to build out the permission.
- 5.21. **Part F of Policy H5** states that where an application does not meet the requirements set out in Part C it must follow the Viability Tested Route. This requires detailed supporting viability evidence to be submitted to be scrutinised by the LPA's independent Viability Assessor and the GLA. Viability Tested schemes are subject to Early and Late-Stage Viability Reviews. For larger multi-phase schemes, mid-term reviews are also required.
- 5.22. **Policy H6 (Affordable Housing Tenure)** states that the following split of affordable tenures should be applied:
- > Minimum 30% low cost rented homes (London Affordable Rent or Social Rent)
 - > Minimum 30% intermediate products (London Living Rent and London Shared Ownership)
 - > Remaining 40% to be determined by the borough as low rented homes or intermediate.
- 5.23. **Policy H10 (Housing Size Mix)** states that schemes should generally consist of a range of unit sizes, and to determine the appropriate mix, applicants and decision-makers should have regard to:
- > Robust local evidence of need;
 - > The requirement to deliver mixed and inclusive neighbourhoods;
 - > The need to deliver a range of unit types at different price points across London;
 - > The mix of uses in the scheme;
 - > The range of tenures in the scheme;
 - > The nature and location of the site, with a higher proportion of 1- and 2-bed units generally more appropriate in locations which are closer to a town centre or station with higher PTAL;
 - > The aim to optimise housing potential of sites;
 - > The ability of new development to reduce pressure on conversion, sub-division and amalgamation of existing stock; and,
 - > The role of 1- and 2-bed units in freeing up family housing.
- 5.24. **Policy G5 (Urban Greening)** states that major development should contribute to the greening of London by including urban greening as a fundamental element of site and building design, and by incorporating measures such as high-quality landscaping (including trees), green roofs, green walls and nature-based sustainable drainage.
- 5.25. **Part B of Policy G5** sets out a recommended target score of 0.4 for developments that are predominantly residential, and a target score of 0.3 for predominately commercial development (excluding B2 and B8 uses).

- 5.26. **Policy S4 (Play and Informal Recreation)** requires at least 10 sqm of play space to be provided per child generated by the residential element of the development. The policy requires that the play space is overlooked to enable passive surveillance and is not segregated by tenure. The supporting text to this policy states that formal play provision should normally be made on-site, and that when development is to be phased, there should be an early implementation of play space.
- 5.27. The supporting text to Policy S4 further states that off-site provision, secured by an appropriate financial contribution may be acceptable where it can be demonstrated that it addresses the needs of the development whilst continuing to meet the needs of existing residents. It states that this is likely to be more appropriate for facilities for older children and should usually be within 400m of the development and accessible via a safe route
- 5.28. **Policy SI1 (Improving Air Quality)** states that development proposals should not:
- a) lead to further deterioration of existing poor air quality
 - b) create any new areas that exceed air quality limits, or delay the date at which compliance will be achieved in areas that are currently in exceedance of legal limits
 - c) create unacceptable risk of high levels of exposure to poor air quality.

In order to meet the above requirements, as a minimum:

- a) development proposals must be at least Air Quality Neutral
 - b) development proposals should use design solutions to prevent or minimise increased exposure to existing air pollution and make provision to address local problems of air quality in preference to post-design or retro-fitted mitigation measures
 - c) major development proposals must be submitted with an Air Quality Assessment. Air quality assessments should show how the development will meet the requirements of B1
 - d) development proposals in Air Quality Focus Areas or that are likely to be used by large numbers of people particularly vulnerable to poor air quality, such as children or older people should demonstrate that design measures have been used to minimise exposure.
- 5.29. **Policy SI2 (Minimising Greenhouse Gas Emissions)** states that all major development (both residential and non-residential) should achieve zero-carbon, with a minimum on-site reduction of 35% beyond Building Regulations. The residential element should aim to achieve 10% of the reduction and non-residential should aim to achieve 15% via energy efficiency measures. Where it is clearly demonstrated that zero-carbon cannot be fully achieved on-site, any shortfall should be provided off-site or through a cash in lieu contribution to the borough.
- 5.30. **Policy SI3 (Energy Infrastructure) Part D** states major development proposals within Heat Network Priority Areas should have a communal low-temperature heating system:
- 1) the heat source for the communal heating system should be selected in accordance with the following heating hierarchy:
 - a) connect to local existing or planned heat networks
 - b) use zero-emission or local secondary heat sources (in conjunction with heat pump, if required)

c) use low-emission combined heat and power (CHP) (only where there is a case for CHP to enable the delivery of an area-wide heat network, meet the development's electricity demand and provide demand response to the local electricity network)

d) use ultra-low NOx gas boilers

2) CHP and ultra-low NOx gas boiler communal or district heating systems should be designed to ensure that they meet the requirements of Policy SI 1 Improving air quality

3) where a heat network is planned but not yet in existence the development should be designed to allow for the cost-effective connection at a later date.

5.31. **Policy SI5 (Water Infrastructure)** states that development proposals should:

- > minimise the use of mains water in line with the Optional Requirement of the Building Regulations (residential development), achieving mains water consumption of 105 litres or less per head per day (excluding allowance of up to five litres for external water consumption).
- > incorporate measures such as smart metering, water saving and recycling measures, including retrofitting, to help to achieve lower water consumption rates and to maximise future-proofing.
- > seek to improve the water environment and ensure that adequate wastewater infrastructure capacity is provided.
- > take action to minimise the potential for misconnections between foul and surface water networks.

5.32. **Policy T4 (Assessing and Mitigating Transport Impacts)** states that:

- A. Development proposals should reflect and be integrated with current and planned transport access, capacity and connectivity.
- B. Transport assessments/statements should be submitted with development proposals to ensure that impacts on the capacity of the transport network at the local, network-wide and strategic level, are fully assessed. Transport assessments should focus on embedding the Healthy Streets Approach within, and in the vicinity of, new development. Travel Plans, Parking Design and Management Plans, Construction Logistics Plans and Delivery and Servicing Plans will be required having regard to Transport for London guidance.
- C. Where appropriate, mitigation, either through direct provision of public transport, walking and cycling facilities and highways improvements or through financial contributions, will be required to address adverse transport impacts that are identified.
- D. Where the ability to absorb increased travel demand through active travel modes has been exhausted, existing public transport capacity is insufficient to allow for the travel generated by proposed developments, and no firm plans and funding exist for an increase in capacity to cater for the increased demand, planning permission will be contingent on the provision of necessary public transport and active travel infrastructure.
- E. The cumulative impacts of development on public transport and the road network capacity including walking and cycling, as well as associated effects on public health, should be taken into account and mitigated.
- F. Development proposals should not increase road danger.

5.33. **Policy T5 (Cycling)** sets the minimum cycle parking standards, provided below.

Use Class	Long-Stay	Short-Stay
C3 - Dwellings	• 1 space per studio or 1 person 1-bedroom dwelling	• 5 to 40 dwellings: 2 spaces

	<ul style="list-style-type: none"> • 1.5 spaces per 2 person 1-bedroom dwelling • 2 spaces per all other dwellings 	<ul style="list-style-type: none"> • Thereafter: 1 space per 40 dwellings
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- 5.34. **Policy T6 (Car Parking)** emphasises car-free should be the starting point for all proposals in places that are (or planned to be) well-connected by public transport, with developments elsewhere designed to provide the minimum necessary parking ('car-lite'). Whilst car-free development has no general parking, it should still provide residential disabled persons parking (3% of dwellings).

Mayor's Housing SPG (2016)

- 5.35. The Housing SPG was adopted in March 2016 (and updated in May 2016), setting out further guidance on the interpretation and implementation of the new housing standards adopted in the London Plan.
- 5.36. **Standard 4** of the Mayor's SPG states that where communal open space is provided, development proposals should demonstrate that the space: is overlooked by surrounding development; is accessible disabled people including people who require level access and wheelchair users; is designed to take advantage of direct sunlight; and, has suitable management arrangements in place.
- 5.37. **Standard 9** states that the distance from the accessible car parking space of standard 18 to the home or to the relevant block entrance or lift core should be kept to a minimum and should be preferably level or where level is not possible, gently sloping (1:60 – 1:20) on a suitable ground surface.
- 5.38. **Standard 11** requires that 90 per cent of new build housing should meet Building Regulation requirement M4(2) 'accessible and adaptable dwellings' with the remaining 10 per cent meeting Building Regulation requirement M4(3) 'wheelchair user dwellings'.
- 5.39. **Standard 12** requires that each core should be accessible to generally no more than eight units on each floor and **Standard 15** requires all dwellings entered at the seventh floor (eighth storey) and above should be served by at least two lifts.
- 5.40. **Standard 16** states it is desirable that every wheelchair user dwelling is served by more than one lift.
- 5.41. **Standards 26 and 27** set out the private outdoor amenity space standards. Standard 26 requires a minimum of 5sqm of private outdoor space for 1-2-person dwellings and an extra 1sqm for each additional occupant. Standard 27 requires a minimum depth and width for all balconies and other private external spaces of 1500mm.
- 5.42. **Standard 28** requires that design proposals should demonstrate how habitable rooms within each dwelling are provided with an adequate level of privacy in relation to neighbouring properties, the street, and other public spaces.
- 5.43. **Standard 29** states that developments should minimise the number of single aspect dwellings. Single aspect dwellings that are north facing or exposed to noise levels above which significant adverse effects on health and quality of life occur, or which contain three or more bedrooms should be avoided.
- 5.44. **Standard 37** states that dwellings should be designed to ensure that a maximum of 105 litres of water is consumed per person per day in line with the optional requirement of Part G.

- 5.45. **Standard 39** states new development should incorporate Sustainable Urban Drainage Systems and green roofs where practical with the aim of achieving a greenfield run-off rate, increasing biodiversity and improving water quality. Surface water run-off is to be managed as close to source as possible.

Mayor's Affordable Housing and Viability SPG (2017)

- 5.46. The Mayor's Affordable Housing and Viability SPG sets out a preferred tenure split for affordable provision as 30% low cost rent, 30% intermediate and 40% to be determined by the LPA. While the Mayor has set out a preference, the borough's own affordable housing target remains the starting point for consideration of an application. The SPG does not require any more affordable housing than the target set by each borough within their local development framework. The SPG also introduced a fast track viability route for schemes as follows:
- > Fast Track Route: for applications that meet or exceed 35% affordable housing on site, without public subsidy, achieving the prescribed tenure split and meeting other planning requirements and obligations to the satisfaction of the LPA and GLA, where relevant.
 - > Viability Tested Route: for applications that do not provide 35% without a public subsidy or applications which propose off-site or cash-in-lieu contributions. These schemes will require viability information to be submitted and this will be scrutinised by the LPA and GLA, where relevant.
- 5.47. For schemes using the Fast Track Route there will be an early viability review, triggered if an agreed level of progress in delivering units is not made within two years (or otherwise agreed timeframe) from the grant of planning permission. In the event viability has improved since the application stage, additional onsite affordable housing would be required.
- 5.48. For schemes using the Viability Tested Route there will be both an early and a late viability review. The late review occurs at the point when 75% of the units are sold or let. In the event viability has improved since the application stage and surplus profit is identified, this should be split 60/40 between the borough and the developer.

Local Planning Policy

- 5.49. The Royal Borough of Greenwich Core Strategy and Detailed Policies document (adopted in 2014) sets out the vision, objectives, strategy and policies for the borough. It is complemented by SPD documents, including the Greener Greenwich SPD (2014) and Planning Obligations SPD (2015), which are relevant for the Site.

RB Greenwich Local Plan: Core Strategy with Detailed Policies (2014)

- 5.50. **Policy CH1 (Cohesive Communities)** states that all development should help create and maintain cohesive communities, and the development of new and improved community facilities will be supported. These facilities are to be accessible and meet an identified need, with schools and places of education particularly important.
- 5.51. **Policy H1 (New Housing)** of the RB Greenwich Local Plan sets the housing targets for the plan period (2013 – 2028), to provide a minimum of 38,925 net additional dwellings, equating to 2,595 new homes per annum.
- 5.52. **Policy H2 (Housing Mix)** states that a mix of housing types and sizes will be required in all developments. The exact mix on each site will vary according to the location of the development and the character of the surrounding area and will be affected by factors such as accessibility to public transport. It is expected that any development in the Borough will contribute towards meeting the objectively assessed housing need set out below:

No. of beds	Total (%)	Market (%)	Intermediate (%)	Social Rent (%)
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1-2	48.8	56	18	26
3-4	51.2	29	31	40

- 5.53. **Policy H3 (Affordable Housing)** states that developments of 10 or more homes will be required to provide a minimum of 35% affordable housing. The precise percentage, distribution and type of affordable housing will be determined by the particular circumstances and characteristics of the site and of the development, including financial viability. Within this affordable housing provision, the preferred tenure split is 70% social or affordable rent, and 30% intermediate housing. Further to a Council decision of 29 October 2015, RBG now applies the Greater London Authority's household income bands for intermediate housing.
- 5.54. **Policy H5 (Housing Design)** and **Policy DH1 (Design)** state that new residential development will be expected to achieve a high quality of housing design and an integrated environment. Policy H5 states that new dwellings should be dual aspect where possible, avoiding single aspect north-facing units, be safe and secure, accessible, and make adequate provision for external amenity space and waste recycling.
- 5.55. **Policy H5** further states that family sized units should normally have direct access to a private garden and should be located close to amenities, play areas and schools, and that residential developments of 25 units or more should be designed to have 10% of the total dwellings as wheelchair accessible. The Greenwich Wheelchair Site Brief requires that wheelchair housing is evenly distributed across the site and a range of unit sizes be provided.
- 5.56. **Policy H(e) (Children's Play Areas)** states that in residential developments that include over 50 units of family housing, suitably equipped and well-designed children's play areas are required for different age groups.
- 5.57. **Policy DH1 (Design)** requires that non-residential buildings in major developments achieve a BREEAM rating of 'Excellent'.
- 5.58. **Policy E1 (Carbon Emissions)** states all major development proposals will require an energy assessment and that carbon emissions will be reduced in accordance with the Mayor's Energy Hierarchy.
- 5.59. **Policy E(a) (Pollution)** states that planning permission will not normally be granted where a proposed development or change of use would generally have a significant adverse effect on the amenities of adjoining occupiers especially where proposals would be likely to result in the unacceptable emission of noise, light, vibrations, odours, fumes, dust, water and soil pollutants or grit.
- 5.60. **Policy E(c) (Air Pollution)** states that proposals with the potential to result in any significant impact on air quality will be resisted unless measures to minimise the impact of air pollutants are included. Such planning applications should be accompanied by an assessment of the likely impact of the development on air quality.
- 5.61. **Policy IM4 (Sustainable Travel)** states that developments must not go above those maximum parking standards set out in the London Plan, and where appropriate should go below these.
- 5.62. **Policy IM(c) (Parking Standards)** states that developments supported by a high level of public transport accessibility and within Controlled Parking Zones should be 'car-free'. Development in areas of on-street parking stress should be 'car-capped'. The Council will also strongly encourage contributions to car clubs and pool car schemes in place of private parking in new developments across the Borough. Developments must meet the minimum standards for cycle parking as set out in the London Plan.

RB Greenwich Greener Greenwich SPD (2014)

- 5.63. This SPD builds on the environmental and energy policies of the RBG Core Strategy and provides further guidance on the level of detail the Borough expects to be submitted with a planning application regarding energy, water, biodiversity, materials, waste, flood risk and pollution.

Charlton Riverside Masterplan SPD (2017)

- 5.64. The Charlton Riverside Masterplan was published in June 2017. This document is intended to provide guidance for developers as to what is expected at the planning application stage. The document sets out the RB Greenwich's objectives and Vision for Charlton Riverside and is a material consideration for any planning application submitted within the Charlton Riverside area.

- 5.65. The SPD sets out the following vision for Charlton Riverside;

‘...Charlton Riverside is a significant employment site in the Royal Borough of Greenwich. Its rich industrial heritage will shape a series of new neighbourhoods, integrating residential development with modern industrial, office and creative employment opportunities. Incoming residential development will be characterised by medium-rise housing and family homes. A network of streets and open spaces will reflect the historical pattern of paths and boundaries, creating a healthy environment that encourages walking over using a car, where children can play outside and residents and visitors can enjoy a varied and attractive selection of leisure, recreation and social activities. Development will be supported by new schools and facilities. Improvements to Charlton Station and Woolwich Road will help to integrate new development with the rest of Charlton...’.

- 5.66. The vision is underpinned by the following principles:

- > An additional 4,400 jobs (over and above the existing employment);
- > Integration of employment uses into new development;
- > An additional 5,000-7,500 homes (based on a more detailed assessment of the site's capacity);
- > 50% family housing and 35% affordable housing;
- > Low to medium rise development (3-6 storeys);
- > Transport networks that reflect historic routes;
- > In future years, in the Strategic Industrial Location (SIL) area, subject to appropriate relocation, de-designation and new transport infrastructure being provided some taller buildings may be permitted to transition into the Millennium Village and Peninsula;
- > Respect for heritage assets; and
- > Creative design response to the need for flood protection.

- 5.67. The SPD outlines that new development must positively contribute to the following objectives; residential diverse, economically active, connected and accessible, integrated and appropriate for all stages of life, well designed, sustainable and resilient, and viable and deliverable.

- 5.68. The SPD allocates the Site for mixed use, with B1 uses at ground floor and residential development on upper floors.

- 5.69. Within the SPD there is an aspiration for an east-west route to be delivered from Eastmoor Place though to Mirfield Street, although only an indicative alignment is shown.

6.0 Planning Considerations

- 6.1. This section considers the potential for residential redevelopment on the Site, against the adopted and emerging policies.

Principle of Development

- 6.2. The Site is currently occupied by a number of single storey industrial warehouses and open storage space used for storage, vehicle repair/car breaker, scrap metal yard/dealer, and metal castings (Use Classes B2 / B8 / Sui Generis). The existing buildings provide 3,387 sqm GEA of floorspace, or 2,354 sqm GIA. The proposal is for mixed-use redevelopment including 740.6 sqm flexible employment floorspace, 555 sqm flexible retail and community uses, and 149no. new residential dwellings.
- 6.3. The Charlton Riverside Masterplan SPD (2017) allocates the site for mixed-use, with B1 uses on the ground floor and residential development on upper floors.
- 6.4. Whilst the proposal will result in the loss of the existing B2/B8/sui generis uses on the Site, the re-provision of 1,295.6 sqm (GIA) of flexible commercial and retail/community uses floorspace will result in an increase in the number of potential employment opportunities.
- 6.5. Using the figures set out in the HCA's Employment Density Guide (November 2015), it has been calculated that the scheme will deliver a significant uplift in local employment. While the site currently only provides around 20 to 30 local jobs the proposal will provide about 53 jobs. The scheme will provide minimum 23 additional FTE roles than the site currently provides. The scheme will also create 87 construction jobs during the development phase of the project. Further details are included in the Employment Statement and Relocation Strategy.
- 6.6. In addition, the proposed development will make a significant contribution to addressing the need for housing identified in both the London Plan (Policy H1), RBG Core Strategy (Policy H1) and the SPD contributing 149 new high-quality homes.

Design

Siting and layout

- 6.7. The proposal includes two distinct buildings, a L-shape northern building and a U-shape southern building. The buildings are separated by a generous new green link orientated east-west linking Westmoor Street to Moore Park. The northern building line follows the layout of the new East-West Route proposed by the Masterplan SPD providing an active frontage to both the new road to the north and the green link to the south. The southern building is arranged around a first-floor courtyard that sits above the ground floor parking. It provides active frontages to the green link, Moore Park, Westmoor Street and the yard space adjoining the neighbouring site to the south.
- 6.8. Dual aspect apartments have been provided where possible (76% of homes), with single aspect limited to smaller dwellings. These number of dual aspect homes has been maximised within the scheme factoring in the site-specific constraints that exist within the site.

Height, scale and massing

- 6.9. The proposed development is composed of two buildings with varied heights from 1 to 8 storeys, with the tallest element situated to the north-west of the Site marking the junction between Westmoor Street and the new East-West route. The lowest element is the landscaped courtyard at the centre of the southern building, providing communal amenity space and play space on podium above ground floor parking.

- 6.10. The massing has been developed to respond to the emerging context of the Charlton Riverside Area. The proposed heights of the buildings have dominated the design process, in particular the additional height proposed for the site in comparison to the heights specified within the SPD. The building heights have been reduced as part of this iterative process, including following Scheme A's refusal. The proposed heights have been assessed to ensure amenity spaces, the green link and the courtyard space, receive adequate daylight and sunlight.
- 6.11. The massing of the northern building creates a defined taller block at the intersection of Mirfield Street and Westmoor Street, a key focal point along Mirfield Street the future East-West Route. The massing lowers from 8 storeys to 6 storeys towards Moore Park.
- 6.12. The southern building sits a minimum of 14 metres from the northern building increasing to 21 metres across the green link. A large south facing courtyard, 23.5 metres wide is formed at first floor level. The landscaping of the courtyard creates an intimate, relaxing feel in contrast to the external city and river views from the homes. The landscaping of the courtyard creates an intimate, relaxing feel in contrast to the external city and river views from the homes. It is surrounded by three blocks of 4, 5 and 6 storeys in height.
- 6.13. The proposed composition of varying heights has been closely informed by daylight and sunlight testing throughout the process to ensure any impact on any future surrounding residential properties is minimised, as well as any sense of overbearing. This has included several remodelling of the massing and shifting of the maximum height to the north-west corner to highlight the new local hub.
- 6.14. Whilst it is acknowledged that the proposed storey heights exceed the range (3-4 storeys) referred to within the Charlton Riverside SPD, this variance needs to be considered in the context of the wider benefits that flow from the scheme. These changes are evident and can be supported for the following reasons:
- > This level of development is deliverable and achievable in a way that supports the human-scale in the locality, whilst ensuring this vitally important strategic brownfield site maximises its potential in this location.
 - > Viability is a key factor in terms of delivery, and it should be noted that there are a significant number of benefits that flow from this scheme, which align with the need to provide a sufficient critical mass on the site which allows these benefits to be provided. These include:
 - The delivery of 10% affordable homes (the maximum viable) and 10% family homes
 - The provision of a significant level of commercial space to support a mix of uses across the site.
 - High quality landscaping across the site, including significant improvements to the riverfront and the opening up of this area to future and existing residents in the locality.
 - Provision of technical acoustic solutions within the scheme that will ensure that residents are not adversely affected by traffic noise along Westmoor Street.
 - Highly advanced sustainability features.
 - > The scheme has also had to respond to its flood zone status and a suggested three-storey datum height would be very difficult, if not impossible to achieve in this location.
 - > The scheme is carefully designed to ensure that the wider aspirations of the Charlton Riverside area are met and not fettered by the development coming forward.
- 6.15. Overall, the scale of development is considered to be entirely appropriate in this location to ensure that all the above-mentioned benefits can be provided within the scheme and that we can demonstrate that we comply with National Planning Policy which strongly emphasises the need for best and most efficient use of brownfield land. This is particularly important in an area such as Charlton Riverside, which is an Opportunity Area in the London Plan and is looking to provide a substantial number of new homes and flexible commercial floorspace. This cannot and will not be achieved if the critical mass on site is not achieved to subsidise the numerous

components, which allow for a high standard of design and quality of placemaking that are promoted within the scheme.

- 6.16. As is explained further below, the massing of the proposed scheme responds appropriately to the surrounding heritage assets, such that it will have a positive impact on the settings of the locally listed buildings.
- 6.17. It should be noted that the proposed reduction in height and unit numbers from the 188-units Scheme A (LPA Ref. 20/1924/F) results in a reduced ability of the proposals to deliver affordable homes with a provision of 10% of total units number. This is further demonstrated within the Financial Viability Appraisal submitted in support of this application.

Density

- 6.18. The London Plan (2021) is clear that the optimum density of a development should be design-led, and defined with other factors such as context, layout, residential quality, existing and planned public transport, and capacity of surrounding infrastructure taken into consideration, in a move away from the density matrix. However, figure 6.4 of the Charlton Riverside Masterplan SPD shows an indicative density range of 151 – 200 u/ha for the Site.
- 6.19. The residential density of the proposed scheme is 252 u/ha (or 685 hr/ha). Whilst this is slightly greater than the figure in the SPD, the proposed development is considered to be appropriate for its context, having regard to the emerging schemes, infrastructure, access to services and amenities within the Charlton Riverside masterplan, the provision of high-quality residential accommodation with significant amenity space, and the contribution to the aspiration of the SPD for a sustainable mixed-use community.
- 6.20. It should be noted that any further reduction in density of the proposed development would result in a reduced ability of the proposals to deliver affordable homes.

Materials

- 6.21. The proposed materials seek to ensure a contemporary look while preserving the local industrial character.
- 6.22. The proposed buildings have a simple palette of materials drawing on the industrial heritage of the area. The predominant building material proposed is brickwork in two complimentary tones and an additional glazed brick for use within the commercial frontages.
- 6.23. Grey brickwork is used to form the commercial plinth to the buildings providing unity to the ground floor pedestrian areas. Blue glazed brickwork highlights residential and commercial entrances at ground floor. The light buff brickwork highlights most of the residential accommodation on upper floors. The grey brickwork is also used on upper floors to introduce variation and visual interest and to highlight the 8-storey block.
- 6.24. Metal cladding is also used punctually to create layering and depth to the facades. Windows are proposed as double glazed PPC aluminium black metal. Balustrades to balconies are proposed as PPC dark grey metal railings with copper handrail.
- 6.25. A material schedule and CGIs illustrating the visual impact of the proposed materials is included within the accompanying Design and Access Statement.

Heritage, Townscape and Visual Impact

- 6.26. A Heritage, Townscape and Visual Impact Assessment (HTVIA), prepared by Lichfields, has been submitted in support of this application. It provides an analysis of the potential heritage, townscape and visual effects arising from the proposed development.

6.27. In summary, the HTVIA makes the following conclusions.

Townscape Effects

- 6.28. The analysis of existing townscape character found that the development lies within the Westmoor Industrial Estate TCA which has a low sensitivity to change due to a combination of its existing degraded industrial character and the promotion of regeneration across the area through the Charlton Riverside Masterplan and Opportunity Area. The area to the east of the site lies within the Moore Park/ Maryon Park TCA which has a medium sensitivity to change.
- 6.29. The proposals would replace an existing array of unattractive, run-down industrial buildings with a mixed-use redevelopment that would regenerate the site and would positively influence the immediately surrounding area.
- 6.30. There would be a major beneficial effect on the townscape character of the site itself, a minor beneficial effect on the more extensive Westmoor Industrial TCA and a moderate beneficial effect on the adjacent Moore Park/ Maryon Park TCA. There would be minor beneficial effects on the Woolwich Road and Westminster Industrial Estate TCAs and negligible effects on the Bowater Road and Bugsby's Reach/ River Thames TCA due to the screening provided by existing buildings and vegetation.
- 6.31. The scheme would deliver a number of key aspirations of the Masterplan SPD, including facilitating the future delivery of an East-West route across the area, enhancing permeability through the site with two new connections to Thames Barrier Park and the delivery of part of the local centre envisaged at the Westmoor/ Mirfield Street junction. Both the quantum and quality of public realm would be improved with wider, more generous pavements incorporating street trees and an area of amenity space. New active frontages would be provided to new and existing routes that are overlooked by new homes within the development. The introduction of connections through the site, active frontages and natural surveillance would encourage greater use of the park and enhance the perception of safety for people moving through the area.
- 6.32. The scale and massing of the proposal, although taller than that envisaged in the Masterplan SPD, has been reduced through negotiations on the application and is considered acceptable in townscape terms on this site for a number of reasons. It would provide appropriate legibility to the local centre and fit with the proposed hierarchy of route (the east-west link, Westmoor Street and the park all being major routes through the area). The scale of the building has been successfully articulated by the massing and the detailed design including steps in height that would create a varied and interested roofscape and reduce the apparent scale in street level views.
- 6.33. The materials, articulation of the façades and details allude to positive aspects of the historic character of the area, would create an interesting streetscape and an appropriate sense of place that is appropriate to the robust historic industrial context and also meet the aspirations for a high quality mixed use Charlton Riverside area.

Visual Effects

- 6.34. The assessment has found that the proposed development would not result in any harmful visual effects on the surrounding townscape. There would be a range of beneficial effects that would include moderate to major beneficial visual effects on local views from Thames Barrier Park as a result of the new buildings defining high quality frontage.
- 6.35. There would be a moderate to major beneficial effect on people on the Green Chain Walk passing the junction of Eastfield Place and Westfield Street where the proposals would be seen in the context of the frontage of the locally listed former public house. From this location the scheme would replace an existing degraded industrial

site with a high-quality frontage that is angled away to reveal the new east-west connection and the local centre. The ground floor café would provide visual interest and a focal point at the junction and the form, massing, materials and articulation would ensure that the buildings would provide an appropriately scaled and detailed frontage.

- 6.36. Looking south along Westmoor Street, there would be a minor to moderate beneficial effect with a positive new frontage, appropriate to the aspirations of the SPD in marking the local centre and key route through the area. Approaching the site from the Thames Path, the development would only be perceptible in winter through the trees, when the tallest part of the scheme will mark the location of the local centre and enhance legibility.
- 6.37. There would be nil or negligible effects on views from the wider surroundings area due to the level of screening.

Heritage Effects

- 6.38. The Heritage Assessment has established the effects of the proposed redevelopment on the significance of the heritage assets identified for assessment in accordance with the requirement set out at paragraph 196 of the National Planning Policy Framework. It has established the significance of the identified heritage assets, the contribution of their setting to their significance and the role of the site in the setting.
- 6.39. The assessment has shown that, overall, the proposed scheme would result in positive effects on the settings of the surrounding heritage assets. Where the proposals would form a new element in the visual settings of heritage assets, the design and materials would ensure that it would respond to the historic and emerging character of the surrounding area and the articulation would ensure that the massing and scale of the new development is broken down and would be sensitive to the historic environment. The proposed eastern frontage and public realm would enhance the setting of the Thames Barrier and Bowater Road Conservation Area, creating a positive environment at the edge of Moore Park that would increase opportunities for people to overlook and appreciate the historic and architectural significance of the Conservation Area. This in turn would stimulate regeneration in the wider area, therefore helping to achieve wider economic and heritage benefits through investment.
- 6.40. The proposals would preserve the significance of the assets concerned, and in consideration of the statutory requirements under the Planning (Listed Buildings and Conservation Areas) Act 1990, preserve the settings of the surrounding listed buildings and the character and appearance of the surrounding conservation areas in accordance with RBG Local Policy DH2.
- 6.41. Overall, it is considered that the scheme complies with the planning policy context (NPPF para 119, 120, 124, 130; London Plan policies D1, D2, D6, D8, D9, HC3; RBG Local Plan policies DH1 and DH2; and Charlton Riverside Masterplan SPD) and is acceptable in heritage, townscape and visual terms.

Residential Tenure and Mix

Residential Mix

- 6.42. Policy H10 (Housing Size Mix) of the London Plan (2021) states that to determine the appropriate mix of unit sizes in relation to the number of bedrooms for a scheme, applicants and decision-makers should have regard to factors including the local housing need and demand, the range of tenures in the scheme, the nature and location of the site, the aim to optimise housing potential on sites, and the role of one and two bed homes in freeing up family housing.

- 6.43. RBG Core Strategy Policy H2 states that a mix of housing types and sizes is required in all developments. The exact mix on each site will vary according to the location of the development and the character of the surrounding area and will be affected by factors such as accessibility to public transport.
- 6.44. The proposed unit mix is set out below:

Unit size	Private	Shared Ownership	London Affordable Rent	TOTAL
Studio	9	0	0	9
1-Bedroom	40	0	0	40
2-Bedroom	74	2	9	85
3-Bedroom	11	2	2	15
TOTAL no. units	134	4	11	149
TOTAL no. hab. rooms	355	14	35	404

- 6.45. The proposed development provides a proportion of family sized dwellings (3-bed+) with 10% of homes. The number of such family sized homes has been maximised. Increasing this figure further will affect the total number of homes which can be delivered, and it should be noted that proportionally larger homes generate a lower revenue, which would in turn have a significant detrimental impact on the viability and deliverability of the scheme and level of Affordable Housing.
- 6.46. Furthermore, the provision of the family sized homes has been focused within the affordable element of the scheme.
- 6.47. In the light of the above, the proposed development accords with the London Plan Policy H10 and RBG Core Strategy Policy H2.

Affordable Housing

- 6.48. A Financial Viability Assessment, prepared by Redloft, has been submitted in support of the application. In summary, it demonstrates that the scheme is not economically viable. It is therefore not financially possible in planning terms for this scheme to provide any additional affordable housing, Section 106 obligations or other planning gain contributions. The assessment shows that proceeding with this development is a financial risk to the Applicant.
- 6.49. Notwithstanding this, the Applicant is making an offer of 10% affordable housing (with a tenure split in accord with RBG's preferred tenure split of 70% social/affordable rent and 30% intermediate housing) on a without prejudice basis in recognition of the Council's aspirations for affordable home provision.

Residential Standards

Internal Space Standards

- 6.50. All homes have been designed to meet or exceed the National Technical Standards in terms of overall unit sizes and the internal space standards of individual rooms and storage space as set out in Policy D6 (Housing Quality and Standards) of the London Plan (2021).

Outdoor Amenity Space

- 6.51. The Mayor's Housing SPG (2016) and London Plan (2021) state that a minimum of 5sqm of private outdoor space should be provided for each 1- 2 person dwelling, plus an additional 1sqm for each additional occupant.
- 6.52. Each flatted residential dwelling is provided with private amenity space, in accordance with the requirements of the Housing SPG and Policy D6 of the London Plan, in the form of a balcony. The first-floor dwellings fronting the central courtyard have front gardens ensuring a degree of separation to the communal space to maintain privacy.

Children's Play Space

- 6.53. The Mayor's Play and Informal Recreation SPG (2012) and the London Plan (2021) require developments to provide 10sqm of play space per child generated by the scheme.
- 6.54. The play space provision on site is found in the green link, the southern building's first-floor podium and the northern building's upper floors roof terraces. The play space on green link has a strong connection with the adjacent park.
- 6.55. The proposal provides 1105sqm of play space, more than 500sqm above the requirement for 601.1sqm.
- 6.56. Older play space provision is located within the green link at ground floor (547sqm), while 0-5's play is found within the southern building on the first-floor podium (269sqm) and the northern building on the upper floors (289sqm in total).
- 6.57. In addition to the on-site older play, further play is located in the adjacent Moore Park and Maryon Park located to the south. Guidance within the Making London Child-Friendly document seeks to create a greater degree of child independent mobility. The scheme seeks to improve child play space through utilising the 10 elements that influence the mobility of children creating a play friendly landscape. This takes cues from farming permaculture where younger child play is centred on the development and provision for older play and the ability to move to older play is located steadily further away from the central hub of the site. Movement to the South and East is encouraged through improved networks through the park where older play is found.
- 6.58. The reduced traffic to Eastmoor Street would enable the street to be used as a shared surface with hop on hop off areas to the park in coordination with an improved wayfinding experience the street and park will become more permeable and interesting for local residents to use. By enabling an easy to cross site with the removal of physical barriers and creating a clear site line through the scheme. In conjunction with slowing and reducing traffic to Eastmoor Street and the introduction of amenity play and explorative play the design will create a higher degree of independent mobility and enable a holistic design that fosters a full range of community engagement.
- 6.59. In the light of the above, the proposed development accords with London Plan Policy S4 and RBG Core Strategy Policy H(e).

Accessible Housing

- 6.60. Policy D7 of the London Plan (2021) requires that 90% of new housing meets Building Regulation requirement Part M4(2) 'accessible and adaptable dwellings' and 10% of new housing meets Building Regulation requirement Part M4(3) 'wheelchair user dwellings', i.e. is designed to be wheelchair accessible, or easily adaptable for residents who are wheelchair users.
- 6.61. The proposed development comprises 15 units that comply with Part M4(3) requirements, which represents 10% of total units. The rest of the units are compliant with Part M4(2) requirements. The proposed locations and layouts of the Part M4(3) wheelchair dwellings have been discussed with RB Greenwich Officers. The location and mix of the Part M4(3) dwellings are set out in the accompanying Design and Access Statement.

- 6.62. In the light of the above, the proposed development accords with London Plan Policy D7 and RBG Core Strategy Policy H5.

Residential Amenity Impact

Privacy, Overlooking and Outlook

- 6.63. There are no minimum separation distances specified by RBG. A general guideline of 18-21m between directly facing habitable room windows is given in the Mayor's Housing SPG (2016), but this is not to be applied rigidly.
- 6.64. The distance between homes in opposing cores within the scheme measures a minimum of 14m on the western side and 21m from the centre to the eastern side. The southern elevation is set back by 9m metres from the Site's red line boundary to ensure that appropriate level of privacy is achieved with the adjacent emerging scheme. The Optivo site to the south has agreed to mirror this 9m set-back to ensure a minimum 18m separation.

Daylight, Sunlight and Overshadowing

- 6.65. NPPF (2021) Paragraph 125(c) states that Local Planning Authorities should refuse applications which they consider fail to make efficient use of land for housing, and in this context, authorities should take a flexible approach in applying policies of guidance relating to daylight and sunlight, where they would otherwise inhibit making efficient use of a site. This signals a policy shift away from rigid application of such guidance, which may unduly preclude new housing development.
- 6.66. A Daylight, Sunlight and Overshadowing Assessment has been carried out by Avison Young to measure the impact of the proposed development on neighbouring properties and the light levels within the proposed development, in accordance with the BRE daylight, sunlight and overshadowing guidelines. The full assessment accompanies this planning application and is summarised below.
- 6.67. The assessment of the proposed development has considered two scenarios in response to the surrounding context:
- > Scenario 1 - Surrounding baseline conditions as existing and the emerging Optivo site massing.
 - > Scenario 2 - The emerging Optivo site massing and hypothetical future massing in an attempt to reflect the emerging context and aspirations of the Charlton Riverside Masterplan SPD.
- 6.68. The assessment has established no existing neighbouring residential receptors in proximity of the Site and, therefore, a daylight and sunlight assessment is not necessary.
- 6.69. There is one existing outdoor amenity area, Moore Park, which will meet the recommended BRE Guidelines for the sun hours on ground (SHOG) test.
- 6.70. In terms of light within the proposed development, Avison Young has worked alongside the architects throughout the design process in order to maximise levels of light. Overall, the proposed development is considered to make very good use of the daylight available to the Site with a high pass rate. In relation to sunlight, the levels of sunlight amenity are comparable with schemes of a similar typology. Furthermore, residents will have access to well sunlit amenity areas internal and external to the proposed development.
- 6.71. Daylight and sunlight should not be considered in isolation, and it is important to remember that there are a number of other factors that contribute to the wellbeing of future occupants within a proposed development. To this end, the proposed development offers a range of amenities, including communal amenity space and play space for residents and replacement employment uses, which is in line with the Mayor's Housing SPG.

- 6.72. In summary, the Proposed Development is considered to perform well against the recommended guidance and represents an acceptable level of amenity.
- 6.73. In the light of the above, the proposed development accords with London Plan Policy D4.

Air Quality

- 6.74. RB Greenwich Core Strategy Policy E(c) requires major developments to submit an Air Quality Assessment. The Site is located within the RBG borough-wide Air Quality Management Area.
- 6.75. Ardent have carried out an investigation into potential air quality impacts arising from the construction and operation of the proposed development and produced the Air Quality Assessment report submitted with this application.
- 6.76. In summary, the report concludes that the proposed development will not have significant impact on the local air quality, and there are no air quality constraints to the proposed development in accordance with local, regional and national policy and guidance.
- > There is the potential for dust and PM10 impacts during the construction phase. However, with the proposed mitigation measures in place, the overall residual effect will be 'not significant'.
 - > Taking into consideration anticipated volumes of demolition and construction traffic, the maximum duration of the demolition and construction phase and the anticipated implementation of a CEMP, it is judged that the overall effect of emissions from development-generated demolition and construction traffic is likely to be 'not significant'.
 - > The impacts of operational traffic generation associated with the proposed development has been considered and will fall below the relevant screening criteria for human health. As such, the overall effect of development-generated operational traffic is considered to be 'not significant'.
 - > The impact of local air quality on future residents at the proposed development Site has been considered. Taking into consideration the proximity of the development Site to nearby emission sources, the layout of the Site and baseline air quality conditions in the local area, it is judged that baseline pollutant concentrations within the proposed development Site will be below the relevant objectives.
- 6.77. As a result, the scheme is considered to be better than 'Air Quality Neutral' in terms of both building and transport emissions, and no further action will be required to offset excess development emissions. This is in line with the London Plan requirements for new developments.

- 6.78. In the light of the above, the proposed development accords with London Plan Policy SI1 and RBG Core Strategy Policy E(c).

Noise

- 6.79. A Noise Assessment, prepared by Ardent, is submitted in support of this planning application. It was undertaken to assess the suitability of the Site for the proposed development with regard to noise and vibration.
- 6.80. In summary, the assessment concludes that:
- > The Site is considered a 'medium to high risk' site, when considered in accordance with guidance in ProPG. Expert Acoustics advice has been sought and good acoustic design processes should be followed to reduce sound levels across the site.

- > The assessment of operations at existing industrial operations will result in a low impact in line with BS4142:2014.
- > Noise associated with the proposed commercial use, such as fixed plant and equipment should not exceed cumulative rating levels at the closest receptor of 48dBLAeq during the day and 35dBLAeq at night, in accordance with local policy.
- > The site risk assessment of noise impact, when considered in accordance with AVO Guidance, results in a high risk of noise impact during an overheating condition during the day and night – assuming open windows are used to control overheating.
- > Noise associated with the safeguarded wharves has been fully considered and the outcome of the calculations has informed the mitigation proposals for the development. With the implementation of these mitigation measures noise from the wharves will be sufficiently controlled so as to protect residential amenity from potential noise impact from the wharves.
- > External sound levels will be reduced to as low a level as practicable. Sound levels on balconies are marginally above the guidance criteria. A separate communal garden space is provided that benefits from screening by the building meaning residents have a choice between a private balcony or garden.
- > Control measures should be implemented to manage potential impacts from construction noise.
- > The site is suitable for development, subject to the recommendations included in the report.

6.81. In respect of the final bullet point above, the report includes the following mitigation recommendations:

- > External Building Fabric - Non-Glazed Elements: it is assumed these comprise masonry cavity wall.
- > External Building Fabric - Specification of Glazed Units: Closed windows will be necessary to achieve the suitable amenity sound levels, during both the day and night. Windows are not sealed, and residents can choose to open windows whilst accepting higher internal sound levels. Up-rated double glazing and relatively standard ventilation is suitable to achieve recommended internal noise levels across the majority of the development, with exception of properties with line of sight to Westmoor Street, which will require slightly up-rated ventilation.
- > External Building Fabric - Specification of Vents: Ventilation with minimum acoustic performance in lieu of openable windows. All major building elements should be tested in accordance with BS EN ISO 10140-2:2010. Sole glass performance data would not necessarily demonstrate compliance with this specification. No further mitigation measures would be required to achieve the recommended internal noise levels.
- > Overheating: To achieve suitable internal amenity sound levels, windows must remain closed and an alternative means of ventilation provided. Consideration has been given to the potential for adverse noise impact during overheating conditions; where residents may open windows to control temperature. The site would be categorised as having a high risk of noise impact under overheating conditions. A separate overheating assessment accompanies the application and is informed by this assessment.
- > External Amenity Space: External sound levels on balconies are marginally above those set out in relevant authoritative guidance. The guidance in BS8233:2014 notes that a compromise between elevated noise levels and other factors, including other planning requirements should be considered.

- > Construction Phase: Construction activities should only take place between the hours of 08:00 and 18:00 on weekdays and between 08:00 – 13:00 on Saturdays. No construction activity should be carried out during the night, on Sundays or on bank holidays without additional consideration to controlling noise and with the prior approval of the LPA. During construction, the contractor will employ best practicable means to control noise from construction operations.

6.82. In the light of the above, the proposed development accords with RBG Core Strategy Policy E(a).

Sustainability and Energy Efficiency

6.83. JAW Sustainability have prepared an Energy Strategy, BREEAM Pre-assessment, Circular Economy assessment, Overheating Assessment, Life Cycle Carbon Assessment, Sustainability Statement, and Overheating Assessment, submitted in support of this application.

Energy Strategy

- 6.84. The report shows that the development employs an efficient building fabric, including well insulated walls and highly efficient glazing. In addition, efficient systems, Air Source Heat Pumps and PV panels are specified to maximise carbon savings for the site.
- 6.85. At the 'Be lean' stage, this results in a 18% and 31% improvement over Part L for the flats and commercial parts of the development respectively. This exceeds the minimum requirement of 10% for the flats and 15% requirement for commercial. The building fabric has been specified to be very efficient, while also ensuring non-combustible insulation is used. Additionally, the reduction in heating demand is balanced against reducing overheating.
- 6.86. At the be green stage the overall results are a 72% and 48% improvement over Part L for the flats and commercial parts of the development respectively, which exceeds the minimum requirements set out by the London Plan.
- 6.87. The development follows the energy hierarchy, heating hierarchy and cooling hierarchy. The development will install ASHP. In addition, the PV system is the largest that the roof can accommodate. The proposals therefore represent the best on site savings. The development will further achieve 'net-zero carbon' through an offset payment in line with the London Plan guidance.

BREEAM Pre-assessment

- 6.88. The BREEAM Pre-assessment report demonstrates that the development has the potential to achieve a Very Good rating with a target score of 67.58%.

Circular Economy

- 6.89. An overall strategic approach to Circular Economy was identified for both the existing buildings on the site and the proposed new development. A number of key commitments and design strategies have been identified to ensure the development contribute towards a circular economy, involving design decisions to minimise resources use, minimise waste and strategies to manage waste effectively. The report outlines their implementation as well as an end-of-life strategy considered from an early stage for the proposed development.

Life Cycle Carbon Assessment

- 6.90. An assessment has been conducted to assess the whole life cycle carbon emissions of the proposed development. The reports identifies actions to help reduce the impact of the development in each of the life

cycle stages, including the selection of low carbon materials, the reduction of transportation distances for materials to be brought to site, best practice installation of refrigerant pipework, scheduled material replacement and refurbishment, efficient building fabric, low flow fixtures and fitting, and recycling of materials at the building's end of life.

Overheating

- 6.91. The Overheating Assessment demonstrates that in developing the proposals action was taken at each stage of the cooling hierarchy in order to minimise overheating. Following initial runs of the model, a number of further measures were incorporated, including blinds.
- 6.92. The report concludes that all the assessed units within the development pass the overheating assessment for the current DSY1 scenario, which is the weather file recommended to be used in TM59. However, in more extreme heat such as the DSY2 and DSY3 scenarios the building struggles to meet some of the Criteria 2 requirements. Guidance will be provided to help occupants prevent overheating.

Sustainability

- 6.93. The Sustainability Statement demonstrates that the proposal follows the energy hierarchy set out in the London Plan, incorporating passive design measures and energy efficient equipment. The development employs an efficient building fabric, including new insulation and highly efficient glazing, efficient heating strategy and renewable systems. Measures are also incorporated to minimise pollution, reduce the embodied carbon footprint of the development, and reduce water use. The report concludes that the development complies with sustainability policy of the London Borough of Greenwich and the London Plan, for major developments.
- 6.94. In the light of the above, the proposed development accords with London Plan Policies SI2 and SI3 and RBG Core Strategy Policies DH1 and E1.

Environment

Landscaping

- 6.95. A Landscape Strategy has been prepared by Studio Bosk and is submitted in support of this application.

Ground Floor

- 6.96. Emphasis is placed on bringing the community into contact with nature throughout the seasons. New trees and species rich planting will bring a softer, green edge to the development. The ground floor landscape is divided into a series of character areas. The characters of the spaces respond to or promote the intended identity and function of the areas. They can be summarised as follows:
- > Eastmoor Street – The existing arrangement is reduced to a one-way street and a dedicated cycle lane adjacent to the park boundary. The layout introduces a change in direction to slow the speed of vehicles and cyclists at connection to East-West route. Drainage swales provide a natural boundary to the park, creating an unobtrusive barrier to vehicular and pedestrian movements while maintaining the visual connection between the spaces. Pedestrian paths through the park are realigned to reflect the removal of the car parking and the new alignment and uses along Eastmoor Street.
 - > East-West Route – This green link is a new pedestrian connection linking Westmoor Street and Barrier Park. The proposed trees on the northern side of the space frame views through to the surrounding context. A planted drainage swale provides a playful edge to the route with steppingstones creating a sense of adventure and exciting circulation between the spaces. At each end of the two entrance

spaces create opportunities for users of the surrounding commercial units to break out into the space. Framed by native species planting and hedging at the centre of the east west route, a 'residents' garden' celebrates the widening between the buildings. Planting divides the garden into a series of intimate, multifunctional, playful spaces that can be used by all residents of the development. Views from the residential cores into the garden are framed by multi-stem trees and low-level planting.

- > Optivo's neighbouring site – The interface between the site and the Optivo land to the south will create a strongly planted east west route with movable seating creating a multifunctional and flexible space, that caters for both the servicing requirements as well as creating a usable space.
- > Mirfield Road – A new extension to the existing pedestrian footpath will provide a connection to the proposed commercial spaces. Tree planting will continue the rhythm of the architectural colonnade on the western corner and provide a buffer to the new highway layout.
- > Westmoor Street – The predominantly hard paved street creates a 4m paved interface with banding taking queues from the architectural fenestration. The built form along the boundary is broken up by large scale tree planting. Encroaching from the east west green links, a loading bay is located along the street as part of the shared space pavement.
- > Moore Park – The proposals seek to provide additional crossing points to the western boundary and realign some of the pathways to create a stronger north south and east west pedestrian connection. With the removal of some select trees along the western boundary the park will become more visually permeable. Additional wayfinding and play equipment will also help to activate the park for different users.

Roof Plan

- 6.97. The development provides a range of podium deck and roof garden residential amenity spaces split over different levels. These spaces range from smaller more intimate terraces to larger areas combining a range of spaces in an open communal terrace. The roof top landscape is also divided into a series of character areas.
- > Communal Podium and Roof Garden – The podium courtyard and roof gardens will provide residential amenity space across both buildings and across levels. These spaces will share a common design language, building on the industrial heritage theme and response picked up in the ground floor landscape. The same strong geographic lines will be used in the formation of spatial arrangements and patterns of surfacing. The linear panel design of the hard flooring will be continued but with a more refined scheme of concrete block planking as the main paved surface. Other fixtures and detailing will also have an industrial theme but combined in a contemporary manner.
 - > Biodiverse Roof – The areas of biodiverse roof will include where possible, fallen logs, PV panels, recycled aggregate to support a variety of plants, birds, animals and invertebrates. With the inclusion of species rich wildflower meadow supplemented with insect houses, bat and bird boxes to fully enhance and increase the sites biodiversity.

Lighting

- 6.98. A hierarchy of routes and spaces will be lit to ensure that the lighting meets the required levels for vehicular and pedestrian areas. This will be supplemented by decorative lighting for certain features like; trees, benches and steps to enhance the character of the spaces. Where possible the lighting will be integrated within the fabric of the building, external walls, seats and other structures to minimise clutter. Lighting will be designed to ensure that light spill will not adversely affect the surrounding residential units. It will also be developed to minimise the impact of light spill on local wildlife.

Urban Greening Factor

- 6.99. The Urban Greening Factor (UGF) for the site has been calculated as 0.4463, which meets the target score set out by the London Plan Policy G5.

Ecology

- 6.100. A Preliminary Ecological Appraisal and Preliminary Bat Roost Assessment has been prepared by The Ecology Partnership and is submitted in support of this application.

- 6.101. In summary, the report demonstrates that:

- > There are two sites of statutory designation within 2km of the red line boundary. Gilbert's Pit (Charlton) (SSSI) 150m SE and Maryon Wilson Park & Gilbert's Pit (LNR) 530m SE.
- > There is one internationally designated site located within 10km of the red line boundary; Epping Forest (SSSI, SAC) 9.2km N.
- > The proposed development is located on an existing hard-standing footprint and is isolated from these designated sites by dense urbanisation with limited ecological connections. It is considered there will be no direct or indirect effects on the designated sites in the local area.
- > Habitats of Principal Importance and Priority Habitats are present within the local surroundings, including: mudflats, good quality semi-improved grassland, wood-pasture and parkland, deciduous woodland. These habitats were separated from the site by dense urbanisation consequently it is not considered that the proposals will directly or indirectly affect these habitats.
- > The habitats on site are dominated by hard-standing and exhibit no areas of natural vegetation.
- > Internal and external building inspections found negligible potential for roosting bats and the site is not considered to be constrained by this species. Furthermore, no suitable foraging or commuting habitat was found on site.
- > Owing to a lack of any other suitable habitat, the site is not considered to be constrained by other protected species that could feasibly occur, namely GCN, dormice and reptiles, therefore no further surveys are recommended.
- > Birds are likely to nest within buildings on site and as such demolition should be undertaken outside of the bird nesting season (March – September). If this is not possible, a qualified ecologist should re-check the buildings immediately prior to their demolition.

- 6.102. In this context, the report makes the following recommendations for enhancements, aimed at improving the ecological value of the site post development.

- > Green roofs and green/living walls should be provided to increase opportunities for biodiversity.
- > Trees should be planted around the buildings and on the road frontage to add to the green infrastructure of the local area and provide new street tree planting, which will greatly improve the amenity value of the site and the local area. These should be native species.
- > Nest boxes should also be installed to provide new nesting opportunities for birds and to achieve ecological enhancements.
- > A bee post can be incorporated into the edges of the site or associated with the green roof/living walls area.
- > Demolition of buildings should occur outside of the breeding bird season (March - September) or immediately after a nesting bird check by a suitably qualified ecologist. If active nests are identified, works in the vicinity of the nest must cease until the birds have fledged the nest.

Trees

6.103. A Tree Survey, undertaken by Keen Consultants, has been submitted in support of this application.

6.104. In summary, the report demonstrates that:

- > There are no trees upon the site itself, but trees exist within the pavement on the adjoining roads. None of the trees are of any particular merit, with most having some sort of defect.
- > On the corner of Mirfield Street with Eastmoor Street is a London plane that is of some merit but is not an exceptional specimen.
- > Within the pocket park to the east of Eastmoor Street is a collection of larger trees, some of which have potential but others, mostly poplars, have reached a stage of development where they should be removed to enable replacements to be planted.
- > In general, the quality of the trees directly abutting the site are not of sufficient quality to constrain the development. If these trees need to be lost to deliver the scheme, they can be offset by a suitable scheme of new tree planting.

6.105. The trees that need to be removed to accommodate the proposed development will be compensated with additional number of native tree species at each boundary of the site, within the green link and on the podium. Green roofs are also proposed on each block. The green coverage is thus maximised to ensure a continuity with the adjacent park.

Flooding and drainage

6.106. A Flood Risk Assessment and Drainage Strategy, prepared by Ardent Consulting Engineers, has been submitted in support of this application.

6.107. In summary, the report demonstrates that:

- > The Site is located within tidal Flood Zone 3, associated with the River Thames, which benefits from defences.
- > There is a residual risk in the event of a breach in the defences. Maximum predicted flood level for the 200yr breach event for the 2115 epoch is 6.128m AOD at the Site of interest. Commercial units are to be located at the ground floor. The first floor, which is proposed for residential use, is set at 6.475mAOD, 5cm above the breach level including freeboard. The 200 year plus climate change breach level being 6.428mAOD.
- > In the event of a breach, the Site will be surrounded by Extreme Hazard rating. If time allows and following the advice of the emergency services Site occupiers can evacuate the Site via Eastmoor Street, heading west along Woolwich Road and then south through Maryon Park. The section of Woolwich Road to the north of Maryon Park as well as the entire area south of Woolwich Road remain dry during the 200 year plus climate change breach event.
- > It is recommended that any levels below the flood level should incorporate flood resistance and/or resilience construction to minimise the potential damage that could be caused by flooding. Residential development is proposed to have safe refuge for occupants at higher levels.
- > The site is considered to be at a generally 'low' risk of pluvial flooding (less than 0.1% probability), although pockets of the site are shown to be a 'medium' risk. Eastmoor Street to the east and Mirfield Street to the north are located at 'high' risk (greater 3.3% probability).
- > Surface water run-off will be managed as part of the proposed surface water drainage strategy. Therefore, the risk of pluvial flooding is assessed as 'low'.

- > The surface water drainage strategy will reduce flood risk by restricting surface water flows in accordance with the London Plan. Storm water attenuation is provided for all storms up to and including the 1 in 100-year critical event (including a 40% allowance for climate change). The proposed development results in significant betterment over the pre-development scenario in terms of a reduction of surface water runoff.
- > A management company will be appointed to maintain communal areas, landscaping, and shared SuDS throughout the development. All maintenance will be in accordance with the best practices and the CIRIA Manual C753.

6.108. In conclusion, this FRA demonstrates that the proposals are consistent with the aims of the NPPF and the Planning Practice Guidance to the NPPF along with the aims of the Strategic Flood Risk Assessment. The Site will not be at significant risk of flooding or increase the flood risk to others.

6.109. In the light of the above, the proposed development accords with and RBG Core Strategy Policy E2.

Transport, Parking and Servicing

6.110. A Healthy Street Transport Assessment, Framework Travel Plan, Car Park Management Plan, and Delivery and Servicing Management Plan, produced by Ardent Consulting Engineers, are submitted in support of this planning application. The Transport Assessment considers the impact of the proposal on the existing local transport network and the accessibility of the Site.

East-West Route Delivery

6.111. Within the SPD there is an aspiration for an East-West Route to be delivered to the north of the Site. However, only an indicative alignment is shown.

6.112. The proposals link the existing highways Eastmoor Place and Mirfield Street. In accordance with the SPD this route makes use of both '**existing routes**' and '**historic routes**', as shown on **Figure 2** below which overlays the proposed east-west alignment with the historic mapping, effectively recovering this historic route for public use.

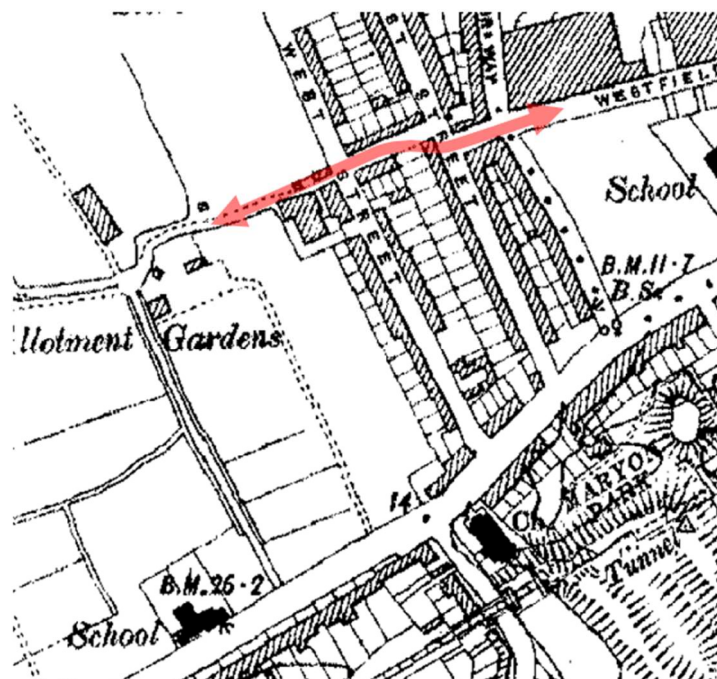


Figure 2: Extract from Historic OS Mapping (1920) with Indicative Alignment of east-west route

- 6.113. As shown on **Figure 3** below, this east-west alignment respects property boundaries, in-line with the aspirations of the SPD as far as possible. To the west of Mirfield Street the east-west route runs along the southern boundary of Title no SGL507495, avoiding numerous separate land titles if this alignment were further south.

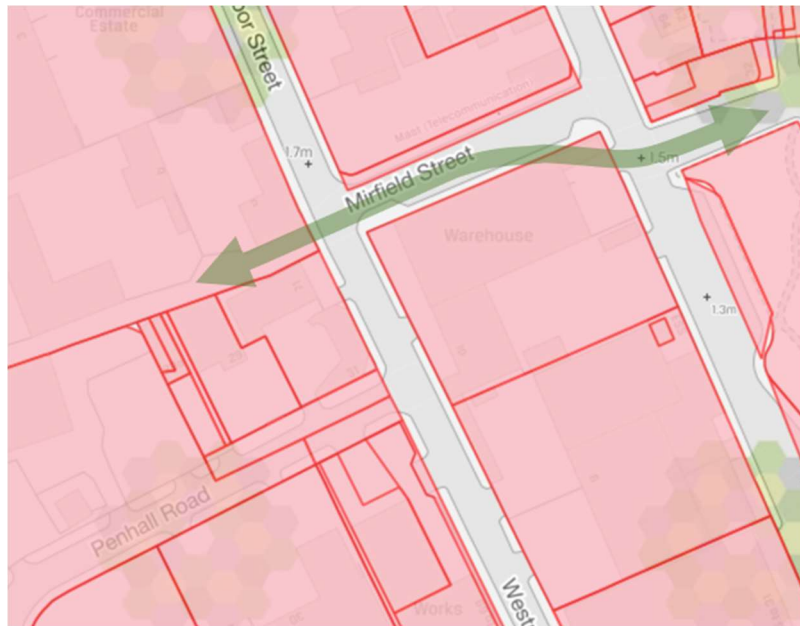


Figure 3: Land Registry Extract

- 6.114. A detailed assessment of the how the proposed development accords with the relevant sections of the SPD is provided at **Appendix 2**, including the enabling of the East-West Route.

Car Parking

- 6.115. The Site has a Public Transport Accessibility Level (PTAL) score of 2 (where 1 is low and 6 is high). There is no Controlled Parking Zone (CPZ) in the area. The site is also in an Inner London Opportunity Area and has a future PTAL rating of 4 ("Good"), therefore, in accordance with the London Plan, the residential aspect of the development should be car free, with disabled parking provided in line with the London Plan standards for both the commercial and residential aspects of the scheme.
- 6.116. In line with the above requirements, a suitable level of disabled parking provision will be provided, with 11 disabled parking spaces provided on site within an undercroft parking area accessed via vehicle crossover onto Westmoor Street.
- 6.117. It is noteworthy that there is typically a relatively low uptake of disabled parking on new developments, with the initial 3% provision as set out within the London Plan being in excess of the average blue badge holder statistics, both for London and RGB. Average blue badge uptake in London and RB Greenwich are as follows:
- > London = 2.7% as percentage of population
 - > RB Greenwich = 2.25% as percentage of population
- 6.118. At this stage the end occupiers of the commercial element of the scheme is unknown. Once detail of the end land uses is known, further detail on the disabled parking provision can be provided.
- 6.119. For the commercial and residential uses proposed, all spaces require at least passive EV charging with 20% of the residential disabled parking spaces requiring active EV charging point provision. In line with this

requirement, 3no. of bays will be provided with active charging point and the remaining 8no. with passive charging point.

- 6.120. Parking survey results show there is ample spare capacity on the local highway network. However, it is understood that parking restrictions being implemented as part of any redevelopment with residents prohibited from applying for permits for the future situation when the wider area is redeveloped. Notwithstanding this, Policy T6 (c) of the London Plan states: *'An absence of local on-street parking controls should not be a barrier to new development, and borough should look to implement these controls wherever necessary to allow existing residents to maintain safe and efficient use of their streets'*.
- 6.121. It should also be noted that Policy T6 (Car Parking) of the London Plan emphasises that car-free developments should be the starting point for all proposals in places that are (or planned to be) well-connected by public transport, with developments elsewhere designed to provide the minimum necessary parking. The requirement for a car free development was re-iterated at the pre-application stage by the GLA.
- 6.122. In the light of the above, the proposed development accords with London Plan Policy T6.

Cycle Parking

- 6.123. Cycle parking has been incorporated into the scheme with secured covered cycle stores located at ground floor level with step-free access from the street.
- 6.124. Cycle parking within the site has been designed in accordance with Chapter 8 of TfL's London Cycle Design Standards (LCDS). The design includes suitably sized doorways (2m+), corridors, lifts etc and adopts 5% of spaces for larger bicycles (provided in Sheffield type stands) and a further 10% or 20% of standard Sheffield Stands for commercial or residential respectively. The remainder of cycles will be accommodated in tiered stands, with a minimum aisle width of 2.5m beyond the lowered frame, to allow cycles to be loaded, with an overall aisle width of 3.5m provided where there are racks either side of the aisle.
- 6.125. Circulation space for site occupiers to move their cycle around within the building incorporates wide corridors and door openings to assist movement, whilst the number of doors utilised along cycle wheeling routes has been minimised in order to further aid movement of cycles within the building. Where multiple doors are necessary, these are provided with push buttons or sensors to automatically open allowing more convenient access.
- 6.126. The proposed quantum of cycle parking for long-stay and short-stay is in accordance with Policy T5 of the London Plan, with the more onerous non-residential standards used where the end land use is not known (using old use class A2-A5 requirements for the proposed flexible retail/community space, and B1 offices for the proposed flexible commercial space) as set out in the table below.

Land Use	Long-stay policy requirement	Required provision	Proposed
Flexible retail / community use (Use class E and F2)	Retail (A2-A5): • 1 space per 175 sqm	3	3
Flexible employment use (Use class E(g) and B8)	Business offices (B1): • 1 space per 75 sqm (max.)	11	11
Residential (C3)	• 1 space per 1B1P dwelling • 1.5 spaces per 1B2P dwelling • 2 spaces per all other dwellings	269	269

Land Use	Short-stay policy requirement	Required provision	Proposed
Flexible retail / community use (Use class E and F2)	Retail (A2-A5): • 1 space per 20 sqm (up to 750 sqm)	31	31
Flexible employment use (Use class E(g) and B8)	Business offices (B1): • 1 space per 500 sqm	2	2
Residential (C3)	• 1 space per 40 dwellings	5	5

- 6.127. A total of 274no. cycle parking spaces are provided for residential use, which includes 269no. long-stay spaces within secure storage rooms within the building at ground floor and 5no. visitor spaces. A total of 69no. spaces (25.2%) are provided as Sheffield stands spaced at 1.2m, 14no. spaces (5.1%) as Sheffield stands spaced at 1.8m to accommodate larger cargo/ exceptional bikes, and the remaining 191no. spaces (69.7%) provided on Josta two tier bicycle stands.
- 6.128. A total of 34 no. cycle parking spaces are provided for flexible retail/community use units, including 3 long-stay spaces and 31 short-stay spaces.
- 6.129. A total of 13 no. cycle parking spaces are provided for the flexible commercial use units, including 11 long-stay spaces and 2 short-stay spaces.
- 6.130. In the light of the above, the proposed development accords with London Plan Policy T5.

Trip Generation

- 6.131. Given the car-free nature of the proposed development, the Healthy Street Transport Assessment anticipates a significant reduction in vehicle trips.

Framework Travel Plan

- 6.132. The Framework Travel Plan includes a number of measures to increase sustainable travel by the future occupiers of the proposed development. These can be summarised as follows:
- > Lift Share – Liftshare will be promoted to members of staff, informing them that prospective users can sign up free-of-charge. Use of Liftshare will provide the opportunity for easy access to car travel in lieu of using private hire vehicles, whilst also reducing the potential demand for car ownership.
 - > Cycle to Work Scheme – For the commercial units, employers will be encouraged to offer a tax exemption for employees purchasing cycles that allows them to spread the cost over 12 months, improve health and fitness and reduce the cost as a non-taxable benefit.
 - > Car Club Membership – It is proposed to accommodate a car club vehicle within the site. As such, it is considered appropriate that a fixed term membership will be provided. This will cater for any short-term potential need for use of a car and hence also disincentivise use of private hire vehicles/taxis too.
 - > Legal Agreements – The applicant is willing to include within the S106 agreement that future residents will be prohibited from applying for a car parking permit, within any future CPZ's in the area.

- > Welcome Pack – A pack will be produced prior to occupation, bespoke for each separate land use of the development for distribution to upon their occupation. It will comprise a concise package setting out details of public transport services available within the vicinity of the site as well as details of local amenities and facilities.
- > On-site Infrastructure - The development is car-free with cycle parking provided on site, in line with London Plan requirements. The use of these spaces will be monitored, if demand for cycle parking regularly exceeds the supply the provision of additional spaces will be investigated. If additional cycle parking spaces are required for either the residential or commercial elements, potential locations for an uplift in provision will be explored.

Refuse Collection, Servicing and Emergency Vehicle Access

- 6.133. The Delivery and Servicing Management Plan concludes that the development can be appropriately serviced on a day-to-day basis on site without detrimentally effecting highway safety.
- 6.134. The Design and Access Statement also includes details regarding the of the servicing of the site, as summarised below.
- 6.135. A servicing bay has been provided outside the northern block for commercial units 01-03. This space will also serve as a drop- off taxi space and delivery space for residents. The commercial units along the green link, units 04-05, can be serviced via a bay located on Westmoor Street. This bay can also be used for residential deliveries/post. Servicing to units 05-06 can take place from Eastmoor Street. Commercial unit 07 can be serviced from Westmoor Street, vehicles will be able to enter the landscaped yard and exit in forward gear via Eastmoor Street. Unit 08 can be serviced via Eastmoor Street or the landscaped yard.
- 6.136. The Design and Access Statement also confirms that a fire engine pump appliance can access the buildings and cores via Mirfield Street, Eastmoor Street and Westmoor Street.
- 6.137. As far as the refuse strategy is concerned, the design of both the residential and commercial bin stores has been achieved using the Royal Borough of Greenwich’s ‘Guidance notes for the storage and collection of waste and recycling materials’ (May 2018). The bin store dimensions are based on the number of bins required per core in relation to the number of units served by it. The stores will cater for recycling, residual waste, organics, textiles, and waste electrical and electronic equipment. Separate stores have been allocated for larger items i.e. bulky waste. Residential and commercial waste has been kept separate.
- 6.138. Refuse vehicles can access refuse stores located on Mirfield Street via a enlarged paving area in front of the building. On the southern block, a refuse vehicle will use the hard landscaped yard space to access the refuse store and exit in a forward gear via Eastmoor Street. A further refuse store is accessed via Eastmoor Street. Eastmoor Street will be closed to cars but can be used by servicing/emergency vehicles. If Eastmoor Street becomes closed to all vehicles, then all refuse and recycling can be collected from the southern store and the refuse vehicle can turn within the yard space to exit via Westmoor Street. Wheelie bins will be moved from the store on Eastmoor Street through the car park to the larger southern bin store on collection day and returned after collection.
- 6.139. Cores A & B have refuse and recycling stores located on Mirfield Street. The refuse stores are within close proximity to entrance cores. Resident travel distances do not exceed 30m. On collection day refuse bins are collected and emptied via Mirfield Street, the maximum bin drag distance is 15m or less. Core C has a refuse and recycling store located on Eastmoor street. Residents exit the stair core and access the refuse store via a ventilated lobby. Core D has a refuse and recycling store located on the landscaped yard space accessed via the car park area. Residents will personally arrange bulky waste collections via RBG. Battery, textile, and WEE

recycling bins have also been provided. Bin stores have been provided with 1500mm wide double doors and level access routes.

- 6.140. Commercial units will each have their own refuse stores provided with level access and 1500mm wide double doors.

Archaeology

- 6.141. An Archaeological Desk-Based Assessment has been undertaken by RPS and is submitted in support of this planning application. As confirmed in the Assessment, there is designated heritage assets within or immediately adjacent to the Site.
- 6.142. The Assessment concludes that the groundworks associated with the proposed development are not considered likely to result in the impact or removal of non-designated archaeological remains.

Health

- 6.143. A Health Impact Assessment, prepared by RPS, has been submitted in support of this planning application.
- 6.144. The HIA concludes that the proposed development does not present any material risk to existing communities, and would serve to facilitate a healthy, vibrant and cohesive population growth to the area.
- 6.145. During construction, the nature of the site presents little risk to public health. Moreover, a construction management plan and a transport management plan will be in place to further manage temporary changes in air quality, noise and community safety. Once operational, the proposed development will increase and enhance local housing stock and mix and offer significant accessibility and adaptability to cater to a wide demography. The proposed urban design includes highly accessible links to and between community assets and amenities, addressing existing community barriers, and improving natural surveillance. Healthy urban design features complement the priorities set by the Greenwich health and wellbeing board and place a great emphasis on health promotion through active, vibrant and cohesive communities, proactively supporting good mental health and wellbeing through shared community assets and social capital.

Utilities & Services

- 6.146. A Utilities and Services Statement, prepared by Ardent Consulting Engineers, is submitted in support of this planning application.
- 6.147. In summary, the statement concludes that:
- > Enquiries have been made to all relevant utility companies to obtain asset plans, identify any diversionary work required and to confirm if capacity is available to serve the proposed development.
 - > There may be private utilities present within the site which would not be picked up within the report.
 - > No services are anticipated to need diverting subject to detailed design.
 - > Outline provision costs for serving the site have been identified including potential infrastructure upgrade works anticipated at this stage of design.
- 6.148. In the light of the above, it has been demonstrated the development can be served by electricity, gas, water and wastewater without detriment to the existing local infrastructure.

Planning Obligations Statement

- 6.149. The Greenwich Planning Obligations SPD provides guidance on the likely type and scale of planning obligations for development proposals in Greenwich to ensure that the impact of development on infrastructure and services can be adequately mitigated.
- 6.150. Having regard to the Planning Obligations SPD and discussions with RBG Officers during the pre-application phase, we set out the potential contributions that will be requested in addition to CIL and secured by s106 legal agreement, below:
- > Affordable Housing: securing tenure and mix, securing units in perpetuity, and setting out clawback period
 - > Transport: securing payment of Charlton Riverside Road Infrastructure Charge (up to a maximum of **£3,000 per unit**) towards funding the East West route, Riverside Route (Herringham Road link / North South loop) and necessary improvements to Westmoor Street to facilitate public transport access) including pedestrian and cycle improvements in Westmoor Street) and the introduction of a new Controlled Parking Zone, securing TfL Bus Tariff to fund new extended bus service into Charlton Riverside (**£2,812.50 per unit**), Clause to restrict access to parking permits for future occupants (except Blue Badge holders), Provision of car club bays on site, Payment of car club membership for future occupants for 5 years, Contributions towards cycle training, Contribution towards Improvements to public realm in Eastmoor Street and adjacent park (subject to an agreed period for implementation of works), Travel plan monitoring, Clause to secure public access through the site (northern green link and southern link).
 - > Employment and Training: Commitment and participation towards GLLaB and business support and payment of contributions, Approval and implementation of a Business Relocation Strategy to assist with the relocation of the existing business occupier.
 - > Environment: Carbon off-set contribution, Future connection of off-site district heating network, Renewable / low carbon monitoring, GLA's 'Be Seen' monitoring, cost of replacement of street trees (for trees T11, T13, T14, T15 and T16: **£23,070** and, for trees T10 and T12: **£14,087** should it not be possible to retain the street trees in situ).
 - > Other: Contribution towards monitoring, review, legal and professional fees, Noise monitoring contribution, Environmental Amenity Policy Statement (advising future occupants of measures available to address noise issues with specific reference to pre-existing noise generating sources, including nearby industry such as the safeguarded Angerstein, Murphy's and Riverside Wharves and provisions to ensure that future occupants of the development are made aware of the EAPS for the lifetime of the development), Environmental Health and Waste Management, Participation in Considerate Constructors scheme;
- 6.151. The Applicant will also enter into s278 and s38 legal agreements to secure off-site highways and landscaping works respectively.

Community Infrastructure Levy (CIL)

Royal Borough of Greenwich CIL

- 6.152. The Site falls within Zone 1 of RB Greenwich CIL charging schedule dated 25th March 2015. The current charge for residential development is £70/sqm. There are charges for non-residential uses: supermarket and retail warehousing (280+ sqm) are charged £100/sqm.

Mayoral CIL

- 6.153. Greenwich falls within Zone 3 of the current Mayoral CIL charging schedule date 1st April 2019. The levy is £25/sqm for all types of development, excluding education and health development.

7.0 Conclusions

7.1. This Planning Statement supports the submission of a full planning application to the RB Greenwich (RBG) for the redevelopment of an important strategic development site. This application is made following the refusal of a previous application for full planning permission for the Site (LPA Ref. 20/1924/F) validated on 17th July 2020 and refused on 6th October 2021.

7.2. Full planning permission is sought for:

Demolition of existing structures and erection of buildings between 4 and 8 storeys in height (including roof access level areas with a maximum total height of 29.5m AOD), comprising 149 residential units, 740.6 sqm flexible employment floorspace (Use class E(g) and B8) and 555 sqm flexible retail and community uses (Use class E and F2) with associated landscaping and new public realm, access and infrastructure works, refuse and recycling storage, car parking and cycle parking and associated development.

7.3. This report has demonstrated that the proposed development, which would provide significant social, economic and environmental benefits to the Site and surrounding area, is in conformity with applicable national, regional and local planning policy as well as relevant guidance.

7.4. The proposal would deliver a significant number of new homes on a strategically important site within the Charlton Riverside Opportunity Area, which is identified by the GLA and RB Greenwich to provide large quantities of new housing. The proposals would also provide 10% affordable housing on Site.

7.5. As set out in the detailed SPD tracker (**Appendix 2**) the proposed development fully addresses a significant number of the issues raised within the SPD. There are essentially only three areas of disparity relating to (a) the scale and density; (b) the 50% target for family housing; (c) the full re-provision existing employment space; and (d) the 35% target for affordable housing.

7.6. The variance in terms of scale/density needs to be considered in the context of the wider benefits that flow from the scheme, as follows:

- > Mixed-use development delivering 149 high-quality new homes, with maximum viable 10% affordable rent tenure with a focus on family accommodation.
- > Green link at ground level, courtyard at podium level and accessible roof terraces providing a total of 2778.8 sqm of communal amenity space including play space.
- > A high-quality human scale development of between 4 and 8 storeys in height.
- > Total of 1295.6sqm flexible employment and retail and community uses on ground floor.
- > Active frontages on all streets.
- > Concealed Blue Badge and commercial units parking below podium.

7.7. The scale of development is considered to be entirely appropriate in this location to ensure that all the above-mentioned benefits can be provided within the scheme and that we can demonstrate that we comply with national planning policy which strongly emphasises the need for best and most efficient use of brownfield land. This is particularly important in an area such as Charlton Riverside, which is an Opportunity Area in the London Plan and is looking to provide a substantial number of new homes and flexible commercial floorspace. This cannot and will not be achieved if the critical mass on site is not achieved to subsidise the numerous components, which allow for a high standard of design and quality of placemaking that are promoted within the scheme.

7.8. The massing has been developed to respond to the emerging context of the Charlton Riverside Area. The heights of the buildings have been considered to ensure amenity spaces, the green link and the courtyard space

receive adequate daylight and sunlight. The massing of the northern building creates a defined taller block on the intersection of Mirfield Street and Westmoor Street, a key focal point along the future East-West Route. The massing lowers from 8 storeys to 6 storeys towards Moore Park.

- 7.9. The residential density of the proposed scheme is 252 u/ha. Whilst this is greater than the indicative figure in the Masterplan SPD, the proposed development is considered to be appropriate for its context, having regard to the emerging schemes, infrastructure, access to services and amenities within the Charlton Riverside masterplan, the aforementioned benefits of the proposed development, and the contribution to the aspiration of the SPD for a sustainable mixed-use community. In any event, density figures should only be used as a starting point and guide rather than as an absolute rule so as to also take proper account of other objectives, especially for dwelling mix, environmental and social infrastructure, the need for other land uses (e.g. employment or commercial floorspace), local character and context, together with other local circumstances, such as improvements to public transport capacity and accessibility.
- 7.10. The scheme includes 10% family homes. The number of such family sized homes has been maximised. Increasing this figure further will affect the total number of homes which can be delivered, and it should be noted that proportionally the larger homes generate a lower revenue than the smaller ones, which would in turn have a significant detrimental impact on the viability and deliverability of the scheme. The overall provision of a mix of units in this location is considered to be a good mix, which can viably be supported to help facilitate the regeneration of this strategically important Opportunity Area. Furthermore, the provision of the family sized homes has been focused within the affordable element of the scheme, where the need is greatest.
- 7.11. It is acknowledged that the scheme does not fully re-provide the existing level of employment space. However, the re-provision of 1,295.6 sqm (GIA) of commercial floorspace will result in an increase in the number of potential employment opportunities. Using the figures set out in the HCA's Employment Density Guide (November 2015), it has been calculated that the scheme will deliver a significant uplift in local employment with the site currently only providing around 20 to 30 local jobs the commercial facilities will provide 53 jobs. The scheme will provide minimum 23 additional FTE roles than the site currently provides. The scheme will also create construction jobs during the development phase of the project.
- 7.12. In addition, a package of support services has been devised to help existing businesses on the site to relocate to new premises. The scheme will also support the wider redevelopment ambitions of the Charlton Riverside Masterplan to help any other existing businesses on surrounding development plots to be supported to lease the new commercial space created by this development.
- 7.13. All of the proposed homes would meet the National Minimum Space Standards and provide a high quality of residential accommodation, incorporating appropriate mitigation measures to protect future occupants from any road noise, whilst fully respecting 'agent of change' principles in London Plan policy. All homes would be provided with a private balcony or garden to meet the minimum London Plan standards.
- 7.14. The proposed development also includes exceptional public realm enhancements, open space and play space provision for children of all age.
- 7.15. The Transport Assessment demonstrates that the proposed development would result in a significant reduction in vehicle trips. In accordance with the London Plan, the residential aspect of the development is car free, with disabled parking provided in line with the London Plan standards for both the commercial and residential aspects of the scheme. 20% of the bays will have active Electric Vehicle Charging Points (ECVPs), and the remaining will be passive. Cycle Parking will be provided in accordance with the London Plan.
- 7.16. The Energy Strategy demonstrates that the proposed development follows the energy hierarchy, heating hierarchy and cooling hierarchy. The development will install ASHP. In addition, the PV system is the largest

that the roof can accommodate. The proposals therefore represent the best on site savings. The development will further achieve 'net-zero carbon' through an offset payment in line with the London Plan guidance.

- 7.17. The proposals would significantly increase opportunities for biodiversity throughout the site, compared to the existing. Trees will be planted around the buildings and on the road frontage to add to the green infrastructure of the local area. Intensive green roofs will also be provided to increase opportunities for biodiversity.
- 7.18. The FRA demonstrates that the proposals will not be at significant risk of flooding or increase the flood risk to others. This has been ensured by locating the commercial units at the ground floor. Sleeping accommodation has been designed such that finished floor levels are above the 200-year plus climate change breach level.
- 7.19. The applicant is fully committed to providing of the maximum viable number of genuinely affordable homes as part of a high-quality mixed-use scheme that includes family housing, and flexible commercial/employment units on the ground floors, which will see a net increase in the number of jobs provided on the site.
- 7.20. Overall, it is concluded that this submission meets or exceeds the planning policy requirements set out in National Policy, the London Plan and RB Greenwich's Local Policy, by optimising development on this strategically important brownfield site within the GLA Opportunity Area. In accordance with NPPF Paragraph 11(c) the application should therefore be approved without undue delay.

Appendices



Appendix 1. Site Location Plan



address : 88 Union Street London SE1 0NW
email : mail@alancamp.com

phone : 020 7593 1000
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job:
Eastmoor St - Resubmission
Charlton, SE7 8LX

drawn:
MH

date:
10.21

drawing:
General Arrangement
OS Site Location Plan

scale:
1 : 1000 @ A3
1 : 2000 @ A5

job number:
3199

drawing number:

EX-SP-L00 rev. PL 1

note: all dimensions to be checked on site before commencement of the work. if this drawing exceeds the quantities in any way the architects are to be informed before work is commenced. this drawing is copyright.

revisions (most recent listed below, refer to DIS for all revisions)

rev	date	description	drawn	check
1	22.10.21	Issued for Planning	MH	SC



Appendix 2. Charlton Riverside Masterplan SPD Tracker





Eastmoor Street – Charlton Riverside

Scheme Comparison with SPD – Executive Summary

Introduction

This document has been prepared as an executive summary to a detailed SPD tracker document. Both form an appendix to the Planning Statement, which has been submitted in support of the planning application.

SPD Compliance

The detailed SPD tracker document demonstrates that we have fully addressed a significant number of the issues raised within the SPD (62 of the 72 relevant issues), with only 10 issues where we have either not addressed or only partially addressed the guidance. It should be noted that of these 10 issues, 6 relate to the provision of 3+ bed family housing. The application has a large proportion of 2 bed 4 person homes. The London Plan recognises that this type of home is suitable for families. Taking this into consideration, the overall percentage of family homes is much greater.

It is important to stress here that the SPD is intended to provide guidance for developers to ensure that developments respond positively to the guidance in order to meet the objectives of Charlton Riverside. The SPD should not be used as a 'blueprint' for detailed planning applications because there is a considerable amount of further detailed available to developers and applicants relating to the specifics of their site/sites, that would have been available at the time the SPD was written.

The SPD is written in a way that means that land ownership boundaries are largely ignored and is therefore aspirational in many ways. The delivery of the fundamentals of this vision can and should be applied to all development proposals coming forward and this duty has been taken extremely seriously from the outset of the planning and design process. However, it is not possible to fully replicate the exact requirements of the SPD and nor was the SPD written in a way that was meant to ensure that this was its aim.

Therefore, the principles and guidance within the document have been followed as far as possible to achieve the aim of meeting the Council's Vision for the site. Where there are inevitable areas of discrepancy, these are explained, and the rationale provided within this tracker document.

Key Areas of Variance

The principles and guidance within the document have been followed as far as possible to achieve the aim of meeting the Council's Vision for the site. Where there are inevitable areas of discrepancy, these are explained, and the full rationale provided within the tracker document.

The main areas of disparity between the SPD and the proposed development relate to:

- > The scale and density;
- > The 50% target for family housing;



Charlton Riverside SPD Tracker – Executive Summary

- > The 35% target for affordable housing; and
- > The full re-provision existing employment space.

Rationale for Variances and the Benefits of the Proposed Development

The detailed responses behind the rationale for the deviation from some of the above-mentioned guidance is set out within the detailed tracker.

As stated previously, the SPD is a guidance document and it is not possible, nor should it be expected to fully address every element of this guidance. There are wider planning policies to consider at the national and local level and there is always an 'on balance' decision to be made on detailed planning issues that relate to any scheme and the proposed development, which will deliver a number of significant benefits, is no different.

The following is a summary of rationale behind the variances between the SPD and the proposed development.

Scale

It should be noted that high quality design and placemaking have been at the heart of the scheme proposals from the outset.

The scheme comprises 4-8 storeys. Whilst this remains below the overall maximum height (10 storeys) in the SPD for the entire Charlton Riverside area, it is acknowledged that it does exceed the guidance (3–4 storeys) for this part of the area.

However, this variance needs to be considered in the context of the wider benefits that flow from the scheme. These changes are evident and can be supported for the following reasons:

- > This level of development is deliverable and achievable in a way that supports the human-scale in the locality, whilst ensuring this vitally important strategic brownfield site maximises its potential in this location.
- > Viability is a key factor in terms of delivery and it should be noted that there are a significant number of benefits that this scheme will deliver which align with the need to provide a sufficient critical mass on the site which allows these benefits to be provided. These include:
 - The delivery of 10% affordable homes across the site. It should be noted that a further reduction in massing/floor areas/unit numbers would result in the inability to deliver any affordable homes.
 - The provision of a significant level of high quality flexible commercial space to support a mix of uses across the site, significantly increasing the potential employment yield of the site.
 - High quality landscaping across the site, including significant improvements to east – west permeability.
 - Provision of technical acoustic solutions within the scheme that will ensure that residents are not adversely affected by traffic noise along Westmoor Street.
 - Highly advanced sustainability features.



Charlton Riverside SPD Tracker – Executive Summary

- > The scheme has also had to respond to its flood zone status and a suggested three-storey datum height would be very difficult, if not impossible to achieve in this location.
- > The scheme is carefully designed to ensure that the wider aspirations of the Charlton Riverside area are met and not fettered by the development coming forward.

Overall, the scale of development is considered to be entirely appropriate in this location to ensure that all the above-mentioned benefits can be provided within the scheme and that we can demonstrate that we comply with national planning policy which strongly emphasises the need for best and most efficient use of brownfield land. This is particularly important in an area such as Charlton Riverside, which is an Opportunity Area in the London Plan and is looking to provide a substantial number of new homes and flexible commercial floorspace. This cannot and will not be achieved if the critical mass on site is not achieved to subsidise the numerous components, which allow for a high standard of design and quality of placemaking that are promoted within the scheme.

The massing has been developed to respond to the emerging context of the Charlton Riverside Area. The heights of the buildings have been considered to ensure amenity spaces, the green link and the courtyard space receive adequate daylight and sunlight. The massing of the northern building creates a defined taller slender block on the intersection of Mirfield Street and Westmoor Street, a key focal point along Mirfield Street, part of the future East-West Route. The massing lowers from 8 storeys to 6 storeys on the majority of the building towards the intersection of Mirfield Street and Eastmoor Street.

Density

The residential density of the proposed scheme is 252 u/ha (or 685 hr/ha). Whilst this is slightly greater than the figure in the SPD, the proposed development is considered to be appropriate for its context, having regard to the emerging schemes, infrastructure, access to services and amenities within the Charlton Riverside masterplan, the provision of high-quality residential accommodation with significant amenity space, and the contribution to the aspiration of the SPD for a sustainable mixed-use community.

The London Plan (2021) is clear that the optimum density of a development should be design-led, and defined with other factors such as context, layout, residential quality, existing and planned public transport, and capacity of surrounding infrastructure taken into consideration, in a move away from the density matrix. It should be noted that the density figures should only be used as a starting point and guide rather than as an absolute rule so as to also take proper account of other objectives, especially for dwelling mix, environmental and social infrastructure, the need for other land uses (e.g. employment or commercial floorspace), local character and context, together with other local circumstances, such as improvements to public transport capacity and accessibility.

Family Housing

The scheme includes 10.1% family homes. The number of such family sized homes has been maximised. Increasing this figure further will affect the total number of homes which can be delivered and it should be noted that proportionally the larger homes generate a lower revenue than the smaller ones, which would in turn have a significant detrimental impact on the viability and deliverability of the scheme.



Charlton Riverside SPD Tracker – Executive Summary

The overall provision of a mix of homes in this location is considered to be a good mix, which can viably be supported to help facilitate the regeneration of this strategically important Opportunity Area.

Furthermore, the provision of the family sized homes has been focused within the affordable element of the scheme, where the need is greatest. This results in an affordable housing provision of 14.9% when calculated by habitable rooms.

The housing mix will help to provide a genuinely mixed and balanced community within Charlton Riverside and, as we have stressed throughout this process, Aitch Group firmly supports the housing mix as a sustainable way forward for the creation of a new neighbourhood.

Affordable Housing

It is acknowledged the provision of 10% affordable housing is below the London Plan and Charlton Riverside SPD target of 35%, although it should be noted this provision meets the minimum requirement set out by the NPPF.

A Financial Viability Assessment, prepared by Redloft, has been submitted in support of the application. In summary, it demonstrates that the scheme is not economically viable. It is therefore not financially possible in planning terms for this scheme to provide any additional affordable housing, Section 106 obligations or other planning gain contributions. The assessment shows that proceeding with this development is a financial risk to the Applicant. Notwithstanding this, the Applicant is making an offer of 10% affordable housing (with a tenure split in accord with RBG's preferred tenure split of 70% social/affordable rent and 30% intermediate housing) on a without prejudice basis in recognition of the Council's aspirations for affordable home provision.

Re-provision of Existing Employment Space

The Site is currently occupied by a number of single storey industrial warehouses and open storage space for car repair/breaker (Use Class B2/B8). The existing buildings provide 3,387 sqm (GEA) of B2/B8 floorspace. The proposal is for a mixed-use development including 1,295.6 sqm (GIA) of commercial floorspace (Use classes E, F2 and B8) and 149no. new residential dwellings.

As previously stated, the Charlton Riverside Masterplan SPD (2017) allocates the site for mixed use, with B1 uses on the ground floor and residential development on upper floors.

To enable the implementation of the masterplan and the redevelopment of the site the existing businesses on the site will need to vacate. To ensure that the existing businesses are supported during this time, a package of support services has been devised to help them relocate to new premises.

The scheme will also support the wider redevelopment ambitions of the Charlton Riverside Masterplan to help any other existing businesses on surrounding development plots to be supported to lease the new commercial space created by this development.

The package for existing Eastmoor Street Commercial tenants includes the following level of support:

- A subsidised and bespoke agency package to assist existing tenants/ businesses with finding suitable alternative premises (a grant of £1,500 per business per relocation has been suggested).
- An electronic liaison service to link existing tenants with active commercial agents in the immediate area – this could go through a specialist commercial agent or a site finder.



Charlton Riverside SPD Tracker – Executive Summary

- Continued communication to all tenants of suitable instructions for available units within the wider area.
- The offer of professional advice and assistance in negotiating terms on new premises.
- The recommendation of reputable local solicitors.

It is proposed the above measures can be captured as a planning obligation within any s106 agreement for the proposed development.

Furthermore, the re-provision of 1295.6 sqm (GIA) of commercial floorspace will result in an increase in the number of potential employment opportunities. Using the figures set out in the HCA's Employment Density Guide (November 2015), it has been calculated that the scheme will deliver a significant uplift in local employment, the site currently only provides around 20 to 30 local jobs whereas the proposal would have the potential of providing 53 jobs. The scheme will also create additional jobs during the construction phase of the project.

Conclusion

Overall, the scale of development is considered to be entirely appropriate in this location to ensure that all the aforementioned benefits can be provided within the scheme and that we can demonstrate that we comply with national planning policy which strongly emphasises the need for best and most efficient use of sustainably located brownfield land.

This is particularly important in an area such as Charlton Riverside, which is an Opportunity Area in the London Plan and is looking to provide a substantial number of new homes and a sustainable future for numerous businesses to exist and work in harmony with the new homes in this newly created neighbourhood.

This aspiration cannot be delivered if the critical mass on site is not achieved to subsidise the numerous components, which allow for a high standard of design and quality of placemaking that are included within the proposed development to meet the overall objectives of the SPD.



Land at Nos. 6, 61-81 and Coopers Yard, Eastmoor Street and Nos. 6 & 10 Westmoor Street, Charlton, London, SE7 8LX

Project No: 19-180 (Scheme B)

Charlton Riverside SPD Tracker

November 2021

1 Introduction	Compliance with SPD	Notes / Comments
<p>1.8 How to use this SPD</p> <p>The design principles, grouped into Themes that relate to the Vision and Objectives for Charlton Riverside, are intended to provide guidance for developers as to what is expected from their proposals, in respect of a number of key components that would, typically, be addressed at the planning application stage. These are high level design principles and developers will be required to supply sufficient detail to show how they respond positively to the guidance and meet the objectives of the Vision for Charlton Riverside.</p>	<input checked="" type="checkbox"/>	<p>It is important to stress here that the SPD is intended to provide <u>guidance</u> for developers to ensure that developments respond positively to the guidance in order to meet the objectives of Charlton Riverside.</p> <p>The SPD should not be used as a 'blueprint' for detailed planning applications because there is a considerable amount of further detailed available to developers and applicants relating to the specifics of their site/sites, that would have been available at the time the SPD was written.</p> <p>The SPD is written in a way that means that land ownership boundaries are largely ignored and is therefore aspirational in many ways. The delivery of the fundamentals of this vision can and should be applied to all development proposals coming forward and this duty has been taken extremely seriously from the outset of their planning application work. However, it is not possible to fully replicate the exact requirements of the SPD and nor was the SPD written in a way that was meant to ensure that this was its aim.</p> <p>Therefore, the principles and guidance within the document have been followed as far as possible to achieve the aim of meeting the Council's Vision for the site. Where there are inevitable areas of discrepancy, these are explained, and the rationale provided within this tracker document.</p>
2. Vision & Objectives	Compliance with SPD	Notes / Comments
<p>Objective 1. An economically active Charlton Riverside</p>	<input checked="" type="checkbox"/>	<p>At ground floor a total of 1,295.6 m² of flexible commercial space is proposed (flexible employment, retail and community uses). Located at a designated Local Centre, the</p>

<p>Employment provision will be as important to the regeneration of Charlton Riverside as new housing. There will be 4,400 additional jobs in a range of different industries and at a range of scales. The correct space will be ensured to facilitate this employment growth. The proposed mix of uses, including retention of existing heavier, industrial uses and provision of a range of new workspace units and housing types and sizes, will encourage a wide spectrum of people to live and work in Charlton Riverside.</p>		<p>Mirfield Street building has the potential to provide a cafe at the prominent intersection of Mirfield Street and Eastmoor Street which, being dual aspect, can also overlook the quieter gardens of the green link. Two further units provide the opportunity to create retail and workspace within this building. A small, local convenience store will provide for the day-to-day shopping needs of new residents. In the southern building a variety of sizes of commercial units have been provided with the potential to have mezzanine level office space.</p> <p>Based on the HCA Employment Density Guide (November 2015), it has been calculated that there is likely to be a net gain of minimum 23 jobs on site, which assists with the overall numbers across Charlton Riverside.</p>
<p>Objective 2. A residentially diverse Charlton Riverside</p> <p>Provide a range of housing types and tenures, including a significant proportion of family housing, to ensure that local residents have the opportunity to be part of the new development. Housing will be provided at a human scale (typically varying between 3 to 6 storeys, allowing for 10 storeys in some areas) and actively contribute to a sense of place.</p>	<p><input checked="" type="checkbox"/> / <input type="checkbox"/></p>	<p>The scheme provides a range of housing types and tenures, including 10% affordable housing (the maximum viable).</p> <p>The scheme comprises 4 to 8 storeys, with most of the proposed development at 6 storey or lower. Whilst this remains below the overall maximum height (10 storeys) in the SPD for the entire Charlton Riverside area, it is acknowledged that it does exceed the guidance (3–4 storeys) for this part of the area.</p> <p>However, this variance needs to be considered in the context of the wider benefits that flow from the scheme. These changes are evident and can be supported for the following reasons:</p> <p>The proposals are a result of detailed pre-application discussions and engagement, including two CABE Design Review Panels and detailed review by the Council’s Design Officer. The proposals respond positively to this feedback (specifically to the proposed heights) and will deliver a high quality, well considered part of the new Riverside neighbourhood. Further detail is contained within the submitted Design and Access Statement.</p> <p>There is a clear townscape justification for the small element of 8 storey built form proposed. The SPD proposes a Local Hub at this location and this building will help define this location as important part of the emerging Riverside neighbourhood, with retail and</p>

		<p>community uses proposed at the ground floor. Further detail is contained within the submitted Heritage, Townscape and Visual Impact Assessment.</p> <p>This level of development is deliverable and achievable in a way that supports the human-scale in the locality, whilst ensuring this vitally important strategic brownfield site maximises its potential in this location.</p> <p>Viability is a key factor in terms of delivery, and it needs to be restated that there are a significant number of benefits that flow from this scheme, which align with the need to provide a sufficient critical mass on the site which allows these benefits to be provided. These include:</p> <ul style="list-style-type: none"> ○ The delivery of 10% affordable homes across the site. ○ Making efficient use of a brownfield, previously-developed and allocated site to deliver new homes to address the significant housing shortage in London ○ The Riverside has been allocated for redevelopment for a number of years. To date, no planning applications have been approved which would see the SPD Vision delivered. These proposals represent a deliverable piece of the SPD and will ensure the SPD objectives are delivered upon. ○ The provision of a significant level of commercial space to support a mix of uses across the site. ○ High quality landscaping across the site, including significant improvements to the riverfront and the opening up of this area to future and existing residents in the locality. ○ Provision of technical acoustic solutions within the scheme that will ensure that residents are not adversely affected by traffic noise along Westmoor Street. ○ Highly advanced sustainability features. ○ The scheme has also had to respond to its flood zone status and a suggested three-storey datum height would be very difficult, if not impossible to achieve in this location. ○ The scheme is carefully designed to ensure that the wider aspirations of the Charlton Riverside area are met and not fettered by the development coming forward.
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		<p>Overall, the scale of development is considered to be entirely appropriate in this location to ensure that all the above-mentioned benefits can be provided within the scheme and that we can demonstrate that we comply with national and regional planning policy which strongly emphasises the need for best and most efficient use of brownfield land. This is particularly important in an area such as Charlton Riverside, which is an Opportunity Area in the London Plan and is looking to provide a substantial number of new homes and flexible commercial floorspace. This cannot and will not be achieved if the critical mass on site is not achieved to subsidise the numerous components, which allow for a high standard of design and quality of placemaking that are promoted within the scheme.</p> <p>It should be noted that any further reduction in massing/floor areas/unit numbers would result in the inability of the proposals to deliver any affordable homes and other planning benefits and contributions.</p> <p>The massing has been developed to respond to the emerging context of the Charlton Riverside Area. The heights of the buildings have been considered to ensure amenity spaces, the green link and the courtyard space, receive good levels of light whilst also being overlooked and resulting in safe places to use throughout the day.</p> <p>The massing of the northern building creates a defined taller slender block on the intersection of Mirfield Street and Westmoor Street, a key focal point along Mirfield Street the future East-West Route. The massing lowers from 8 storeys to 6 storeys on the rest of the building towards Moore Park.</p> <p>The southern building sits a minimum of 14 metres from the northern building increasing to 21 metres across the green link. A large south facing courtyard, 23.5 metres wide is formed at first floor level. The landscaping of the courtyard creates an intimate, relaxing feel in contrast to the external city and river views from the homes. The landscaping of the courtyard creates an intimate, relaxing feel in contrast to the external city and river views from the homes. It is surrounded by three blocks of 4, 5 and 6 storeys in height.</p>
Objective 3. A connected and accessible Charlton Riverside	<input checked="" type="checkbox"/>	<p>Parking and vehicular use has been restricted on the site in order to promote a sustainable dependence on public transport, cycling and walking.</p>

<p>Create new, public-transport accessible routes and walkable neighbourhoods throughout the development area, increase accessibility to the Thames Path, re-design Woolwich road to prioritise public transport, pedestrians and cyclists, and create a green bridge or green link connecting Charlton Riverside with areas south of the Woolwich Road</p>		<p>The site occupies a key location on the proposed East-West Route and also the secondary green link. The proposals step back into the site red line ownership boundary and facilitate a revised junction arrangement at Mirfield St, Eastmoor St and Eastmoor Place. This facilitates the SPD aspiration to close/restrict Eastmoor St and improves the relationship to the adjacent Moore Park. This also delivers on the SPD aspiration to utilise existing and historic routes to deliver the E-W route, whilst also ensuring this part of the E-W route is delivered (likely under a s278 agreement by the Applicant) and fixed.</p> <p>The proposals also facilitate a much-improved relationship from the emerging Riverside neighbourhoods to Moore Park. This is a fantastic piece of open green space that is currently forgotten about and largely unused. By providing walking and cycling links across our site (to the north, middle and southern parts of the site, greater permeability is being provided which will significantly improve the relationship and attractiveness of Moore Park as a place for future residents to use.</p> <p>In terms of existing site ownership and existing road layout, the secondary green link has the potential to naturally develop as a continuation of the existing Penhall Road. This placement allows for the secondary green link to extend through the centre of the site and be animated by residential entrances and edged by the active frontages of the commercial units.</p>
<p>Objective 4. An integrated and lifetime ready Charlton Riverside</p>		
<p>Ensure appropriate provision of schools and community facilities within neighbourhoods to ensure development benefits all residents and provides facilities within neighbourhoods to support community life. Ensure that there is sufficient and flexible space provided within the development blocks and movement network for potential changes of use over time to meet the needs of the community over its lifetime; with open spaces designed to meet the recreation and leisure needs of all age groups, from young children to the elderly.</p>	<input checked="" type="checkbox"/>	<p>The SPD shows schools in other locations across the Riverside area. The CIL monies associated with this development will help to deliver them.</p> <p>The public realm includes a green link at ground level and buildings include a green space at podium level plus roof terraces, thus providing 2778.8 sqm of communal amenity space. On-site child play space, exceeding the requirement for children of all age, will be provided within the green link at ground floor, the courtyard area on podium, and on the roof terraces, for a total play space provision of 1105 sqm. Moore Park is directly adjacent to the east of the Site and Maryon Park is located across Woolwich Road, 200m south-east of the Site. These parks also include play facilities for older children.</p>

		The commercial floorspace has been designed to be flexible, with the option to sub-divide units to accommodate the needs of future tenants.
<p>Objective 5. A well-designed Charlton Riverside</p> <p>Ensure that the design and layout of new development, including building heights, is appropriate to the topography and neighbourhood character of the area. High quality design, build, materials and finish will characterise built development at Charlton Riverside. Design of housing and employment uses will be able to accommodate change of use over time. As part of this comprehensive approach to the integration of uses across Charlton Riverside, and the retention of some existing industrial activities, there will be a need to define and manage buffer zones between such uses and any new residential led development.</p>	<input checked="" type="checkbox"/>	<p>The proposal includes two distinct buildings on the site, the northern building defining the proposed local hub and the southern building with a central green courtyard. The buildings are separated by a generous new green link linking Westmoor Street to Moore Park. The northern building facilitates the new E-W link bus route/road to the north providing an active frontage to both the new road and the green link behind. The southern building provides active frontages to the green link, Moore Park, Westmoor Street and the yard space adjoining the neighbouring site to the south.</p> <p>The buildable site area has been impacted by the redesign proposed for the roads that surround the site. The proposed E-W link to the north of the site has had to be considered as well as proposals for the improvement of the park edge along Eastmoor Street.</p> <p>The massing has been developed to respond to the emerging context of the Charlton Riverside Area. The heights of the buildings have been considered to ensure amenity spaces- the green link and the courtyard space receive adequate daylight and sunlight.</p> <p>The scheme uses high quality materials that reference the industrial heritage of the site. The predominant building material proposed is brickwork in two complimentary tones plus an additional glazed brick for use within the commercial frontages. The buildings and the landscaping proposals aim to reflect the historical past of the site.</p> <p>The design principles of the scheme will help to complement the proposed characteristics of the area and will form a strong bond which creates a neighbourhood feel and sense of community in the longer term.</p> <p>The submitted Heritage, Townscape and Visual Impact Assessment (HTVIA) includes a detailed assessment of how the proposals will sit within the immediate area of the site, but also in the much wider context. It makes reference to the topography of the area from the higher level Maryon Park to the lower-level Riverside context. The HVTIA demonstrates the proposals do sit well within this context and will lead to a well-designed Charlton Riverside area.</p>

Height	<input checked="" type="checkbox"/>	It is acknowledged that the scheme exceeds the guidance (3 to 4 storeys) within the SPD. The justification for this is set out above, in Section 2 (Objective 2). Without repeating this justification, it is worth reiterating that high quality design and placemaking have been at the heart of the scheme proposals from the outset and the proposals have responded positively to extensive and detailed CABE DRP feedback and Officer feedback.
Objective 6. A sustainable and resilient Charlton Riverside Capable of adapting to change. Ensure that natural assets, including water supply and disposal, habitat and open space are managed appropriately, and new development is designed to maximise energy efficiency of buildings and the wider environment through measures to combat climate change. Public space will be carefully located, and well designed and managed, consisting of a combination of new public open space, enhancement of existing open space and provision of accessible private open space.	<input checked="" type="checkbox"/>	The scheme will include a range of carefully considered technical and environmental measures. These include: <u>Energy</u> <ul style="list-style-type: none"> - Air Source Heat Pumps - Solar PV panels - A central energy centre serving entire development - Passively reducing carbon production through an efficient building envelope - Optimised façade design will be implemented to reduce energy demand whilst achieving good internal daylight and sunlight, and mitigating overheating risk in the summer months - External balconies will be designed to support solar shading strategy and provide private amenity - Dual aspect flats, allowing for cross ventilation and good daylight and sunlight levels <u>Transport</u> <ul style="list-style-type: none"> - Cycle parking for all users including residents, employees and visitors - Improved pedestrian routes - Improved cycle routes - Low car parking on site to deter car usage and car restrictions in place for future residents - Car club spaces and memberships for all future residents - Travel plans to encourage a car free development - Provision of electric car charging points – 20% of spaces will have ‘active’ charging facilities from the outset with the remaining spaces having ‘passive’ provision, in accordance with policy.

		<p><u>Landscape and Ecology</u></p> <ul style="list-style-type: none"> - Enhancement of existing Moore Park - SuDS drainage incorporated in landscaping design - New green route to provide new wildlife corridors and connection to Moore Park - Tree lined streets and terraces to provide canopy cover, wind mitigation and seasonal interest <p>Facilitating the SPD aspiration to close Eastmoor Street and improve the relationship with Moore Park.</p> <p>New child play areas across the site.</p> <p>A net biodiversity gain over the existing site situation, including tree planting and green roofs.</p>
<p>Objective 7. A viable and deliverable Charlton Riverside</p> <p>A programme of phased development will ensure that the requisite infrastructure is put in place to support the intensification of housing, employment and community uses. Royal Greenwich will work with developers on appropriate delivery mechanisms and potential meanwhile uses to maximise community benefit. Developers will be required to contribute to the costs of open space, education, transport, etc., and development will only be permitted where the appropriate infrastructure is provided up front.</p>	<input checked="" type="checkbox"/>	<p>In addition to the infrastructure that is to be provided on site and at the site boundaries as part of the proposals (existing highway reconfiguring, pavement enhancements etc.), there will also be further provisions through S.106 provisions and CIL monies. In combination the proposals will provide improved infrastructure in the immediate area, as well as improved infrastructure in the wider area.</p>
2.3 Charlton Riverside - Living, working Neighbourhood	Compliance with SPD	Notes / Comments
<p>A Place to Live & Work</p> <p>The range of employment uses will include industrial workshops, small scale start-up and grow-on office space, creative industry studios, and small-scale retail and leisure provision, as well as health and education facilities. The masterplan makes provision for an</p>	<input checked="" type="checkbox"/>	<p>At ground floor a total of 1,295.6 m² of flexible, commercial space is proposed. The northern building has the potential to provide a café/retail/community use at the prominent intersection of Mirfield Street and Eastmoor Street which, being dual aspect, can also overlook the quieter green spaces of the proposed green link. Two further units provide the opportunity to create retail and community spaces within this building. A small, local convenience store could provide for the day-to-day shopping needs of new</p>

advanced manufacturing and employment cluster in the middle of the site and a creative industries/start-up hub at the eastern end. Employment uses, including studios and workshops, will be fully integrated into new development to accommodate existing businesses on Charlton Riverside and businesses wishing to relocate there, this may include heavier industries incorporated into certain building blocks		residents. In the southern building a variety of sizes of employment spaces have been provided.
Medium-rise Family Housing Residential development at Charlton Riverside will include at least 50% family housing (3+ bedrooms), in line with the provisions of the Core Strategy (Policy H2). Charlton Riverside is designated as an Opportunity Area in the London Plan, with the potential to deliver a significant number of new homes. However, the location and context for development at Charlton Riverside, including low levels of public transport accessibility, and allied to the requirement for family housing, requires an appropriate urban form. Low-to-medium rise, high density development, as characterised by the mansion block form, allows for flexibility of living spaces, supports shared green spaces and provides high, yet liveable densities for sustainable, walkable urbanism.	☒	<p>The scheme includes 10% family homes as part of the residential mix.</p> <p>The number of such family sized homes has been maximised. A viability assessment has been submitted that demonstrates this is the maximum amount that can be delivered. This level is comparably greater than other proposals in the Riverside area. Increasing this figure further will have a significant detrimental impact on the viability and deliverability of the scheme.</p> <p>The provision of the family sized homes has been focused within the affordable element of the scheme.</p> <p>Section 2.2 (part 2) above, clearly sets out the significant benefits of the scheme and the overall provision of a mix of homes in this location is considered to be a good mix, which can viably be supported to help facilitate the regeneration of this strategically important Opportunity Area.</p> <p>The housing mix will help to provide a genuinely mixed and balanced community within Charlton Riverside and, as we have stressed throughout this process, Aitch Group firmly supports the housing mix as a sustainable way forward for the creation of a new neighbourhood in this location.</p> <p>Furthermore, the application has a large proportion of 2 bed 4 person homes. The London Plan recognises that this type of home is suitable for families. Taking this into consideration, the overall percentage of family homes is much greater.</p>

<p>Streets & Squares</p> <p>Medium rise, mansion block housing will enable development to follow the grid alignment of historical routes, and to include a series of green squares, providing shared public and private space suitable for children and families. Streets and squares will connect residential development to its surrounding context.</p>	<input checked="" type="checkbox"/>	<p>A mansion block layout is proposed, to follow the grid alignment of historical routes and the aspirations of the SPD Vision. In this regard, the proposal includes two distinct buildings on the site, the northern building accommodating the proposed new local hub and the southern building with central courtyard. The buildings are separated by a generous new green link to Moore Park. The northern building facilitates the new E-W route to the north providing an active frontage to both the new road and the green link behind. The southern courtyard block provides active frontages to the green link, Moore Park, Westmoor Street and the more commercial and service focussed space adjoining the neighbouring site to the south.</p> <p>New public realm around the site has been designed to act as an extension to Moore Park. The harsh, industrial landscape will be softened by green landscaping and street trees which will also increase the biodiversity of the area. Penhall Gardens is proposed as a lively, green and sustainable environment where residents can relax whilst children play. The landscaping will also help to foster a local community where residents recognise and chat to neighbours whilst entering and leaving their homes.</p>
<p>Transport</p> <p>Transport networks will encourage walking and cycling and social interaction. Treatment of streets will follow Transport for London's Healthy Streets Framework: people will feel safe and there will be plenty to do and see. Streets will be easy to cross, with shade and shelter, and places to stop. This will minimise noise and air pollution, as well as increasing accessibility and affordability.</p>	<input checked="" type="checkbox"/>	<p>The site ensures that the phased delivery of the East-West Route is achievable; it also delivers the first phase of a green link to Moore Park, in order to improve connectivity within the area for pedestrians, cyclists and motorists. Potential improvements to public transport provisions, bus routes, will create further links to the wider city for residents. Parking and vehicular use has been restricted on the site to promote a sustainable dependence on public transport, cycling and walking. Further detail is contained within the suite of Transport and Highway related submission documents prepared by Ardent Consulting Engineers.</p>
<p>Existing Character & Sense of Place</p> <p>Large land parcels were allotted to factories such as GA Harveys, Siemens, British Ropes and United Glass Bottles. Two companies, Stone Foundries and Cory's Barge Works, have historic links to the site. The maximum height of buildings on site is six storeys. Remnants of the original boundaries and network of</p>	<input checked="" type="checkbox"/>	<p>The proposed buildings have been designed to draw on the industrial heritage of the site and wider area. The proposals utilise and promote the existing and historic routes and adopted highways to deliver improved connectivity across the new Riverside Neighbourhood.</p>

paths remain. New development will reflect the historical context and industrial nature of the site, with buildings at a similar scale		The materials proposed have an industrial aesthetic being predominantly brick, in three complimentary tones. Further detail of the proposals is included within the Design and Access Statement.
Views Buildings will be oriented perpendicular to the river, where possible, to maximise views towards and across the river, and to create natural routes from the interior to the river front. This will also protect the views from the north of the river to Maryon Park and Shooters Hill.	<input checked="" type="checkbox"/>	Buildings on the site have been orientated perpendicular to the river and to respect the existing adopted highways and routes. The northern building has been designed to optimise views to the river, and the southern building benefits from the views to the adjoining Moore Park and Gilbert's Pit/Maryon Park further to the south. Further detail is included within the submitted Design and Access Statement.
Buffer Zones New housing development and conversions should incorporate in the design and layout, protection against noise, air quality, lighting and/or vibration to preserve the amenity of future residents. Locations close to busy transportation links and those that are adjacent to safeguarded wharves are particularly vulnerable. Buffer zones, using a variety of materials and treatments, and with a limited range of uses, will make the transition between heavy industry and surrounding mixed use development.	<input checked="" type="checkbox"/>	<p>A Noise Assessment was undertaken to assess the suitability of the Site for the proposed development with regard to noise and vibration. The assessment identifies the main sources of noise to be due to commercial activity from surrounding premises and to road traffic on local roads: Woolwich Road to the south, Westmoor Street to the west and Eastmoor Street to the east.</p> <p>In this regard, the scheme includes a number of technical acoustic solutions, in order to ensure that residents are not adversely affected by noise. Closed windows will be necessary to achieve the suitable amenity sound levels, during both the day and night. Up-rated double glazing and relatively standard ventilation is suitable to achieve recommended internal noise levels across the majority of the development, with exception of properties with line of sight to Westmoor Street, which will require slightly up-rated ventilation. Consideration has also been given to the potential for adverse noise impact during overheating conditions; where residents may open windows to control temperature.</p> <p>The Site is located within the RBG borough-wide Air Quality Management Area. An Air Quality Assessment concludes that assuming good dust control measures are implemented during the demolition and construction phase of the development, potential air quality impacts are predicted to be not significant. Pollutant concentrations were tested through dispersion modelling. The location of the Site was considered suitable for the proposed</p>

		end-use without the inclusion of protective mitigation techniques to protect future users from poor air quality. As a result, the scheme is considered Air Quality Neutral, and no further action will be required to offset excess development emissions.
Neighbourhood Centres The district centre at Anchor and Hope Lane will create a new high street, with shops, offices, and health and community facilities. Neighbourhood centres near the river, in the centre of the site and to the west will provide, respectively, a focus for leisure, small scale retail and links to the existing community.	<input checked="" type="checkbox"/>	The site occupies a key location as part of an identified local centre / local hub. In accordance with the SPD, the proposals provide 1295.6 m ² of flexible commercial space at ground floor including retail and community uses within the northern building to respond to the local needs. Moreover, the green link created through the centre of the site from west to east offers a pedestrian and play space connection towards Moore Park.
4. Development Concept	Compliance with SPD	Notes / Comments
4.1 Development Concept	<input checked="" type="checkbox"/>	Complies with the development concept plan with the provision of new East - West routes/permeability, Green links, and improvements to the access to the Thames Path riverside promenade.
4.2 Movement Network The Development Concept provides a high-level framework for the spatial planning of Charlton Riverside. In broad terms, the Development Concept provides direction as follows: <ul style="list-style-type: none"> • Identifies where development opportunity exists; • Identifies those parts of Charlton Riverside where it is not expected that there will be significant change in built form or land use; • Shows the network of primary connections, including new and upgraded roads; • Sets out a rationale for a series of mixed use neighbourhood and local centres to provide for the day-to-day needs of residents, employees and visitors; • Proposes locations for new community hubs, focused on education sites, both new and existing, 	<input checked="" type="checkbox"/>	<p>The site occupies a key location on the proposed East - West Route and also the secondary green link. The new E-W route is proposed to pass to the north of the site and continue east and, whilst a significant step back into the application red line is required, the E-W route has been facilitated by utilising the existing highway network, whilst also delivering on the SPD Vision of closing Eastmoor Street.</p> <p>In terms of existing site ownership and road layout, the secondary green link has the potential to naturally develop as a continuation of the existing Penhall Road. This placement allows for the secondary green link to extend through the centre of the site and be animated by residential entrances and edged by the active frontages of the commercial units.</p> <p>From the inception of this project, there has been a real emphasis to ensure that the correct infrastructure is in place to ensure that an appropriate movement network is provided for, both in the interim position and to meet the wider site aspirations. The need to consider these routes in the longer term has framed the proposed blocks within the site. Legal land ownerships and adopted highways and access routes are significant</p>

<p>and</p> <ul style="list-style-type: none"> Identifies a Green Infrastructure strategy comprising a network of spaces (both new and existing) linked by a series of green routes and streets with a focus on providing improved access to and use of, the River Thames, via a new promenade along the line of the Thames Path 		<p>constraints that have not been considered in detail within the SPD. Our proposal facilitates the delivery of a key movement route for the SPD.</p>
<p>4.3 Development Opportunity</p> <p>The central section of the site has the greatest potential of development, with much of the existing commercial activity either replaced by mixed use or more intensive forms of commercial/ industrial activity. The protected wharf based in this area will remain in place.</p>	<input checked="" type="checkbox"/>	<p>Proposals are for a mixed-use development with an increase in commercial activity and the number of jobs able to be accommodated at the site.</p>
<p>4.4 Centres and Hubs</p> <p>Figure 4.7 gives direction as to the location of the main centres (neighbourhood and local) and their relationship with the surrounding area.</p> <p>A Local centre is located close to Charlton Riverside Park. The emphasis will be more on providing local services, retail and community services.</p>	<input checked="" type="checkbox"/>	<p>The northern part of the site adjacent to Mirfield Street is proposed as a local centre in the SPD. Recognising this the proposed retail and community uses at ground floor in Mirfield Street could include a café, convenience store, child nursery, or community space.</p>
<p>4.5 Green and Blue Infrastructure</p> <p>There is currently a deficit of open space at Charlton Riverside, and the introduction of a significant residential population will require additional provision of open space, sports facilities and children's play. In addition, the site's waterfront location, allied to new development that provides improved access to the waterfront, means that there</p>	<input checked="" type="checkbox"/>	<p>All homes will be provided with a private balcony or garden to meet the National Technical and London Plan Standards.</p> <p>On-site child play space will be provided, to exceed the requirement for children play of all age. This provision is met within the green link at ground floor, the central courtyard on podium, as well as the roof amenity areas. Moore Park is directly adjacent to the east of the Site and Maryon Park is located across Woolwich Road, 200m south-east of the Site. These parks also include play facilities for older children.</p>

<p>will also be a need to create spaces along the Thames which complement development and celebrate the river.</p>		
<p>4.6 Character Areas</p> <p>The key factors influencing the extent and nature of each of the character areas are as follows:</p> <ul style="list-style-type: none"> • Proximity to the River Thames and strategic open space; • Quantum of employment/commercial activity relative to new residential development, and • Spatial relationship with existing communities. <p>A set of high-level design and development principles will apply across all the character areas, albeit with the potential for localised variation in their application, depending on context.</p> <p>In respect of the ‘Villages, East and West’, the SPD states that:</p> <p>These character areas have a large residential component, with a rich mix of complementary uses, such as schools and community facilities, interspersed with significant commercial activity at ground and lower floors on key routes and frontages. There will be a greater proportion of houses within the residential mix and a focus on family provision in these areas. The public realm, including the mix of street types and the design and management of public space, will create a more intimate, village feel. Each of the Village character areas will look south, as well as north, making connections with existing residential areas via a re-designed Woolwich Road. These character areas are important interfaces</p>	<p>☑</p>	<p>The proposed scheme is a residential-led mix use development with a significant commercial activity on the ground floors, which provides an active frontage to all adjacent key routes.</p> <p>The scheme includes 10% family homes. The number of such family sized homes has been maximised. The proposals represent the largest proportion of family homes to date on the Riverside, and when combined with the proposed level of 2-bedroom 4 person homes, represents a significant proportion of the new homes proposed.</p> <p>The green link created through the centre of the site from west to east offers a pedestrian and ecological connection towards Moore Park. It is a multifunctional space that strengthen the relationship with the park and introduces enhanced urban greening at ground floor. The proposed landscaping on the podium provides an inclusive space for residents. It seeks to create a sustainable, ecological, and biodiverse environment, as well as bringing visual interest. At the southern boundary of the Site, the servicing road shared with the neighbouring development will also be planted and will act as a green route towards the park.</p>

between the new and the existing neighbourhoods of Charlton.		
5.0 Theme 1 - An Economically Active Charlton Riverside	Compliance with SPD	Notes / Comments
5.1 Introduction Proposals will need to provide suitable alternative employment spaces on site, not only to replace existing job numbers/businesses but also to create opportunities for new employment and business development.	<input checked="" type="checkbox"/>	<p>At ground floor a total of 1295.6 m² of flexible, commercial space is proposed. The northern building has the potential to provide a cafe at the prominent intersection of Mirfield Street and Eastmoor Street which, being dual aspect, can also overlook the quieter gardens of the green link. Two further units provide the opportunity to create retail and workspace within this building. A small, local convenience store will provide for the day- to -day shopping needs of new residents. In the southern building a variety of sizes of employment spaces have been provided.</p> <p>Based on the HCA Employment Density Guide (November 2015), it has been calculated that there is likely to be a net gain of minimum 23 jobs on site, which assists with the overall numbers across Charlton Riverside.</p>
5.2 Existing Land Use The Charlton Riverside Masterplan Employment and Heritage Study suggests that the current uses are ‘mixable’ – more so than other key industrial locations studied in London. Integration will be possible between residential and heavier industries as well as more common mixable industries such as creative art hubs, retail and office spaces.	<input checked="" type="checkbox"/>	<p>As well as including flexible commercial floorspace, mitigation measures will be incorporated to ensure acoustic protection is afforded to the new residents.</p>
5.3 Strategic Industrial Land and Protected Wharves The Core Strategy recognises the protected status of Riverside Wharf, but also allows for a process of review with respect to its use and its protected status. In the longer term, if activity at Riverside	N/A	

Wharf significantly reduces, its protected designation may be reviewed via the London Plan.		
5.4 Future Development - Principles for Retention and Creation of Employment Use New development will be expected to maintain or re-provide equivalent employment floor space within B1 and B2 Use Classes; and significantly increase job densities within B Use Classes.	☒/☑	<p>It has not been possible to fully re-provide the equivalent commercial floorspace. This is because of the requirement to provide a mix of uses and a significant proportion of new homes, new public realm and amenity space.</p> <p>However, it should be noted that everything possible has been done to support an increase in job densities in the locality and although the actual floorspace is being reduced, the quantum of jobs is expected to increase by minimum 23 jobs. In total, about 53 jobs are provided.</p>
Managed workspace and accommodation suitable for start-up enterprises and SMEs will be actively pursued in any development proposals.	☑	The proposed development includes flexible commercial floorspace, which could be used by start-up enterprises and SMEs.
Re-location strategies showing how existing businesses can be suitably accommodated;	☑	<p>The site currently contains a small number of existing businesses located in poor quality not fit for purpose buildings. To enable the implementation of the masterplan and the redevelopment of the site these existing businesses will need to vacate – this has always been the case and was known by the Council when they allocated the Riverside area.</p> <p>To ensure existing businesses are supported during this time, a package of support services has been devised to help them relocate to new premises.</p> <p>The scheme will also support the wider redevelopment ambitions of the Charlton Riverside Masterplan to help any other existing businesses on surrounding development plots to be supported to lease the new commercial space created by this development.</p> <p>The package for existing Eastmoor Street Commercial tenants includes the following level of support:</p> <p>A subsidised and bespoke agency package to assist existing tenants/ businesses with finding suitable alternative premises (a grant of £1,500 per business per relocation has been suggested).</p> <p>An electronic liaison service to link existing tenants with active commercial agents in the immediate area – this could go through a specialist commercial agent or a site finder.</p>

		<p>Continued communication to all tenants of suitable instructions for available commercial spaces within the wider area.</p> <p>The offer of professional advice and assistance in negotiating terms on new premises.</p> <p>The recommendation of reputable local solicitors.</p> <p>It is proposed the above measures can be captured as a planning obligation within any s106 agreement for the proposed development.</p> <p>Further details on these measures is contained within the Employment Statement and Relocation Strategy submission document.</p>
<p>5.4 Future Development - Distribution of Employment Uses by Character Area</p> <p>A number of employment activities, including food and drink production, small-scale manufacturing and makers and leisure uses, have the potential to be integrated into future development at Charlton Riverside.</p> <p>The SPD states that within the 'Villages', the type of proposed employment activity includes '<i>start-up and grow-on space, small scale manufacturing, retail, leisure, community, health, education.</i>'</p>	☑	<p>The proposed development includes flexible commercial floorspace, which could be used as start-up or grow-on space or for small-scale manufacturing.</p>
5.5 Relationship with Protected Wharves	N/A	
6. Theme 2 - An Economically Active Charlton Riverside	Compliance with SPD	Notes / Comments
<p>6.1 Introduction</p> <p>The housing delivered at Charlton Riverside will focus on the delivery of family housing and a significant proportion of affordable housing in keeping with the targets set out in the Core Strategy and the London</p>	☒	<p>The scheme provides 10% three bed homes. The rationale for providing this level family homes is set out in section 2.3 (family housing) above.</p>

<p>Plan. Specific attention will have to be given to development in close proximity of the safeguarded Angerstein, Murphy's and Riverside Wharves. Development around the safeguarded wharves will need to be designed to minimise the potential for conflict and ensure the operations of the wharves are not prejudiced.</p>		<p>The scheme provides 10% affordable housing which is considered the maximum viable as per the Financial Viability Assessment. The provision of the family sized units has been focused within the affordable element of the scheme.</p> <p>Development is not in close proximity to the safeguarded wharves.</p>
<p>6.2 Housing Type & Tenure</p> <p>Charlton Riverside will be comprised of a rich mix of different housing types to suit the needs of residents at all stages of life. More specifically, the Royal Borough is committed to a minimum of 50% of units built being provided for family housing. This is evidenced in policy H2 of the Core Strategy (2014), which outlines the requirements for different housing types across Royal Greenwich.</p>	☒	<p>The scheme provides 19.8% three bed homes. The rationale for providing c.20% family homes is set out in section 2.3 (family housing) above.</p>
<p>The Royal Borough also requires 35% affordable housing, with the actual quantum, type and mix to be addressed on a phase-by phase or parcel-by-parcel basis, as appropriate, and subject to the test of viability</p>	☒	<p>A Financial Viability Assessment, prepared by Redloft, has been submitted in support of the application. In summary, it demonstrates that the scheme is not economically viable. It is not financially possible for this scheme to provide any additional affordable housing, Section 106 obligations or other planning gain contributions. The assessment shows that proceeding with this development is a financial risk to the Applicant.</p> <p>Notwithstanding this, the Applicant is making an offer of 10% affordable housing (with a tenure split in accord with RBG's preferred tenure split of 70% social/affordable rent and 30% intermediate housing) on a without prejudice basis in recognition of the Council's aspirations for affordable home provision.</p> <p>It should be noted that any further reduction in massing/floor areas/unit numbers would result in the inability of the proposals to deliver any affordable homes.</p>
<p>6.3 Scale, Massing & Density</p>	☒	<p>The proposed buildings range between 4 and 8 storeys. It is acknowledged that this is greater than the 3-4 storeys, however the development, which mainly 6 storeys in height</p>

<p>Typically, building heights will vary between 3 and 6 storeys (although there is scope of buildings up to 10 storeys in certain locations), with the detailed consideration at design proposal and planning stages to ascertain what is appropriate for a specific location. It should be noted that a notional maximum building height in any given location does not mean that all buildings in that location should seek to achieve that height. Variations in height within and between individual development blocks will help to give character to development, break up individual massing and allow light penetration into the interior of blocks.</p> <p>For an urban location, with PTAL 0-5 a density range of 35-260dph is proposed in the London Plan Density Matrix (bear in mind, the potential uplift in PTAL is not guaranteed, so the change to a scenario with 35% of the site at PTAL 4-5, as per Figure 6.3, might not be achievable). Overall, the average net density across the site, based on the proposed SPD layout (Figure 6.4), is 146 dwellings per hectare. In any case, planning application must propose density that is based on the PTAL score as it stands at the time of application</p>		<p>with only a slender element of 8 storey, is still considered appropriate in the SPD within the Riverside area.</p> <p>The scheme has a density of 252 dwellings per hectare. Whilst this is slightly greater than the figure in the SPD, the proposed development is considered to be appropriate for its context, having regard to the emerging schemes, infrastructure, access to services and amenities within the Charlton Riverside masterplan, the provision of high-quality residential accommodation with significant amenity space, and the contribution to the aspiration of the SPD for a sustainable mixed-use community. The design has evolved following detailed pre-application discussions and design reviews by CABE and has resulted in a successful and considered part of the emerging Riverside neighbourhood.</p> <p>Further detail is set out in Section 2.1 (part 2) above and details of the pre-application design evolution and stakeholder engagement is contained within the submitted Design and Access Statement and Statement of Community Involvement.</p>
7. Theme 3 - A Connected and Accessible Charlton Riverside	Compliance with SPD	Notes / Comments
<p>7.1 Introduction</p> <p>The proposed movement strategy for Charlton Riverside is based on the historic network of paths and boundaries. It assumes a moderate increase in public transport (mainly bus) services and promotes</p>	<input checked="" type="checkbox"/>	<p>The proposed development ensures that the phased delivery of the east-west route is achievable and utilises the existing highways and routes; it also delivers the first phase of the green link to Moore Park, in order to improve connectivity within the area for pedestrians, cyclists and motorists. Potential improvements to public transport provisions, bus routes, will create further links to the wider city for residents. Parking and</p>

public transport, walking and cycling. This is in line with the principles of the Royal Borough's transport strategy documents and the Transport for London's Healthy Streets framework.		vehicular use has been restricted on the site to promote a sustainable dependence on public transport, cycling and walking.
7.2 Hierarchy of Routes Primary – these are the main routes through the site; often strategic, as well as providing local access and circulation, they are the main public transport routes and will have a boulevard character; Secondary - these routes are the mainstay of the movement network, and give the wider site its structure and form; they may also have public transport and, while narrower in section than the primary routes will still be substantial movement corridors; Tertiary - these routes, typically, relate to residential areas and have a character and section that reflects that setting and context, and Shared Surface/pedestrian priority - these are a form of tertiary street, but one where the relationship between vehicles and pedestrians is more balanced; these routes will often sit within development parcels providing access, circulation and penetration into the block, for example, in the development parcels adjacent to the River Thames.	✓	The road network of the proposed scheme has closely followed this hierarchy of routes. Further detail is contained within the submitted Transport Assessment documents and the Design and Access Statement.
7.3 Contribution to the character of Charlton Riverside Physical activity and social connectivity should be designed in from the outset, promoting active travel and providing the easiest and most affordable way	✓	Pedestrian and cycle routes to the site have been considered and developed with the design. Car parking has been kept to minimal levels with the majority of parking spaces provided being for blue badge holders.

for the people who will live and work in the area to get more active and live healthier lives.

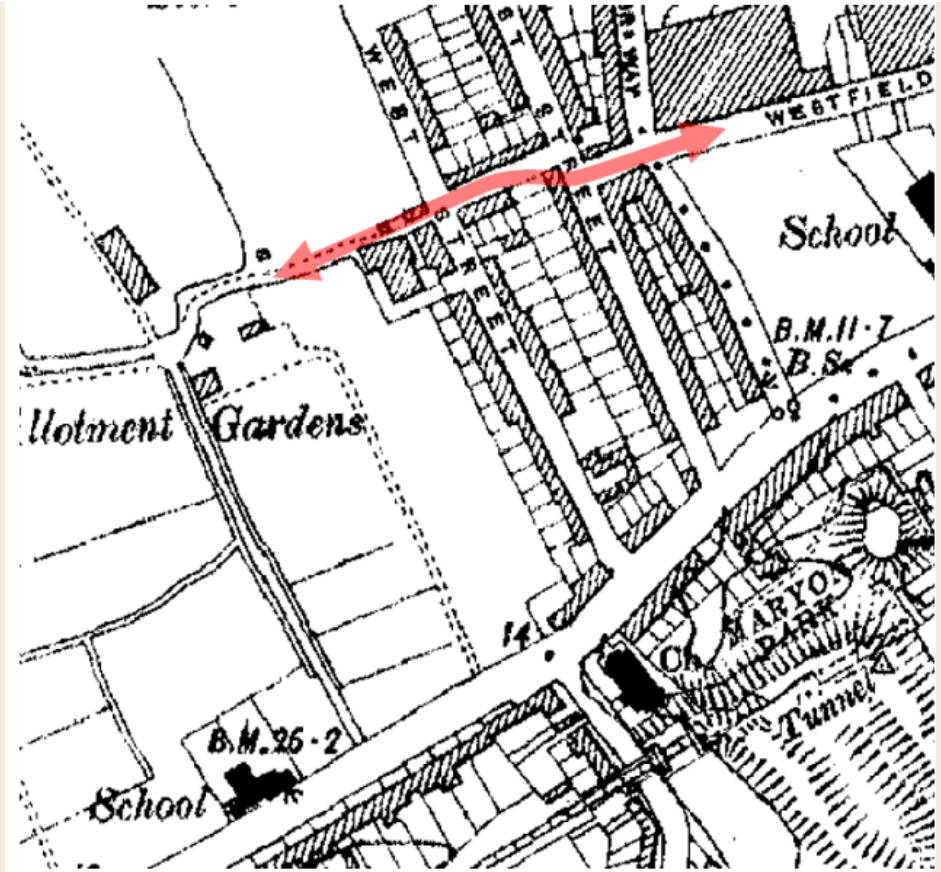
7.4 East-west Route

A core requirement of the Development Concept is the delivery of a new east-west route extending from the junction of Bugsby's Way and Anchor and Hope Lane to the junction of Warspite Road and Woolwich Road.



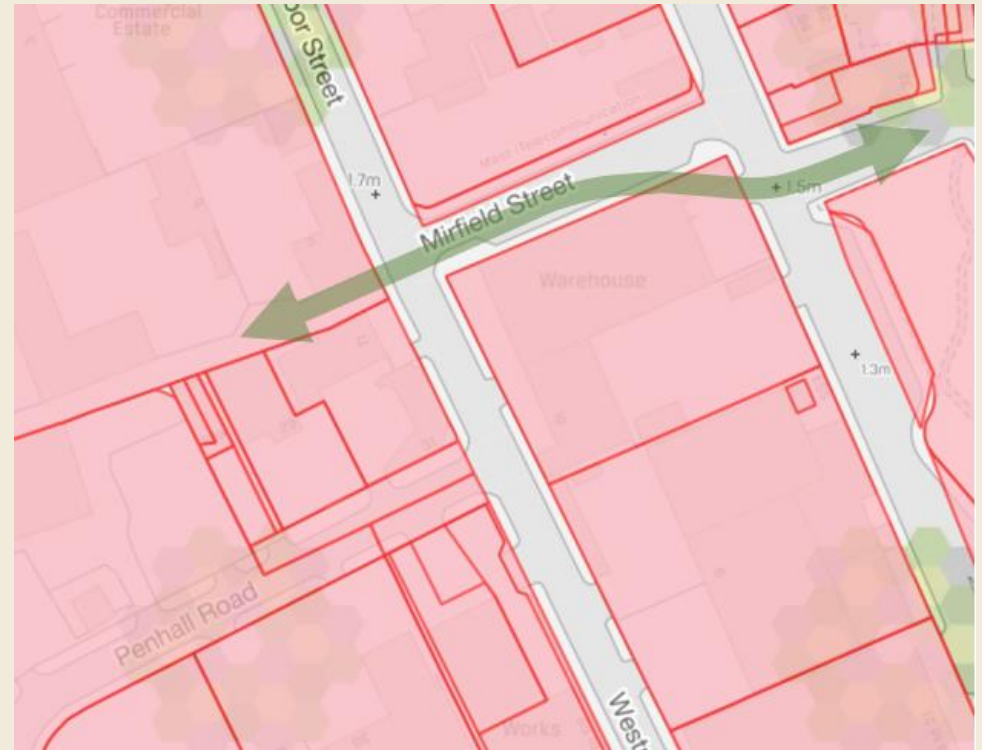
The SPD shows an indicative alignment in the illustrative drawings. In accordance with the SPD it is proposed that this is provided to the north of the Site extending from Eastmoor Place though to Mirfield Street, utilising the existing and historic routes and allowing continuation of this route to the west. In accordance with the SPD this route makes use of both '*existing routes*' and '*historic routes*', as shown on figure below which overlays the proposed east-west alignment with the historic mapping, effectively recovering this historic route for public use.

For land ownership and legal site boundary reasons, this route is also the most efficient way to ensure the delivery of the E-W route.



As shown on figure below, this east-west alignment respects property boundaries, in-line with the aspirations of the SPD as far as possible. Even though this alignment requires a significant proportion of land from the Applicant's land ownership, it has been facilitated to meet the aspirations of the SPD. It has also been tracked with buses in line with TfL's requirements and has been demonstrated that the arrangement works. To the west of Mirfield Street the east-west route runs along the southern boundary of Title no

SGL507495 (owned by RBG) utilising an existing and historic route, and avoids numerous separate small and sporadic land titles if this alignment were further south as shown in the image below



In order to secure the delivery of this E-W route, the most suitable solution has been presented which aligns with the SPD aspirations to utilise existing and historic routes and is also being facilitated by the proposed development.

It is also worth noting that this route location has been facilitated and proposed by Hyde Housing in their live planning application, where the E-W route would pass by the southern part of their application red line boundary.

<p>7.5 Public Transport</p> <p>The new east-west route will be designed to facilitate future bus services, and the Royal Borough will continue to work with TfL to look at a potential east-west rapid transit system. There is also the potential for additional bus routes to be added within the development, a possible example of this is shown in Figure 7.3.</p>	<input checked="" type="checkbox"/>	<p>The proposals allow for the extension of the 301-bus route (or a potential suitable alternative) which follows part of the example route shown in the diagram. Further detail is contained within the Transport Statement.</p>
<p>7.6 Walking and Cycling Routes</p> <p>The Development Concept for Charlton is predicated on the creation of a network of streets and public spaces that support non-vehicular movement with a higher degree of connectivity and which is safe, direct and convenient. A variety of different types of routes will be available for cyclists:</p> <ol style="list-style-type: none"> 1. Designated, segregated cycle routes within the carriageway which will be along primary and some secondary and can be on- or off-street. 2. Unsegregated on-street cycle routes along secondary and tertiary streets. 3. Defined routes through public open space and along Thames Path. 4. Undefined routes through public open space. 	<input checked="" type="checkbox"/>	<p>Cycle and pedestrian access through and around the site have been carefully considered, with the provision of safe routes/footways and the planting of street trees to create a pleasant environment.</p>
<p>7.7 Access to the River Thames</p> <p>Access to the River Thames is a priority and any development which seeks to inhibit, restrict or remove direct public access to the River Thames will not be permitted.</p>	<input checked="" type="checkbox"/>	<p>The layout of the buildings on the site, in a north south orientation, facilitates the creation of direct pedestrian routes to the river Thames. The orientation also allows for direct views to the river.</p>

<p>7.8 Thames Path</p> <p>Where the Thames Path sits immediately adjacent to the river, a minimum depth of 16 metres will be maintained between the river defence line (typically the river edge) and adjacent development/ building line. The intention is that a more open and accessible Thames Path, well overlooked by adjacent development, will create an attractive and well used public space adjacent to the River Thames. In addition, maintaining the 10m setback will allow greater access for inspection, maintenance, repair and replacement of the river defences (the 16m setback is required by the Environment Agency). In terms of its character, as it passes through Charlton Riverside, the Thames Path is envisaged as being a riverside promenade, opening out onto the River Thames and framed by mixed use development that creates vitality and attracts people to the river front.</p>	N/A	
8. Theme 4 - An Integrated and Lifetime Ready Charlton Riverside	Compliance with SPD	Notes / Comments
<p>8.1 Introduction</p> <p>Community infrastructure will need to be provided within reasonable walking distance from as many new homes as possible to ensure those who do not have access to their own transport can reach them easily and to encourage people to walk or cycle where possible, reducing reliance on private cars and promoting healthier lifestyles (in line with Core Strategy Policy CH2 Healthy Communities). In particular, family housing should, as far as practicable, be within reasonable walking distance of nursery and primary schools, local shops, play areas</p>	<input checked="" type="checkbox"/>	<p>The proposed commercial uses at ground floor in Mirfield Street include a café and a convenience store.</p>

and amenities (in line with Core Strategy Policy H5 Housing Design).		
8.2 Public Open Space Provision The expansion of the residential population will require additional provision of open space, sports facilities and children’s play. There will also be a need to create spaces along the Thames which complement and celebrate the river. A framework of local open spaces, serving their local neighbourhoods, will be interconnected by a series of green routes (which might be tree-lined streets or linear spaces) that integrates Charlton Riverside with its hinterland and also connects it with the River Thames. Along the river, an enhanced Thames Path would link a series of pocket parks arrayed along the southern bank of the river. These spaces would provide a series of destinations at key points, where development and open space combine to create attractive waterfront public space animated by occasional commercial activity.	<input checked="" type="checkbox"/>	<p>The proposals include the creation of a green link providing connectivity to the Park and ground floor play space. All streets around the development (Westmoor Street, the new East-West route and the southern servicing yard) will be tree-lined. This green link and tree-lined streets will contribute to the green framework mentioned by the SPD.</p> <p>The ground floor landscape is pivotal to the success of the project, connecting the scheme into the wider masterplan and providing a positive contribution to the local area which benefits both the existing and new communities. Emphasis has been placed on bringing the community into contact with nature, throughout the seasons. New trees and species rich planting will bring a softer, green edge to the development. A planted drainage swale provides a playful feature with stepping-stones creating a sense of adventure and exciting circulation between the spaces.</p>
8.3 Public Open Space Typology There will be an emphasis on accessible, multifunctional open space provided in close proximity to residential development, such that no home will be more than 5 minutes’ walk from a substantial open space (i.e., greater than 1 hectare and equivalent to a local park). In addition, there will be a finer grain of smaller spaces – pocket parks, rain gardens, local play areas and civic spaces within development parcels, providing very immediate and instantly accessible open space adjacent to	<input checked="" type="checkbox"/>	<p>The site is located at a key location for open space as it is adjacent to Moore Park. The creation of a green link through the site will deliver a multifunctional space interconnected with the park. It will provide play space, public space with active frontage for the retail and commercial units, entrance to the residential cores, and Sustainable Urban Drainage Systems (SUDS).</p> <p>Private open space is also provided within the development: the courtyard on podium at first floor within the southern block, and multiple accessible roof terraces on upper floors. Impermeable surfaces are minimised with the use of accessible and non-accessible green roof terraces across the scheme.</p>

residential development. It is also anticipated that private open space will be provided as part of residential development. Roof terraces and gardens should be considered to maximise access to usable open space and minimise impermeable surfaces. The amount and spatial distribution of each of the different types of open space will be addressed during pre-application discussions.		
8.4 Charlton Riverside Park The existing sequence of spaces leading from Woolwich Road to the Thames Barrier will be conjoined and expanded to create a new District Park. This enhanced space will include some components of land currently given over to employment uses. It will be big enough to accommodate the full range of open space functions and will serve as a resource for the adjacent primary and secondary schools.	<input checked="" type="checkbox"/>	The proposals include an enhanced landscape strategy to integrate within the existing and emerging natural environment and connect the development with Moore Park. The proposals ensure the scheme is future-proof with the evolution of the park to later form the Charlton Riverside Park.
8.5 Green Connection A key challenge along the Woolwich Road corridor is how to improve the connections across the Woolwich Road between Barrier Gardens and Maryon Park. There are three alternative options, a green bridge with a straight ramp, a green bridge with a zig-zag ramp and a Green Zone.	N/A	
9. Theme 5 - A Well-designed Charlton Riverside	Compliance with SPD	Notes / Comments
9.1 Introduction Charlton Riverside will become a new, mixed use neighbourhood, with the introduction of more than	<input checked="" type="checkbox"/>	The proposals fully support this aspiration and do everything possible to maximise the use of this strategically important brownfield site to maximise the number of homes and also to meet employment provision on the lower floors.

5,000 (and up to 7,500) new homes. Although large parcels of land will remain in commercial/industrial use, there will be a significant change in land use across the rest of the site to clusters of smaller scale retail, commercial and industrial at ground floor level, with residential blocks above, together with some purely residential blocks. Where possible, and appropriate, a mix of uses (including commercial and employment) will be delivered, integrated both horizontally and vertically within development parcels.		
Within the main body of the site, in addition to the neighbourhood and local centres, there will be the opportunity, for local retail which would be embedded within individual development parcels. These small, local shops would help to activate and animate street frontages, introduce non-residential uses at ground floor and provide for the day-to-day needs of resident and working populations in the immediate vicinity.	<input checked="" type="checkbox"/>	There is an opportunity for a small convenience store at the ground floor of the building facing Mirfield Street.
9.3 Other Commercial Activities With the introduction of a new resident population and an existing, and potentially greater, employee population, there will clearly be both a demand and an opportunity to dramatically increase the level, mix and quality of food and drink provision.	<input checked="" type="checkbox"/>	Café, retail and community uses are proposed on the ground floor at the intersection of Mirfield Street and Eastmoor Street.
9.4 Heritage Assets A strong planning and design rationale will be required for any proposals which result in the removal of buildings deemed to be heritage assets	<input checked="" type="checkbox"/>	Whilst the site is close to two locally listed former public houses (The Lads of the Village' and 'The Victoria'), and adjacent to a Conservation Area, there are no designated or undesignated heritage assets on the site itself.

and planning applicants are encouraged to engage early in the planning process with the Royal Borough on such sites.		
9.5 Residential 50% family housing delivered in a low-to-medium rise, high density form. Family dwellings might be individual houses (typically town houses over three floors), maisonettes or duplexes and larger apartments.	<input checked="" type="checkbox"/>	The scheme includes 10% family homes. The number of such family sized homes has been maximised. Further detail on this is shown above.
Charlton Riverside is almost entirely located within zones of flood risk, both fluvial and surface water. Housing proposals will have to make reference to this risk and show how it has been mitigated by detailed design.	<input checked="" type="checkbox"/>	All housing is located above the TE2100 flood level and this has a significant bearing on how the scheme has been designed. Please refer to the Flood Risk Assessment for further details.
9.6 Housing Design		
The provision of dual aspect dwellings should be maximised and, where single aspect dwellings are provided, they should not be north facing.	<input checked="" type="checkbox"/>	<p>We have provided dual aspect where possible (76% of total homes), otherwise dual orientation with single aspect limited to smaller dwellings.</p> <p>These homes have been maximised within the scheme factoring in the site-specific constraints that exist within the site.</p>
Rear boundaries of properties should not back onto streets.	<input checked="" type="checkbox"/>	Predominately flatted development with front doors to streets and amenity space adjoining communal gardens.
Dwellings should have access to private individual or communal open space, as appropriate, sufficient for the needs of residents, accessible and well-managed.	<input checked="" type="checkbox"/>	All homes have been provided with a private terrace or balcony (sizes to meet London Plan requirements) accessible from the living/kitchen/dining room; in some homes the master bedroom also has direct access to the terrace.

Development proposals will deliver the minimum required in terms of vehicular parking and maximise cycle parking	<input checked="" type="checkbox"/>	<p>In accordance with the London Plan (2021) the residential aspect of the development is car free, with disabled parking provided in line with the London Plan standards for both the commercial and residential aspects of the scheme. 20% of the bays will have active Electric Vehicle Charging Points (ECVPs), and the remaining will be passive.</p> <p>Cycle parking is provided in line with the GLA and TfL's current requirements. Further detail is contained within the submitted Transport Assessment and Design and Access Statement.</p>
Housing should be well-designed, attractive, distinctive and use a palette of materials that reflect local context and are durable, sustainably sourced and of high quality.	<input checked="" type="checkbox"/>	<p>The proposed buildings have a simple palette of materials drawing on the industrial heritage of the area. The predominant building material proposed is brickwork in two complimentary tones and an additional glazed brick for use within the commercial frontages.</p> <ol style="list-style-type: none"> 1. Light brickwork 2. Grey brickwork 3. Blue glazed brickwork 4. Dark metal panels <p>Grey brickwork is used to form the commercial plinth to the buildings providing unity to the ground floor pedestrian areas. Blue glazed brickwork highlights residential and commercial entrances at ground floor. The light buff brickwork highlights most of the residential accommodation on upper floors. The grey brickwork is also used on upper floors to introduce variation and visual interest and to highlight the 8-storey block.</p> <p>Metal cladding is also used punctually to create layering and depth to the facades. Windows are proposed as double glazed PPC aluminium black metal. Balustrades to balconies are proposed as PPC dark grey metal railings with copper handrail.</p>
9.7 Parking Development proposals will deliver the minimum required in terms of vehicular parking and maximise	<input checked="" type="checkbox"/>	<p>See 9.6 above. Car and cycle parking are provided out of sight on the ground floor/in the undercroft, in locations that do not interfere with the operations of ground floor, non-residential uses.</p>

cycle parking. The planning and design of high-quality parking facilities for all cycle users will provide fit-for-purpose, secure, well-located cycle parking, which is essential for supporting the development of cycling as a practical transport choice. There will be a variety of parking solutions, including on-street, on plot/integral to individual buildings, in undercrofts and full and half basements. Parking solutions should avoid creating dead frontages or structures that are out of scale with the surrounding development and should not interfere with the operations of ground floor, non-residential uses.		
9.8 Residential Development Parcels Typically, development blocks that comprise residential or residential led, mixed-use development will take the form of perimeter blocks with buildings aligned parallel to the street, resulting in a relatively high degree of continuity of frontage and enclosure of the street.	<input checked="" type="checkbox"/>	Both proposed blocks are aligned parallel to the street.
9.9 Residential Building Typologies		
Dual Aspect Maisonettes/Apartments	<input checked="" type="checkbox"/>	Larger family homes have, where possible, been located on the corners of blocks or are dual aspect across the width of a block. In each block the larger two bed and three bed family dwellings have been designed to be dual aspect. On upper floors the massing of the blocks has been stepped to create further dual aspect flats.
Single Aspect Apartments	<input checked="" type="checkbox"/>	Building blocks have been orientated to ensure a minimal amount of north facing single aspect homes. The majority of single aspect homes have either east or west orientation.
9.10 Ground Floor Uses	<input checked="" type="checkbox"/>	The proposal incorporates non-residential uses at Ground floor level, and therefore both blocks exceed the minimum height recommendation of 3.5m recommended on a primary

<p>The ground floor of buildings fronting onto primary routes and key nodes will have more generous floor to ceiling heights and servicing options that allow for both residential and non-residential uses.</p>		<p>route. This is not only for flood resilient reasons, but also to provide attractive and high-quality employment spaces.</p>
<p>9.11 Active Frontages</p> <p>Active frontages are essential to successful place-making, given their capacity to create character and foster community by providing the opportunity for people to meet and engage in exchange</p>	<input checked="" type="checkbox"/>	<p>The ground floor layout of the site has been designed to provide maximum passive surveillance of pedestrian routes via active commercial frontages. Mirfield Street is the main proposed east-west axis road. The proposed flexible commercial uses at ground floor in Mirfield Street would include a cafe, convenience store and workshop/office space. These uses will provide natural pedestrian footfall along this street in the daytime and also into the evening. Cores A & B front directly on to Mirfield Street and link through to Penhall Gardens.</p>
<p>9.12 Urban Grain</p> <p>At Charlton Riverside it is proposed that this ‘grain’ of development is relatively fine, meaning that parcels are smaller, with more frequent junctions of streets</p>	<input checked="" type="checkbox"/>	<p>The proposed development follows the urban grain shown in Figure 9.11, with buildings parallel to the line of the road and with a public face onto the street.</p>
<p>9.13 Views and Vistas</p> <p>There is considerable opportunity to create long and middle-distance views into, across and out of Charlton Riverside, and development proposals must show how these are captured, framed and protected. The Thames waterfront is important in that it affords broad views along the sweep of the river, out over the Thames Barrier and across to the Royal Docks. Views to the site, from either side along the south bank and from the north bank of the Thames are also important, and not only because of the potential for development to act as a visual stop along the waterfront</p>	<input checked="" type="checkbox"/>	<p>A Heritage, Townscape & Visual Impact Assessment has been completed to assess the impact of the proposals on the neighbouring and wider context.</p> <p>The assessment has found that the proposed development would not result in any harmful visual effects on the surrounding townscape. There would be a range of beneficial effects that would include moderate to major beneficial visual effects on local views from Thames Barrier Park as a result of the new buildings defining high quality frontage.</p> <p>There would be a moderate beneficial to moderate to major beneficial effect on people on the Green Chain Walk passing the junction of Eastfield Place and Westfield Street where the proposals would be seen in the context of the frontage of the locally listed former public house. From this location the scheme would replace an existing degraded industrial site with a high-quality frontage that is angled away to reveal the new east-west connection and the local centre. The ground floor café would provide visual interest and a</p>

		<p>focal point at the junction and the form, massing, materials and articulation would ensure that the buildings would provide an appropriately scaled and detailed frontage.</p> <p>Looking south along Westmoor Street, there would be a minor to moderate beneficial effect with a positive new frontage, appropriate to the aspirations of the CRM in marking the local centre and key route through the area. Approaching the site from the Thames Path, the development would only be perceptible in winter through the trees, when the tallest part of the scheme will mark the location of the local centre and enhance legibility.</p> <p>There would be nil or negligible effects on views from the wider surroundings area due to the level of screening.</p>
9.14 Development Along the Thames	N/A	The development is not located along the River Thames.
9.15 Streetscape and Public Realm <p>The new east-west link will need to be designed in such a way as to support key public transport infrastructure, set within a space whose character and quality reflect its location in the heart of a mixed-use neighbourhood with significant residential development. While the movement component of the space is important, the social component, i.e., the spaces where people linger meet, and conduct their daily business is an important aspect of the east west route.</p>	<input checked="" type="checkbox"/>	<p>Several options for the treatment of the east-west link have been discussed with adjoining landowners, the RBG and TfL. Our proposals outline and facilitate the most appropriate route which utilises the existing and historic routes and has been tracked for buses in line with TfL's requirements.</p> <p>The pavement width has been increased to 4m and a loading bay has been provided to safely service the development. The remaining area will be landscaped with street trees and furniture to enhance the environment and to provide a civic presence to the local centre.</p>
10. Theme 6 - A Sustainable and Resilient Charlton Riverside	Compliance with SPD	Notes / Comments
10.1 Introduction		
10.2 Water Management Strategy <p>The IWMS recommends a range of potential interventions at different scales, the aim being to</p>	<input checked="" type="checkbox"/>	A Flood Risk Assessment and Drainage Strategy has been prepared in support of the application. In summary, it demonstrates that:

enable strategic development sites to actively manage the water cycle and, especially, flood risk within their boundaries.

The Site is located within tidal Flood Zone 3, associated with the River Thames, which benefits from defences.

There is a residual risk in the event of a breach in the defences. Maximum predicted flood level for the 200yr breach event for the 2115 epoch is 6.128m AOD at the Site of interest. Commercial units are to be located at the ground floor. The first floor, which is proposed for residential use, is set at 6.475m AOD, 5cm above the breach level including freeboard. The 200 year plus climate change breach level being 6.428m AOD.

In the event of a breach, the Site will be surrounded by Extreme Hazard rating. If time allows and following the advice of the emergency services Site occupiers can evacuate the Site via Eastmoor Street, heading west along Woolwich Road and then south through Maryon Park. The section of Woolwich Road to the north of Maryon Park as well as the entire area south of Woolwich Road remain dry during the 200 year plus climate change breach event.

It is recommended that any levels below the flood level should incorporate flood resistance and/or resilience construction to minimise the potential damage that could be caused by flooding. Residential development is proposed to have safe refuge for occupants at higher levels.

The site is considered to be at a generally 'low' risk of pluvial flooding (less than 0.1% probability), although pockets of the site are shown to be a 'medium' risk. Eastmoor Street to the east and Mirfield Street to the north are located at 'high' risk (greater 3.3% probability).

Surface water run-off will be managed as part of the proposed surface water drainage strategy. Therefore, the risk of pluvial flooding is assessed as 'low'.

The surface water drainage strategy will reduce flood risk by restricting surface water flows in accordance with the London Plan. Storm water attenuation is provided for all storms up to and including the 1 in 100-year critical event (including a 40% allowance for climate change). The proposed development results in significant betterment over the pre-development scenario in terms of a reduction of surface water runoff.

		<p>A management company will be appointed to maintain communal areas, landscaping and shared SuDS throughout the development. All maintenance will be in accordance with the best practices and the CIRIA Manual C753.</p> <p>In conclusion, the FRA demonstrates that the proposals are consistent with the aims of the NPPF and the Planning Practice Guidance to the NPPF along with the aims of the Strategic Flood Risk Assessment. The Site will not be at significant risk of flooding or increase the flood risk to others.</p>
<p>10.3 Flood Risk Management</p> <p>It is expected that those development parcels immediately adjacent to the river will be responsible for the improvements to the river defences, in line with TE2100 requirements</p>	N/A	
<p>10.4 Urban Greening</p> <p>There will be a requirement to protect existing and introduce new street tree planting, create incidental open space and use roof space and wall space to introduce planting.</p>	<input checked="" type="checkbox"/>	<p>Tree planting is proposed around the buildings and on the road frontage to add to the green infrastructure of the local area, in order to significantly improve the amenity value of the site and the local area. These will be native species.</p> <p>Green biodiverse roofs are also proposed to increase opportunities for biodiversity.</p>
<p>10.5 Energy</p> <p>The Greener Greenwich SPD (2014) sets out a strategy showing how new development at Charlton Riverside can reduce its energy consumption and carbon footprint. The strategy is predicated on the following objectives:</p> <ul style="list-style-type: none"> • Be lean (use less energy) • Be clean (supply energy efficiently) • Be green (use renewable energy) 	<input checked="" type="checkbox"/>	<p>The development follows the energy hierarchy, heating hierarchy and cooling hierarchy. The development will install ASHP, which will remove any reliance on gas for the development. In addition, the PV system is the largest that the roof and proposals can accommodate. The proposals therefore represent the best on site savings and use of emerging technology for larger scale residential buildings – with no reliance on gas fired plant. The development will further achieve ‘zero carbon’ through an offset payment in line with the London Plan guidance.</p> <p>The development has the potential to achieve a Very Good BREEAM rating with a target score of 67.58%.</p>

10.6 Adaptability Ensuring that Charlton Riverside can adapt with changing demands of its population, climate change and new technologies.	<input checked="" type="checkbox"/>	<p>The proposals comprise a mix of unit sizes from studio to 3-bed family homes within different tenures, thus providing a range of high-quality housing products that will ensure the scheme is adaptable to evolving population demands.</p> <p>The proposals include a Greening Factor of 0.4463 indicating a good green coverage that will help the development to be more future-proof and resilient to climate change.</p>
11. Theme 7 - A Viable and Deliverable Charlton Riverside	Compliance with SPD	Notes / Comments
11.1 Introduction	N/A	
11.2 Land Ownership	N/A	
11.3 Phasing Figure 11.2 illustrates that anticipated phases of development.	<input checked="" type="checkbox"/>	<p>The sites fall in the area indicated as Phase 1.</p>
11.4 Infrastructure Delivery A range of infrastructure will be required to support the delivery of 5,000-7,500 new homes and an increase in employment space to support 4,000 new jobs in Charlton Riverside, and this is summarised in Table 11A, below. Key projects include: A new east-west link road, to improve internal access to parcels of land in Phase 1 to allow both access and act as the first part of a sustainable transport route through the site. Ideally, the east west route needs to be built prior to the redevelopment and intensification of the area, but this is dependent on the timing of redevelopment of sites; Flood alleviation;	<input checked="" type="checkbox"/>	<p>The site occupies a key location on the proposed East - West link road and also the secondary green link. The new E-W link road is currently proposed to pass to the north of the site and continue east. The proposals include appropriate road widths for the link, as discussed with TfL during the design development.</p> <p>Flood mitigation is provided within the development and complies with TE2100.</p>

11.5 Approach to Delivery The SPD sets out the delivery options that are open to RBG.	N/A	Development is delivered by the market, with control through the planning system;
11.6 Funding Sources	N/A	
12. Illustrative Masterplan	Compliance with SPD	Notes / Comments
12.1 Introduction	N/A	
12.2 Illustrative Masterplan	N/A	<p>The masterplan and SPD block arrangements are illustrative. They do not wholly respect land ownership boundaries and have been designed in this manner to guide the principles and Vision of the area.</p> <p>There needs to be a recognition that the SPD will not be delivered in line with the illustrative, high-level and non-detailed plans that are shown. There needs to be a degree of flexibility and recognition that in order to ensure a successful Riverside neighbourhood is delivered in line with the strategic objectives, the significant constraints of land ownerships need to be recognised and facilitated.</p>
12.3 Vision for Charlton Riverside		
The Royal Borough proposes that Charlton Riverside will be a new mixed-use neighbourhood, providing a mix of homes, jobs, education and leisure opportunities for the people of Royal Greenwich and beyond. The sketch view, opposite, is graphic representation of that vision and how the site might look as the vision, and its key objectives, are realised.	<input checked="" type="checkbox"/>	The scheme provides an appropriate mix of residential and commercial accommodation.
The new neighbourhood will have a sizeable component of family housing, which means three bed dwellings and above. Given the character and composition of nearby developments at Greenwich	<input checked="" type="checkbox"/>	The rationale for the number of family homes and height/density is explained above in sections 2.2 (part 2) and 2.3 (medium rise family housing).

Peninsula and Woolwich Arsenal, the Royal Borough is keen to see a different model of development, one that is focused more on larger, family dwellings set within a different type of urban environment. For that reason, the Royal Borough is promoting low to medium rise, high density development at Charlton Riverside, a model of development that is common in many parts of London

The development proposals respond to the guidance in the SPD and the scheme clearly demonstrates that place-making is at its heart. The scheme delivers an intensification in the number of jobs and provides a substantial contribution to residential provision in the area, as well as meeting a large number of the SPD aspirations and meeting the overall Strategic Vision and Objectives for the area.

The scheme demonstrates that the most efficient use of this brownfield site is being achieved.

Appendix 3. Statement of Community Involvement





STATEMENT OF COMMUNITY INVOLVEMENT
JUNE 2020



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INTRODUCTION

BACKGROUND

This report has been prepared by Lexington Communications, a specialist planning communications agency, on behalf of Aitch Group (hereafter referred to as ‘the applicant’). This document summarises the pre-application consultation that has been undertaken with the Royal Borough of Greenwich, local elected representatives, community groups and residents regarding the regeneration proposals for Land at Nos. 6, 61-81 and Coopers Yard, Eastmoor Street and Nos. 6 & 10 Westmoor Street, Charlton, London, SE7 8LX. (hereafter referred to as ‘the site’).

THE PROPOSALS ARE FOR:

Demolition of existing structures and erection of buildings between 6 and 10 storeys in height, comprising 202 residential units, 1350sqm B1/B8 flexible employment floorspace and 522sqm flexible retail and community uses (Use Classes A1 – A5 and D1) with associated landscaping and new public realm, access and infrastructure works, refuse and recycling storage, car parking and cycle parking and associated development.

Once the application for these proposals has been submitted to the Royal Borough of Greenwich, the applicant will continue to engage with key stakeholders in order to keep the local community informed on the progress of the scheme through the formal planning process, and address any questions or queries that may arise.

NATIONAL PLANNING POLICY

Chapter Four of the National Planning Policy Framework (NPPF) encourages developers to conduct early, positive and effective public consultation regarding development proposals, and the Applicant has taken a direct lead from Paragraphs 39 to 41, and 128 which state:

39. Early engagement has significant potential to improve the efficiency and effectiveness of the planning application system for all parties. Good quality pre-application discussion enables better coordination between public and private resources and improved outcomes for the community.

40. Local planning authorities have a key role to play in encouraging other parties to take maximum advantage of the pre-application stage. They cannot require that a developer engages with them before submitting a planning application, but they should encourage take-up of any pre-application services they offer. They should also, where they think this would be beneficial, encourage any applicants who are not already required to do so by law to engage with the local community and, where relevant, with statutory and non-statutory consultees, before submitting their applications.

41. The more issues that can be resolved at pre-application stage, including the need to deliver improvements in infrastructure and affordable housing, the greater the benefits. For their role in the planning system to be effective and positive, statutory planning consultees will need to take the same early, pro-active approach, and provide advice in a timely manner throughout the development process. This assists local planning authorities in issuing timely decisions, helping to

ensure that applicants do not experience unnecessary delays and costs.

128. “...Applications that can demonstrate early, proactive and effective engagement with the community should be looked on more favourably than those that cannot”

LOCAL PLANNING POLICY

National policy toward pre-application consultation is complimented by the Royal Borough of Greenwich’s own Statement of Community Involvement (2016), which emphasises that the Council encourages developers to engage with the Council and the community when submitting planning proposals.

This is set out in clauses 5.3 – 5.6 stated below:

5.3. Developers have a legal duty to consult local communities on very large scale development proposals prior to submitting a planning application. This should include local amenity groups and any constituted neighbourhood forums. For large scale proposals where there is no legal requirement to consult, the Royal Borough agrees with the suggestion in the National Planning Policy Framework that it should strongly encourage developers to involve the local community from an early stage. This may take a variety of forms such as local exhibitions, public meetings, circulation of leaflets or the creation of a well-publicised dedicated website, including a facility to make comments.

5.4. Community involvement should ideally be sought in two stages. Firstly, views should be sought from interested groups on what may be appropriate for a site so that these comments can, wherever possible, be taken on board before the proposals are drawn up. Secondly, further views should be sought on the evolving proposals before a planning application is submitted, so that changes can be made in response before submission, and also so that people are already aware of the background and the proposals when the Royal Borough carries out consultations on the application.

5.5. Where an applicant has arranged some community involvement prior to making a planning application, the Royal Borough expects a supporting report to be submitted setting out the type of community involvement undertaken, the views expressed and the changes made by the applicant in response; if no changes have been made, the report should explain why not.

5.6. Anyone proposing development of any scale is likely to find that early information of this via consultation and discussion with neighbours or others likely to be affected by the development, will ease the planning process.

APPROACH TO COMMUNITY ENGAGEMENT

OBJECTIVES

Throughout the pre-submission stage, the applicant sought to work with the local community by:

- » Engaging with the Royal Borough of Greenwich Council through pre-application discussions to further develop and evolve proposals;
- » Informing local elected representatives, local interest groups, site neighbours and residents living nearby about the proposals;
- » Engaging with the community on its proposals for the site, capturing feedback to inform the further progression of the scheme; and
- » Engaging with immediate local residents to identify and address resident questions and queries;

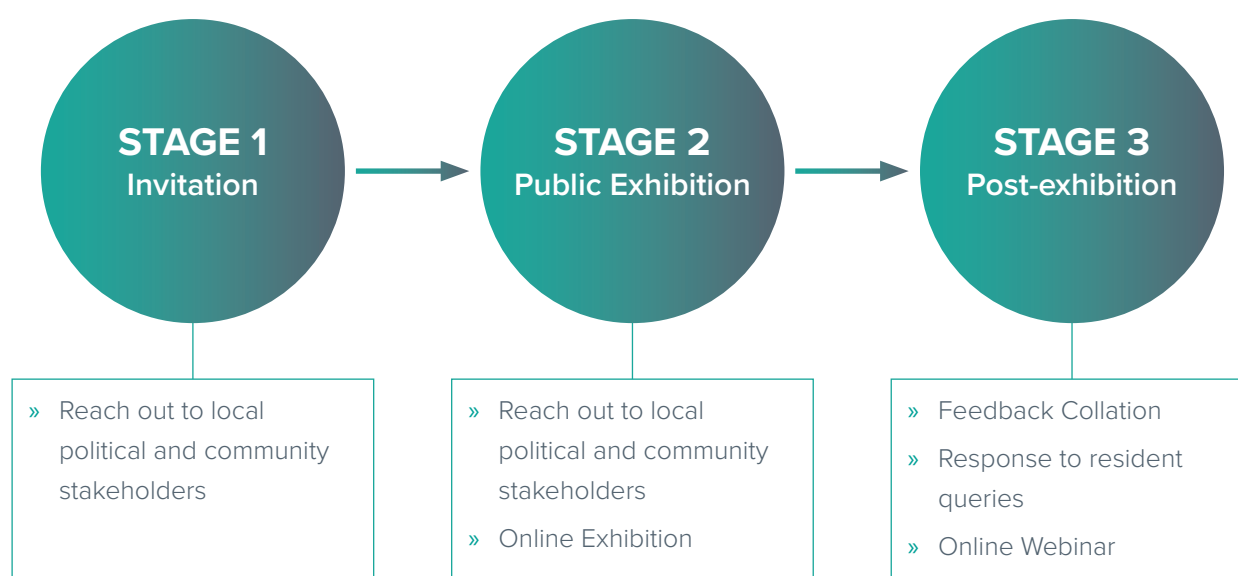
CONSULTATION DURING THE COVID-19 PANDEMIC

The COVID-19 pandemic has had an impact on almost every aspect of life in the UK, and this includes people's capacity and willingness to attend face-to-face public exhibitions on emerging development proposals in their areas.

With government guidance also encouraging the planning process to continue throughout the public health emergency, Lexington Communications and the wider project team put together an innovative consultation programme that engaged people from across the Charlton community, and secured feedback from them on the emerging proposals.

This approach encompassed a range of communication techniques, ranging from a letter to over 3,000 local homes and businesses to a virtual public exhibition and video and phone calls with a range of local political and community stakeholders.

This approach is outlined in the three-stage approach to engagement below:



STAGE 1 – INTRODUCTION

INITIAL ENGAGEMENT

From the very start of the project, the project team identified and sought to engage with the identified community stakeholders set out below. The aim was to introduce Aitch Group, the project team and the regeneration site, and to offer a potential meeting to discuss the emerging proposals:

- » Charlton Society
- » Charlton Together
- » Central Charlton Residents Association
- » Greenwich Planning Alliance
- » Greenwich Society
- » Greenwich Conservation Group
- » Royal Greenwich Trust School
- » Windrush Charlton Primary School

Following repeated engagement attempts, meetings were agreed with Windrush Charlton Primary School and the Royal Greenwich Trust School on 24th March and 31st March 2020 respectively. Unfortunately, both had to be cancelled due to the impending lockdown.

THE VIRTUAL PUBLIC EXHIBITION

In order to publicise the virtual public exhibition, which went ‘live’ on Tuesday 5th May 2020, the following activities were undertaken:

- » **Traditional Invitation:** An invitation to local people - 3,168 letters were hand delivered to local residents and businesses (see Appendix A), all addresses were within a 800-metre radius of the Eastmoor Street site (a map of the delivery area can be found at Appendix B).
- » **Social Media:** The project team also recognised that approximately 41,000 local residents of all ages lives in the three local postcode sectors (SE7 7, SE7 8 and SE18 5) covering the local area. Facebook adverts were then placed on the timelines of these individuals and those who clicked on the adverts were taken directly to the virtual public exhibition website
- » **Local Blog:** Also, separately to our own consultation work, the virtual public exhibition was also publicised by a local blog that is widely read by people living in Charlton, called ‘Charlton Champion’ (the blog post can be found at Appendix C).
- » **Invitation to political stakeholders** – The following political stakeholders were also invited to attend the virtual public exhibition by email and letter (Appendix D):

Ward Councillors for the site – Woolwich Riverside

- » Councillor John Fahy
- » Councillor Dominic Mbang
- » Councillor Jackie Smith

Neighbouring Ward Councillors for the site – Charlton

- » Councillor Gary Dillon
- » Councillor Gary Parker
- » Councillor Linda Perks

MP for Greenwich and Woolwich

- » Matthew Pennycook MP

Cabinet Members

- » Councillor Danny Thorpe – Leader of the Council
- » Councillor Sarah Merrill – new Cabinet Member for Regeneration and Growth



MEETING WITH STAKEHOLDERS

COUNCILLOR JOHN FAHY

Following our initial outreach to the local community at the start of the project in January 2020, the applicant and the wider project team held a video call with local Ward Councillor John Fahy on Friday 22nd May. This discussion introduced Aitch Group and the wider project team, the site, the consultation approach and the potential for a pre-submission 'webinar'. to discuss the proposals and the site.

Royal Greenwich Trust School

On 22nd May 2020 the project team also engaged via a video call with Caroline Longhurst, Gillian Kemp, and Christopher Philpott who are the current Headteacher, Associate Governor and Chair of the School. This discussion again introduced Aitch Group and the wider project team, the site and the consultation approach. It also covered the current and future ambitions for the school and how the School could best secure some of the Section 106 and CIL developer contributions that would result from any future approval of the emerging Aitch Group proposals.

STAGE 2 - THE VIRTUAL PUBLIC EXHIBITION

To make it as easy as possible for local people to find out about the proposals, and to facilitate broad engagement, the applicant ran an online public exhibition, found at the following website – www.eastmoorstreet.co.uk (Appendix E),

This virtual public exhibition provided all the exhibition materials expected from a physical exhibition – such as exhibition banners, a feedback mechanism and the full contact details of a named member of the project team – whilst making them publicly available in an easily accessible online format in light of the COVID19 regulations.

A focus on an online approach, but with the resident letter also giving those without the internet the opportunity to call a named member of the project to discuss the plans in detail if they would like, had the not inconsiderable advantage of increasing accessibility for many local residents.

A common concern regarding physical exhibitions is that many residents do not have the time to attend public exhibitions in the afternoons, evenings, or at weekends. However, by transferring the exhibition online, Lexington was able to broaden the scope of engagement for residents allowing residents to view the proposals at whatever time was convenient to them.

The virtual exhibition website was set out like a public exhibition event and was designed to be familiar to regular exhibition attendees, and people who might not be regular internet users. A screenshot from the website can be seen below:



Image depicting the homepage of the Consultation Website

The webpage had different 'hotspots' which would take the viewer to the different elements of the website. These hotspots included:

» **A set of consultation banners detailing the proposals**

- These were downloadable, and set out in landscape orientation, to aid their readability on a conventional computer screen

» **A range of indicative scheme CGIs**

- This 'carousel' of images showed the emerging proposals from a range of viewpoints on the existing road network

» **A download page where visitors could access:**

- The consultation banners
- The Council's supplementary planning document (SPD)
- Links to Aitch Group's corporate website and development brochure

» **A feedback form so visitors could provide any comments they had**

- This asked visitors to the website a range of questions about their area, and the proposals specifically

» **Contact details for any stakeholder that wished to discuss the site in further detail with a member of the project team**

- This included the full contact details of a named member of the project team

THE CONSULTATION BANNERS

The online exhibition gave the wider local community information on the plans, an opportunity to provide their comments, and discuss the proposals with the development team.

9 Exhibition banners (Appendix F) were displayed at the exhibition which provided the following information:

- » Welcome
- » Aitch Group and the Wider Project Team
- » The Charlton Riverside Masterplan
- » The Current Site
- » The Emerging Proposals
- » Our Vision (2 banners)
- » New Job Opportunities
- » Next Steps & Have Your Say

FEEDBACK MECHANISMS

In order to provide a method of feedback capture on the website, the applicant provided an online feedback form so the visitors could provide any comments or feedback they had. This was very clear on the website, having its own 'hotspot', and a copy of the online feedback form can be found at Appendix G.

Importantly, respondents provided their personal details to ensure that the site was compliant under current GDPR guidelines, with feedback collated in real time.

The site's visitors were asked the following questions:

- » What do you love about Charlton, and what do you think could be improved?
- » What are your views about the Masterplan area, and do you see it as a priority for Charlton?
- » Do you support the regeneration of this site for new housing and commercial space?
- » What sizes and tenures of housing would you like to see?
- » What companies would you like to see take up the commercial space on Eastmoor Street?
- » What would you like seen incorporated into the design of this development?
- » General Comments

COMMUNITY NUMBER AND EMAIL ADDRESS

The full contact details of a specific member of staff, Lexington's Head of Property and Local Government, Michael Stanworth, were available for members of the public to contact the project team should they have any enquiries. The following methods were employed:

- » **Email address:** The full contact details of Lexington Communications' Head of Property, Michael Stanworth, was placed on the letter inviting people to the online exhibition and on the exhibition website.
- » **Telephone:** Lexington's contact number, 07974 300 509 was also provided on the letter invite and the website.

STAGE 3 – EXHIBITION FEEDBACK

Our consultation event, which went ‘live’ on 5th May 2020, reached out to over 20,000 local people living in Charlton. Alongside a letter delivered to over 3,000 local homes and businesses in the local area, our Facebook adverts reached 17,004 Facebook users in three local postcode sectors. This resulted in 1,061 visits to our consultation website and a total of 12 feedback responses, with 9 feedback forms from the website and 3 ad hoc correspondences via email including from the Charlton Society and Charlton Together. All of these have been incorporated into this Statement of Community Involvement.

Headlines

The summary of the feedback is as follows:

In responding to the 7 questions given to visitors, the most numerous comment theme was support for the scheme.	<i>Respondents believed that the proposals offer a number of opportunities including providing much needed new housing and commercial opportunities. People also felt it fits well within the wider Charlton Riverside Masterplan, which they also support.</i>
Commercial opportunities were well received with a number of comments excited about the business and commercial opportunities the site would provide.	<i>Some commentators also had specific requests with the need for a local supermarket to be included as well as bars and restaurants onsite.</i>
The most widespread comment was regarding adherence to the Charlton Riverside Masterplan, with this being mentioned in 5 separate questions.	<i>A mixture of supportive and objecting comments asked that the scheme adhere strictly to the parameters set out by the masterplan. The majority of comments said that the site did so, whilst one comment said they felt it did not given the proposed scheme’s height and design.</i>
The most widespread comment regarding the design of the scheme was wishing to see more green spaces, with this being raised in 4 separate responses.	<i>All these comments wished to see more green space incorporated into the scheme, though it should be noted this was never the prevailing comment for any of the questions asked.</i>

Respondents also wished to see existing and small businesses be incorporated into the scheme, the heritage of the area be reflected in the proposals, local infrastructure concerns (such as additional GP surgeries) be addressed and more parking onsite.

Question One

1. What do you love about Charlton, and what do you think could be improved?

Analysis

The most popular comment was regarding growth, especially in connection with the Charlton Riverside masterplan area. The respondents want to see growth and regeneration, given that local industry has been declining for a number of years. A number of respondents said they had lived in the area for most if not all of their lives and so had witnessed this decline first-hand, and wished to see further development which would create jobs as well as new homes, so to build up Charlton Riverside.

3 comments also mentioned the ‘potential’ of the area. The respondents said Charlton has a lot of potential, saying they wished to see improvements in Charlton more widely. Again, this was influenced by local industrial decline being a reason for this site’s regeneration.

3 comments mentioned the history and heritage of Charlton Riverside. Two comments mentioned the strong industrial heritage of Charlton Riverside, suggesting that Charlton landmarks such as Charlton House (in Charlton Village, at the top of Charlton Church Lane) played an important role in reflecting the history of the area.

3 comments talked about green spaces. They all mentioned the current green spaces the area has and how this made it an attractive area to live in for current residents.

3 comments mentioned community, saying that they wanted the new development to help strengthen the bonds of the local community and have a community ‘feel’.

The final comments were mentioned individually. The majority of these comments were about art and culture – hoping to see this reflected in the design of the proposals. One respondent also hoped that these proposals would help small businesses, whilst another wanted to ensure that the development would be sustainable. One respondent said that they supported regenerating Charlton Village. The final theme was height where one respondent said they wanted to see low-rise, terraced housing onsite.

COMMENT	RESPONSE COUNT
Growth	4
Potential	3
History / Heritage	3
Green Spaces	3
Community	3
Activity	1
Sustainable	1
Art	1
Small Businesses	1
Regeneration	1
Height	1

Question Two

2. What are your views about the Masterplan area, and do you see it as a priority for Charlton?

Analysis

The comments were overwhelmingly in support of the Charlton Riverside Masterplan, with 5 of the 7 comments made saying they supported the Masterplan. In particular, they said it would help regenerate the local area improving facilities, transport and providing a sense of place to Charlton.

The consensus was that Charlton Riverside is run-down and in need of regeneration, and that this scheme fits into the plans by providing homes and commercial opportunities.

3 of the responders said specifically that the area had the potential for strong growth and said that they viewed the masterplan as a priority for Charlton. Respondents thought this was a great opportunity for housing and business.

2 respondents said that the plans for Charlton Riverside would hopefully improve local infrastructure, in particular transport and community facilities.

2 respondents said they hoped the masterplan would boost employment opportunities in Charlton Riverside. Similarly, 2 respondents said they thought the masterplan would bring greater commercial opportunities to Charlton.

2 respondents said that the development should reflect the parameters of the Charlton Riverside Masterplan.

The final comments were all mentioned once. One comment wanted the site to have its own identity, whilst another hoped that local art would be incorporated into the scheme. Similarly, another theme was a hope that the use of local art would help reflect the growing community in the area. The final comment praised the proposals for bringing forward much needed homes.

COMMENT	RESPONSE COUNT
Support	5
Potential	3
Infrastructure	2
Employment	2
Business	2
Masterplan	2
Identity	1
Transport	1
Art	1
Community	1
Homes	1

Question Three

3. Do you support the regeneration of this site for new housing and commercial space?

Analysis

This question showed overwhelming support for regenerating this site, with all 7 respondents saying they supported the development. There were some conditions for this support, including incorporating green space to improve air quality into the development and also to have an appropriate scale and mass. Some comments said the proposals would have a driving benefit for housing and businesses in the area.

2 respondents said that the development should reflect the parameters of the Charlton Riverside Masterplan.

The other comments only had single responses. One respondent said the proposals should include more cycling spaces and another said the proposals should not be too dense in order to increase unit numbers. One respondent said they hoped that the proposals would retain the existing businesses onsite. The final comments said they wished to see high quality housing which had affordable housing.

COMMENT	RESPONSE COUNT
Support	7
Masterplan	2
Cycling	1
Air Quality	1
Unit Numbers	1
Business	1
Housing Quality	1
Affordable Housing	1

Question Four

4. What sizes and tenures of housing would you like to see?

Analysis

2 respondents said they wanted to see affordable housing onsite, which conformed to the Council's guidelines on affordable housing provisions. One of these respondents said that affordable units are key to bringing in young professionals to the area.

2 respondents said they wished to see apartments as part of a mix of tenures (including houses).

2 respondents said they wanted houses as part of a mix of tenures.

2 respondents said they supported the tenure mix in the plans.

The single responses asked that the proposals' scale and massing not be too dense, that townhouses be included in the proposals, and finally expressing neutrality about what housing sizes and tenures came forward. The final response said that the development should reflect the parameters of the Charlton Riverside Masterplan.

COMMENT	RESPONSE COUNT
Affordable Housing	2
Apartments	2
Houses	2
Support	2
Density	1
Townhouse	1
Neutral	1
Masterplan	1

Question Five

5. What companies would you like to see take up the commercial space on Eastmoor Street?

Analysis

This question provided a range of different responses. The respondents either commented with general terms about what they wanted or specific brands. The supermarket brands have been combined under ‘supermarket’ in the above chart, which therefore received 7 responses overall. Of these responses 2 wanted a Sainsbury’s, 2 wanted a Tesco’s, 1 respondent each wanted a Waitrose or a Co-Op. 1 respondent mentioned they wanted a supermarket in general.

5 respondents mentioned businesses. In particular, 3 respondents asked for capacity for small, independent businesses whilst the final responder said they wanted space for existing businesses in the local area.

3 respondents said they wished to see more restaurants in the area, one respondent said they would like a restaurant on the riverside.

2 respondents said they wanted some commercial space for creative businesses, providing space for studios and galleries.

2 respondents also said they wished to see cafés incorporated into the commercial space on Eastmoor street.

The other responses were supportive of the economic regeneration and the creation of jobs. The respondents also requested a GP surgery, a local Boots pharmacy, a bar, a gym and more green space. The final respondent said the gym and green space should be contingent on the density of the proposals.

COMMENT	RESPONSE COUNT
Supermarket	7
Business	5
Restaurant	3
Creative	2
Café	2
Economic Regeneration	1
Jobs	1
GP Surgery	1
Boots	1
Bars	1
Green Space	1
Gym	1
Density	1

Question Six

6. What would you like seen incorporated into the design of this development?

Analysis

The most popular response for the design aspects of the development was a design that reflected and respected the heritage of the local, industrial area. One respondent said there should be a statue included that paid homage to the area’s industrial past.

2 respondents also wanted access through the site to important locations, such as the DLR on the north side of the development and access for north side residents to businesses in Charlton Riverside.

The other comments were regarding the need for Controlled Parking Zones for the development as well as more cycle parking; the need to take into account neighbour access to sunlight; more green space onsite; the desire for the proposals to be reduced in height to six storeys (to reflect the Charlton Riverside Masterplan); connections to the local DLR station; a sense of place for the development and new commercial facilities such as gyms, bars and restaurants. The final response said that the development should reflect the parameters of the Charlton Riverside Masterplan.

COMMENT	RESPONSE COUNT
Heritage	3
Access	2
Parking	1
Sunlight	1
Green Space	1
Transport Connections	1
Height	1
Sense of Place	1
Cycle Parking	1
Commercial	1
Masterplan	1

Question Seven

7. General Comments

Analysis

The final question was for general comment or ad hoc responses from respondents.

3 respondents said they did not support the height of the development saying it was too high for the area.

2 respondents felt that there is not enough parking proposed in the scheme and were worried that people would park on the streets surrounding the site which would impact existing and new residents. Both respondents felt there was already an existing issue with parking in the area and these proposals would not help the current situation.

2 respondents said that the development should reflect the parameters of the Charlton Riverside Masterplan.

2 respondents mentioned the community saying that the proposals should have more community input and that the site should have more community space included.

The other comments were spread across the need for more green space; objecting to the inclusion of flats; need for a GP surgery; the need to take transport infrastructure into account; the hope that the area’s heritage and culture would be included in the design; a reminder to include disability access and also a request to include leisure facilities such as a cinema.

GENERAL & AD HOC COMMENTS	RESPONSE COUNT
Height	3
Parking	2
Masterplan	2
Community	2
Green Space	1
Flats	1
GP Surgery	1
Transport	1
Heritage	1
Culture	1
Disability Access	1
Leisure Facilities	1
Density	1
Business	1



COMMUNITY GROUP FEEDBACK

The feedback included two responses from prominent community groups – Charlton Together and the Charlton Society. The representative from Charlton Together said the area had great potential and supported the regeneration of this site if the proposals conformed to the Charlton Riverside Masterplan. They also wished to see this site benefit the local community through high-quality architecture, inclusion of more green space and an overall design that created a sense of place for the community whilst emphasising the heritage of the area. Charlton Together also wished for the site to be a cornerstone of local economic regeneration by providing business opportunities and creating jobs for the local community. On housing, Charlton Together asked that the site provide affordable housing which complied with the current Borough of Greenwich threshold of 35%. They also wished the proposals to be ‘homes’ rather than ‘units’ and ensure that at least 50% were built for families.

The Charlton Society also sent feedback saying that the proposals should be reduced in height to conform with the Charlton Riverside Masterplan and to prevent the site being overly large in scale. They also fed back that the design should be improved and be an example of “exemplar design” as stipulated in the masterplan.

These comments have been reviewed alongside the rest of the feedback provided during the consultation period and have been taken into consideration when submitting these proposals.

POST-EXHIBITION ACTIVITIES

An Online ‘Webinar’

The project team believe they have engaged proactively, effectively and at an early stage with the Council, local elected representatives, local community groups and local residents, and this approach continued following the public exhibition through an online ‘webinar’.

Advertised by a press release to local ‘blogs’ and through local community groups including Charlton Together, a one-hour ‘webinar’ was held on Thursday 18th June. This additional round of community consultation allowed anyone with outstanding questions or queries about the proposals to pose them directly to the project team for a 60 minute session.

The event, moderated by the project team, attracted 5 visitors. The project team provided a 30 minute presentation based on the proposals displayed at the virtual exhibition as well as explained the changes made to the proposals since the consultation had begun.

The floor was then opened to questions, which are displayed on the next pages:

Question

What is being done to protect the manufacturing jobs currently within the site, most of which are car manufacturing and repairs?

Response

Around 20 people are currently employed onsite which constitutes a relatively low density use and many businesses have already vacated the premises.

There is a slight reduction in Square footage through these proposals, however the new businesses will be able to more efficient with the commercial space given the design.

The development will create more jobs than currently onsite and existing jobs aren't necessarily going to be lost as the applicant will support businesses when relocating.

Bearing in mind the significant amount of retail in Charlton Riverside, does there need to be more retail in the area?

The retail requirement is small onsite so is unlikely to have a detrimental impact to local businesses. Previous development in the area hasn't included much retail so there is a need locally.

The stipulation in the SPD is this site needs to be a local centre and community hub.

Can you confirm the mix of affordable housing?

The site will conform to Greenwich Council's threshold of 35% affordable housing on provision of unit numbers. As a breakdown of the housing mix - 70% of the affordable will be London affordable and 30% will be shared ownership. This will include larger family homes in the London Affordable homes.

What changes have you made in regard to Rockwell report?

The project team has reviewed the Rockwell report and have taken into consideration the comments made on height. As a result, the proposals have been brought down in scale whilst retaining 10 storeys as the highest point as stipulated in the guidance of the SPD.

Have you been in discussions with TFL to discuss sustainable transport?

The project team have been in discussion with TFL and as part of this the team will be contributing to sustainable travel through a financial contribution in this application.

The proposals will also facilitate a new route through the site to improve permeability.

Question

Are onsite businesses being relocated in the local area?

Response

The project team have had conversations with the onsite businesses and we will be supporting them should they relocate.

Have you considered including creative jobs onsite?

The proposals will include employment space, and the applicant would be interested in finding 'meanwhile' uses for the space should these not be occupied immediately on construction. We would be open to utilise this space for creative purposes, i.e. galleries or workshops if possible.

CONCLUSION

Throughout this consultation process, the Applicant's proposals have been subject to wider consultation with the Charlton community and other stakeholders.

In summary:

- » Extensive pre-application discussions with Officers of both the Royal Borough of Greenwich
- » An online public exhibition in May and June 2020, which was widely advertised via resident letter and social media leading to over 1,061 website visits
- » An online webinar which was attended by political and community stakeholders
- » Community Engagement throughout the pre-application and post submission phase of the planning application

Engagement throughout the pre-application and post submission phase of the planning application has been thorough and beneficial, and culminated in proposals that have been generally well received by the local community and other stakeholders.

Our letter to more than 3,000 local homes and businesses, alongside correspondence with local stakeholder groups and Facebook adverts meant that over 20,000 local people were engaged. This is the result of our adverts reaching 17,004 local Facebook users, resulting in 1,061 additional visits to our consultation website than the letters alone would have triggered.

We therefore feel that we have engaged with a wide demographic spectrum in Charlton, and that the resulting feedback is therefore also representative of the broad spectrum of local feeling towards the re-development of this site. In short, Aitch Group's proposals enjoy the support of local people, and no clear adjustments are required ahead of a planned submission to the Council of a formal planning application in June.

The post-submission process will involve further dialogue and engagement, addressing any further comments received from statutory and non-statutory consultees.

POST-SUBMISSION ACTIVITIES

Following the submission of a formal planning application to the Royal Borough of Greenwich, the applicant and the project team will continue to engage with local elected representatives, community groups and local residents. Our consultation website will remain live throughout the formal planning process, and we will quickly and efficiently respond to any resident queries. We will also specifically seek to ensure that those who submitted feedback to the project team during the virtual public exhibition are kept up to date on the progress of the formal planning application, from submission through to a planning committee determination.

The project team are also aiming to hold a post-submission drop-in event, if COVID19 regulations allow.

If you require further information on this report, please contact Michael Stanworth at Lexington Communications:

- » **Email:** Michael.stanworth@lexcomm.co.uk
- » **Telephone:** 07974 300 509
- » **Post:** Michael Stanworth,
Head of Property and Local Government,
Lexington Communications,
5th Floor,
198 High Holborn,
London.
WC1V 7BD



APPENDICES



APPENDIX A

LETTER INVITATION FOR RESIDENTS AND LOCAL BUSINESSES FOR ONLINE PUBLIC EXHIBITION



Dear Resident

May 2020

Invitation to an online consultation regarding redevelopment plans for Eastmoor Street

Lexington Communications is working on behalf of Aitch Group, a London based property developer to undertake community consultation on their new homes proposals in Charlton Riverside. Aitch Group has more than 20 years' experience of bringing forward innovative spaces for communities across the capital to live and work, and are bringing forward plans for new homes here in Charlton.

Their site at 33-81 Eastmoor Street and 6-10 Westmoor Street falls within the Charlton Riverside Masterplan area and we are writing to invite you to take part in our online consultation.

Our Virtual Exhibition

You can find our virtual exhibition at www.EastmoorStreet.co.uk, where you will have the opportunity to learn more about Aitch Group, the site, and their initial ideas for its regeneration. The website will also allow you to ask the project team any questions you might have and also give us your views on the emerging proposals. Your feedback will influence how Aitch Group's plans progress ahead of their submission to the Royal Borough of Greenwich for consideration.

Although current guidance linked to COVID-19 discourages a face-to-face event at this time, the submitted plans will be presented at a community event later in the year.

Getting in Touch

We do hope you are able to log on and pass your views onto the project team. However, if you do not have access to the internet, or if you would like to talk to us separately about a specific aspect of the plans, we are happy to arrange a phone or video call at a date and time at your convenience.

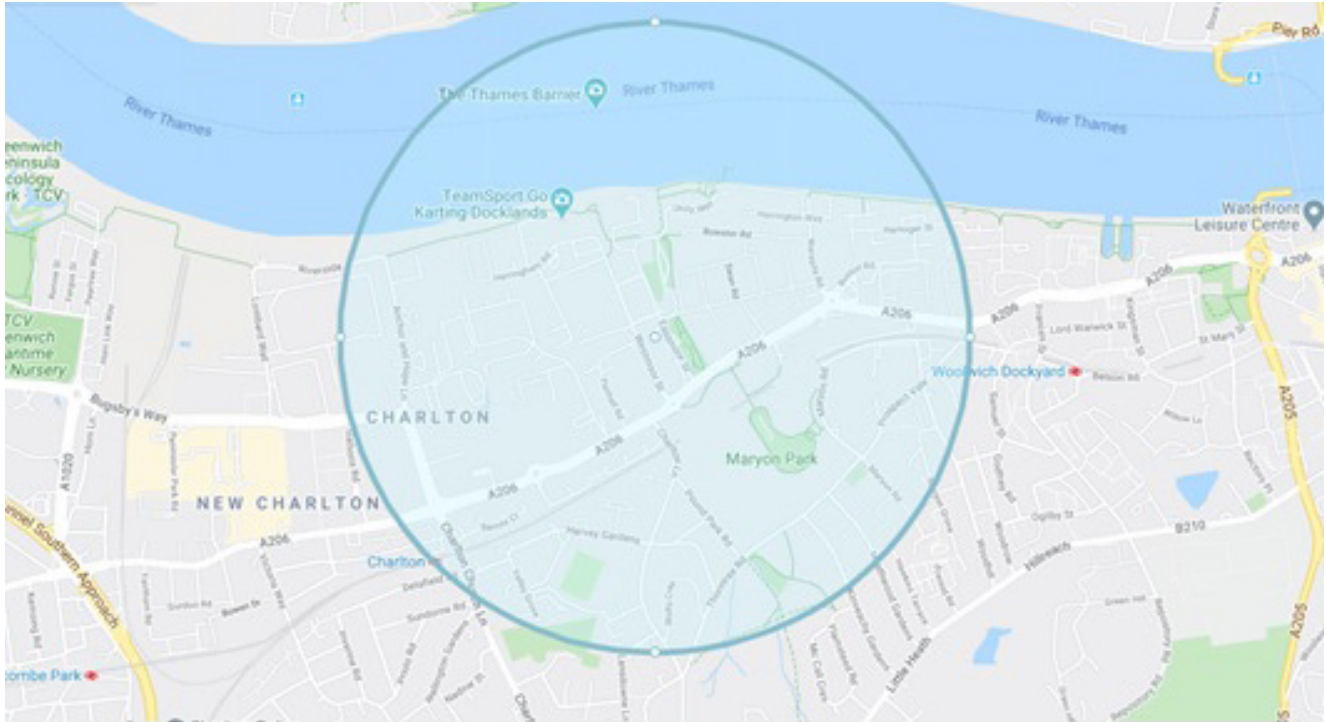
To arrange this, simply contact Michael Stanworth at Lexington Communications by calling 07974 300 509 or emailing Michael.Stanworth@lexcomm.co.uk.

Yours faithfully,

Michael Stanworth
Associate Director, Lexington Communications

APPENDIX B

LETTER DISTRIBUTION MAP



LOCAL BLOG POST REGARDING PROPOSALS AND EXHIBITION ON THE CHARLTON CHAMPION WEBSITE

Charlton Riverside: 230 homes in 10-storey blocks planned for Eastmoor Street

May 7, 2020 Darryl Chamberlain



Altch's plans, looking south: Barrier Gardens is to the left, with the Beaumont Beds (Optivo Homes) site behind and Mirfield Street at the front

Developer Altch Group has launched a consultation into plans for 230 new homes between Eastmoor Street and Westmoor Street on the Charlton Riverside.

With the coronavirus lockdown, it has launched a virtual exhibition of its plans, which would see 10-storey blocks built on a plot behind the current Beaumont Beds warehouse and to the west of Barrier Gardens.

It is the latest in a number of proposals for the riverside, all at varying stages in the planning process. None have yet been approved, never mind built, making imagining what these developments will be like somewhat tricky. While Altch says 35 per cent of the homes would be "affordable", this would be a mix of "affordable rent" and the much less affordable shared ownership. 30 car parking spaces are planned. More details can be found at www.eastmoorstreet.co.uk.

In January, the housing association Optivo Homes held a very short-notice consultation about a development on the Beaumont Beds site.

Elsewhere on the riverside, five major schemes are still in the works:

- The infamous **Rockwell** development off Anchor and Hope Lane, promising 771 homes but hated by its neighbours and refused by both Greenwich Council and, surprisingly, London mayor Sadiq Khan, who said he did not want "growth at any cost". Rather than negotiate with the neighbours, Rockwell has gone to the planning inspectors, with a verdict expected next month.
- 1,500 homes from developer Montreaux on the industrial estate containing the **Stone Foundries plant** behind the Stone Lake retail park. The land was sold last year and the scheme has not yet entered planning.
- Hyde housing association wants to build nearly 1,300 new homes at what it calls **Herringham Quarter**, using a number of sites including Maybanks Wharf.
- 500 homes are planned by developer Komoto at what it calls **Flint Glass Wharf**, the former Johnsen and Jorgensen glassworks which closed in 1981, between the Tarmac works and the Thames Barrier;
- U-I plans 500 homes on the old **Siemens glassworks site** on the Charlton/Woolwich border, along with a co-working hub for local businesses and space for light industry. A planning application for **Faraday Works** was made earlier this year.

APPENDIX D

EMAIL INVITE TO LOCAL AND POLITICAL STAKEHOLDERS

From:
Sent: Wednesday, May 6, 2020 9:19:58 AM
To:
Cc:
Subject: RE: Meeting request regarding development proposals on Eastmoor Street - Charlton Riverside Masterplan

Dear Councillor Fahy, Councillor Smith and Councillor Mbang,

I hope you are well and I wanted to get back in touch, following my emails to you earlier this year, to invite you to Aitch Group's virtual public exhibition on their Eastmoor Street plans.

A letter inviting residents to visit the exhibition and comment is being delivered to over 3,000 homes today. Whilst we recognise that you are extremely busy at this time and will have many other competing priorities for your time, we do hope you are able to find some time to view the plans.

Also, whilst a meeting is not permissible under current government guidance, we would be very happy to hold a video or phone call with you, if that is something you would like us to arrange.

The formal invite is below, and is also being posted to you in hard copy.

Yours sincerely,

APPENDIX E

VIRTUAL EXHIBITION PAGE ON CONSULTATION WEBSITE



APPENDIX F

ONLINE EXHIBITION BANNERS



33-81 Eastmoor Street
and
6-10 Westmoor Street

Thank you for downloading our vision document on the future of 33-81 Eastmoor Street and 6-10 Westmoor Street, which builds on the information that is already available on our consultation website: www.EastmoorStreet.co.uk.

Together, our website and this vision document set out our initial plans, set within the wider context of the Royal Borough of Greenwich's aspirations for the area, as set down in the Charlton Riverside Masterplan.

Our Consultation & COVID19

In line with Government guidance regarding COVID-19, the planning system is still operating at this time. As a result, we are engaging with local people about these proposals and have organised this online consultation to hear your thoughts.

The site location (red) within the wider Charlton area

Aitch Group and the
Wider Project Team

As with all our project teams, Aitch Group has assembled experts with experience both in regeneration of brownfield sites such as here in Charlton, but also people who have worked in the Royal Borough of Greenwich.



www.aitchgroup.com

Based in Old Street, Aitch Group was founded in 1995 and over the last 25 years has worked hard to earn a reputation as a leading property developer in London.

Focused on uncovering the potential for regeneration to help communities to flourish people to thrive their primary focus in recent years has been on east and south east London.



www.alancamp.com

Based in Southwark, Alan Camp Architects are an award-winning practice that specialises in residential led mixed-use schemes, master-planning and bespoke design solutions.

Working with clients, local communities and key stakeholders, they create environments that enhance and respond to their needs. They have an exceptional record of securing planning permissions on constrained

sites and then working with contractors to construct high quality developments.

Their project experience in Greenwich is substantial, and includes working with Hyde Housing on another development site in the Charlton Riverside area as well as Seren Park, next to Maze Hill train station, and a housing development on the western side of Creek Road.



www.bptw.co.uk

Based at 40 Norman Road, just off Greenwich High Road, BPTW brings together specialisms in architecture and planning to transform not just physical spaces, but people's lives.

For over 30 years, innovation in design, planning, sustainability and construction technologies has established BPTW's

reputation as experts in residential development, neighbourhood place-making and mixed-use regeneration. In addition to working across London, they have worked on a range of schemes here in Greenwich, including Essential Living's Union Wharf scheme on Creek Road, Saxon Wharf on Norman Road and Plumstead Library.



Current Aitch Group Development - Bagel Factory, Hackney Wick



Current Aitch Group Development - Empress Works, Bethnal Green

ONLINE EXHIBITION BANNERS

The Charlton Riverside Masterplan

Aitch Group's site is located within the Charlton Riverside Masterplan area, and the Council's clear vision for this area in the coming years includes the delivery of:



5,000 - 7,500 new homes
The delivery of family housing and 35% affordable homes



4,400 new jobs
An integration of employment uses into new developments



A range of building heights, up to 10 storeys



Respect for and enhancement of key local heritage assets



A creative design response to the need for flood protection



New transport networks that reflect the site's historic layout

There are a number of other developments which are coming forward alongside our proposals which together will realise the Council's vision for Charlton Riverside. These can be seen on the map to the right.

Aitch Group are liaising closely with neighbouring developers to ensure their plans reflect a common design style, and to ensure that we are bringing the best possible development for Charlton Riverside's community.



Front Cover of Charlton Riverside Masterplan



Evelyn House development by Optivo, just to the south of our site



Outline of Charlton Riverside Masterplan area



Sketch of Montreaux's Stone Foundries development



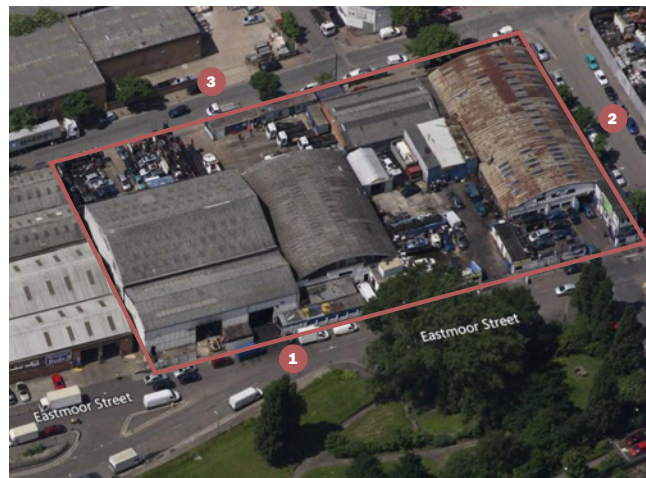
CGI of Rockwell's proposed development on Anchor and Hope Lane

The Current Site

Charlton Riverside will evolve from its current outdated industrial use into a new mixed-use community for the Borough.

Aitch Group's 1.47 acre site is located on Eastmoor Street in the south-east section of the Charlton Riverside Masterplan. It sits with Eastmoor Street to the east, Westmoor Street to the west, Mirfield Street to the north and an industrial unit to the south, with Woolwich Road beyond.

The current site is increasingly unfit for its current industrial uses, and would require a huge amount of work to bring it up to scratch given its inefficiency and state of disrepair. As a result, a major redevelopment of the site is required and, given the aspirations for the Charlton Riverside Masterplan, it is clear that a mixed-use scheme is the best fit for this site.



Overhead view of site viewing west from Moore Park



1. Eastmoor Street looking North



2. Mirfield Street looking West








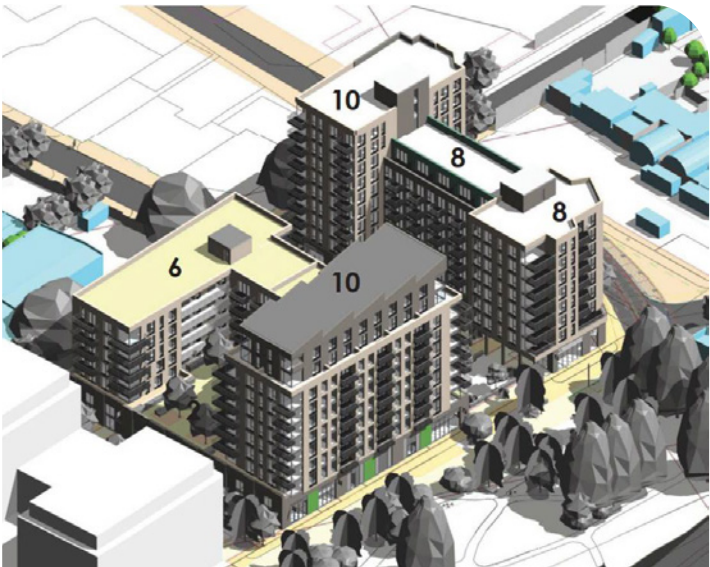
3. Westmoor Street looking North

The Emerging Proposals



Our plan for the regeneration of this site conforms to the aspirations of the Council for the Charlton Riverside Masterplan by hitting the following requirements:

Our proposals	Contribution to the Charlton Riverside Masterplan
 <p>c.230 new homes in a range of sizes, including 1, 2 and 3 bedroom homes in two buildings ranging up to 10 storeys in height</p>	Contributes c.230 new homes to the 5,000 – 7,500 new homes planned for masterplan area
 <p>The provision of 35% affordable housing, available for both affordable rent and shared ownership</p>	Hit the affordable housing targets set by Greenwich Council for new development
 <p>Ground floor provision for commercial space</p>	Integration of employment uses into all new development and contribute to the 4,400 new jobs in the Charlton Riverside Masterplan
 <p>30 car parking spaces alongside 14 wheelchair accessible spaces</p>	Conform to the Council's parking standards and GLA standards for wheelchair accessible parking spaces
 <p>Provision for play space for young children</p>	Realise the Charlton Riverside Masterplan vision for children to play outside with residents and visitors enjoying a varied and attractive selection of leisure, recreation and social activities



Overhead CGI of our draft and emerging proposals

Our Vision

The proposals have been designed to showcase the highest quality in architecture, and will reflect the Council's aspirations to improve accessibility and the public realm in the Charlton Riverside area.

Our plans will deliver around 230 new homes, delivering much needed new housing in a range of sizes and tenures. This reflects the Masterplan's guidance to deliver housing options for a diverse range of groups, including first time buyers and young families.

We will provide 35% affordable housing, which is the expected percentage by the Council for this development, split between affordable rented and shared ownership tenures. 30 car parking spaces will be provided, in line with the Council's parking guidelines.

The homes will be designed to be highly sustainable and energy efficient, with a number of homes specifically designed for wheelchair access.

The design will be to the highest quality, and the new green spaces will provide a welcoming and attractive places for residents to live, work and play.



CGI of the new employment spaces and opportunities being proposed



Indicative elevation CGIs showing details of the design quality



External CGI showing the high quality of the new homes being proposed



CGI showing one of the numerous communal play areas being proposed

Our Vision



View from Barrier Park



Penhall Road - Looking East



Eastmoor Street - Looking South



Westmoor Street Looking East - Pedestrian Route



Westmoor Street - Looking North



Eastmoor Street - Looking West

New Job Opportunities

The scheme will provide new job opportunities through a vibrant, active and accessible selection of commercial spaces, playing its part in the creation of the 4,400 additional jobs the Council wants to see delivered at Charlton Riverside.

To do this, we have designed a variety of commercial spaces, comprising retail and business uses, that will wrap around the ground floor of the scheme.

This ensures that the commercial aspects of this site are accessible to the local community and boosts the design credentials of the site by delivering an 'active frontage'.



Layout plan showing commercial space across the development



The commercial space is designed to be visible to create an active public realm



Opportunities for new café, community and employment spaces are being proposed



This development will provide a variety of commercial uses for a diverse range of businesses

ONLINE EXHIBITION BANNERS

Next Steps
& Have Your Say

Thank you for visiting our site today, and we hope that the information available in this vision document and our wider website was useful.

Next Steps
We would welcome your feedback on our exciting plans for Charlton Riverside. Over the coming weeks we will review all community feedback, ahead of submitting a planning application to the Royal Borough of Greenwich. If our plans are approved we would begin demolition work and construction in early 2021 and seek to welcome our first new residents to Eastmoor Street in 2023.

Have Your Say
Letting us know your views is easy – simply fill out the online feedback form on our website. Your comments are sent directly to the project team.

You can also contact us directly, should you have any specific questions. We are happy to discuss our plans on the phone, via a video call or over email, so please do not hesitate to get in touch.

Our contact details are:

Email
Michael Stanworth at Lexington Communications
michael.stanworth@lexcomm.co.uk

Call:
07974 300 509


Video call request:
email michael.stanworth@lexcomm.co.uk

We look forward to interacting with you and receiving your views in the coming days and weeks, and we look forward to inviting you to a physical community event later this year.



APPENDIX G

FEEDBACK PAGE ON CONSULTATION WEBSITE



We welcome your feedback

Thank you for visiting our site today and we hope that the information available here was useful.

Although our plans are still at an early stage, it is an ideal time to understand your views towards our initial ideas, and letting us know your views is easy – simply fill in the online survey below.

In terms of our questions to you as local residents, we especially want your ideas on:

Full Name (required)

Address (required)

Postcode (required)

Phone (required)

Your Email (required)

Age Group (required)

Please Select

What do you love about Charlton, and what do you think could be improved?

What are your views about the Masterplan area, and do you see it as a priority for Charlton?

Do you support the regeneration of this site for new housing and commercial space?

What sizes and tenures of housing would you like to see?

What companies would you like to see take up the commercial space on Eastmoor Street?

What would you like seen incorporated into the design of this development?

There is also space for your general comments, so please get in touch.





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